

**AGENDA
PLANNING COMMISSION MEETING
CITY OF SHOREVIEW**

DATE: January 26, 2016
TIME: 7:00 PM
PLACE: SHOREVIEW CITY ALL
LOCATION: 4600 NORTH VICTORIA

**1. CALL TO ORDER
ROLL CALL
APPROVAL OF AGENDA**

2. APPROVAL OF MINUTES

December 15, 2015

Brief Description of Meeting Process – Chair Steve Solomonson

3. REPORT ON CITY COUNCIL ACTIONS

Meeting Date: January 4th, 2016 and January 19th, 2016

4. NEW BUSINESS

**A. PUBLIC HEARING –PRELIMINARY PLAT, PLANNED UNIT DEVELOPMENT-
DEVELOPMENT STAGE, COMPREHENSIVE SIGN PLAN***

File No. 2603-16-02

Applicant: Kowalski Companies, Inc./Sidal Realty

Location: 441 Highway 96 West

**B. PUBLIC HEARING – COMPREHENSIVE PLAN AMENDMENT, REZONING,
PRELIMINARY PLAT, PLANNED UNIT DEVELOPMENT-DEVELOPMENT
STAGE ***

File No. 2602-16-01

Applicant: Elevage Development Group, LLC/Elevage Shoreview Holdings, LLC

Location: 3500 Rustic Place, 185 County Road E, 157 County Road E, and 3521 Rice Street.

C. PLANNED UNIT DEVELOPMENT-CONCEPT REVIEW

File No. 2604-16-03

Applicant: Ryan Companies

Location: Lot 1 / Block 1 Rice Creek Parkway

5. MISCELLANEOUS

*A. City Council Assignments for February 1st, 2016 and February 16th, 2016
Commission Members McCool and Doan*

B. 2016 Planning Commission Chair & Vice Chair

*C. Planning Commission Workshop @ 6:00 p.m. before the next regular meeting scheduled
February 23rd, 2016*

6. ADJOURNMENT

** These agenda items require City Council action. The Planning Commission will hold a hearing, obtain public comment, discuss the application and forward a recommendation to the City Council. The City Council will consider these items at their regular meetings which are held on the 1st or 3rd Monday of each month. For confirmation when an item is scheduled at the City Council, please contact the Community Development Department at 651-490-4682 or 651-490-4680 or check the City's website at www.shoreviewmn.gov.*

**SHOREVIEW PLANNING COMMISSION
MEETING MINUTES
December 15, 2015**

CALL TO ORDER

Chair Solomonson called the December 15, 2015 Shoreview Planning Commission meeting to order at 7:00 p.m.

ROLL CALL

The following Commissioners were present: Chair Solomonson; Commissioners, Ferrington, McCool, Peterson, Schumer, and Thompson.

Commissioner Doan was absent.

APPROVAL OF AGENDA

Commissioner Schumer noted the date on the agenda should be December 15, 2015.

MOTION: by Commissioner Schumer, seconded by Commissioner McCool to approve the December 15, 2015 Planning Commission meeting agenda as amended.

VOTE: Ayes - 6 Nays - 0

APPROVAL OF MINUTES

MOTION: by Commissioner Schumer, seconded by Commissioner Peterson to approve the November 17, 2015 Planning Commission meeting minutes as presented.

VOTE: Ayes - 5 Nays - 0 Abstain - 1 (Ferrington)

Commissioner Ferrington abstained, as she was not at the November 17th meeting.

REPORT ON CITY COUNCIL ACTIONS

Presentation by City Planner Kathleen Castle

The following items were approved by the City Council at its December 7, 2015 meeting, as recommended by the Planning Commission:

- Conditional Use Permit, detached accessory structure for Mike Heinze at 223 East Owasso Lane
- Comprehensive Sign Plan by Phoenix Signs for Presbyterian Church of the Way

NEW BUSINESS

COMPREHENSIVE SIGN PLAN

FILE NO: 2600-15-43
APPLICANT: SIGN MAINTENANCE & LIGHTING / WILLOW CREEK
CENTER
LOCATION: 5910 LEXINGTON AVE

Presentation by Senior Planner, Rob Warwick

Sign Maintenance & Lighting submitted an application to amend the Comprehensive Sign Plan for the Willow Creek retail center. The existing changeable copy sign would be replaced with a message center sign with full color and graphic capability. The new message center sign would be incorporated into the existing monument sign. It will be used by the retail center tenants.

The retail center was approved in 1987 with a Conditional Use Permit (CUP) for a fuel station. The CUP for the fuel station was amended in 1996 to add fuel islands and to expand the canopy over the fuel islands. It was amended again in 2009 to allow "pay at the pump" fuel service after hours, between 11:00 p.m. and 6:00 a.m. The retail center and parking area have also been expanded on the west end.

The Comprehensive Sign Plan was approved for a monument sign in 1998. Deviations for 11.4 feet in height and 146 square feet of area were approved. The monument sign was refaced in 2009 to reflect the Cenex fuel brand. No change to the sign base, height and structure is proposed.

A deviation for sign area is requested for this amendment. A minimum setback of 5 feet is required. The existing sign is 2 feet from Lexington Avenue and 16 feet from a trail. No change is requested for the location. Code limits sign area to 50 square feet; the applicants seek 98 square feet of total sign area.

The applicant states that the sign will comply with standards related to audio/pyrotechnics, brightness, and dimmer control. The message center has an area of 20 square feet, the minimum area required by the City in order for the sign to be sufficiently visible. The proposed sign minimizes any visual impact to adjoining residential and will operate during the hours of 6:00 a.m. to 11:00 p.m.

Adjacent to this retail center site are residential uses. The existing and proposed sign will be visible from neighboring residences. The nearest home is across Lexington Avenue, approximately 150 feet. Houses to the east and west are more than 250 feet from the sign location. The following City standards minimize visual impact to nearby residences:

- The sign will have color and graphics capability
- The minimum display duration is 8 seconds in order to be legible to passing motorists

- Displays that flash, blink, scroll or imitate motion are not permitted
- Text is expected to be dominantly used for the messages displayed
- Hours of use are limited from 6:00 a.m. to 11:00 p.m.

In order to approve the sign area deviation, staff made the following findings:

- The plan uses consistent color, size and material throughout the site.
- The 1998 approval affected only the monument sign; no other signs on the property were considered.
- The property has Cenex signs on the canopy over the fuel pumps and wall signs for individual tenants.
- Practical difficulty exists in that the property has a multi-tenant building which generates increased need for signage. The proposed message center sign will replace and update the current reader board sign with manually changed messages.
- The deviation results in a more unified plan and greater aesthetic appeal between signs.
- Temporary signs will be eliminated.
- Approving a deviation does not confer a special privilege on the applicant.
- The sign is located on a busy arterial road.
- The proposal is consistent with prior approvals by the City for sign area.
- The resulting sign will be effective, functional, attractive and compatible with City standards.
- The proposed sign is appropriate for this type of commercial use.

Property owners within 350 feet of the retail center were notified of the request. One resident has submitted several comments with concern about the lighting for the center, and the impact the commercial use has on the neighborhood. Night-time photos were also submitted.

Staff finds that the larger sign area is warranted due to the multi-tenant commercial use located on an arterial road. The new message center sign improves advertisement for goods and services in an efficient medium. Should the Commission have a favorable review, staff recommends the application be forwarded to the City Council with a recommendation for approval.

Commissioner McCool asked if there are hours of restriction on the reader board portion of the current sign. Mr. Warwick answered that there are no hourly restrictions on the sign. Commissioner McCool stated that the application is to simply swap the reader board portion of the sign for a message center sign. Mr. Warwick agreed but noted that the reader board area is 40 square feet. The message center sign area will be 20 square feet.

Commissioner Schumer noted that the photographs submitted relate to lighting of the retail center, not the message center sign. The proposal before the Commission is only for the message center sign. The retail center lighting is an issue to be taken up with the City. Also, the pictures do not resemble what he saw when he visited the site one night. Mr. Warwick noted that when the criteria for message center signs was determined, a formula for measuring brightness was specified in Code. Staff can measure the brightness of the new sign once it is installed.

Chair Solomonson asked how the total area was approved at 98 square feet. Mr. Warwick explained that the monument sign replaced a pylon sign in 1998. At that time, a larger area was

approved for the reader board at 76 square feet, almost double the 40 square feet allowed by Code. Chair Solomonson asked if there have been any complaints in regard to operation of the sign. Mr. Warwick answered that he has not received any complaints.

Chair Solomonson verified the location of the resident who has submitted concerns about the application. Mr. Warwick pointed out the resident's house, which is approximately 500 feet from the proposed sign.

Commissioner Ferrington asked if, with the last approval in 2009, the lights were required to be dimmed after 11:00 p.m. Mr. Warwick, answered that with the approval of the "pay at the pump" sign approved in 2009, there was a condition to restrict the number of canopy lights to six after 11:00 p.m.

Chair Solomonson opened the discussion to public comment.

Mr. Kevin Dunlevy, 5901 Ridge Creek Road, stated that his living room picture window, kitchen table and deck all face toward the retail center. His concern is all about light. The photos he submitted were taken at 3:00 a.m. on Saturday. The lights are not amber but are bright white lights. They are not being dimmed. It is a big source of light pollution. The retail center is unattractive in this residential neighborhood with no screening. The whole neighborhood in every direction is very dark except for this center. He patronizes the center and does not want to see it go out of business. He would like to see the lighting be better blended into the neighborhood. The sign does not achieve this. He is not concerned about the specific sign request in this application.

Commissioner Schumer agreed and stated that the message center will be turned off from 11:00 p.m. to 6:00 a.m.. It will not be as bright as the reader board and will be an improvement. The dimming of lights after hours is a compliance issue. Mr. Warwick responded that staff who were out early for snow plowing were able to note lighting conditions and reported it to be less bright than what is shown in the photos. Staff will continue to monitor conditions for compliance.

Commissioner McCool stated that although there is less light with the proposed sign, he is not convinced a message center will be an improvement over the existing a static sign because of the movement in the sign. His concern is about compliance when something new is being approved. He would like to make sure enforcement of lighting conditions is in place. Mr. Warwick stated that the sign will be set on a timer and will go off and on automatically. Commissioner McCool stated that it does not appear that a time is being used, other than for the sign, for the remaining lights on this site.

MOTION: by Commissioner Schumer, seconded by Commissioner Thompson to recommend the City Council approve the Comprehensive Sign Plan Amendment submitted by Maintenance, Lighting and Electrical, Inc., subject to the following conditions:

1. The sign shall comply with the plans submitted for the Comprehensive Sign Plan application. Any significant change will require review by the Planning Commission and City Council.

2. The applicant shall obtain a sign permit prior to the installation of any signs on the property.
3. The message center sign shall:
 - a. Display text shall be use a minimum 6-inch character height to be readable by passing motorists without distraction.
 - b. Messages shall be displayed in their entirety to allow passing motorists to read the entire copy.
 - c. Messages shall not include telephone numbers, email addresses or internet urls.
 - d. Messages shall be displayed for a minimum of 8 seconds, and shall change instantaneously.
 - e. Messages be presented in a static display, and shall not scroll, flash, blink or fade.
 - f. Advertisement is limited to the goods and services offered on-site. Text shall be the dominant feature of the display.
 - g. The message center sign shall not be operated between the hours of 11:00 pm and 6:00 am.
 - h. Said sign shall comply with the City's standards regarding brightness and dimmer control.
4. Temporary business signs, with the exception of window signs, sandwich board/T-frame signs placed adjacent to the building, and incidental displays in accordance with the Sign Code standards, shall not be displayed on the property.
5. The applicant shall prepare a landscape plan to address screening along the west side lot line. The plan is subject to approval of the City Planner. A landscape surety in the amount of \$2,500.00 shall be submitted to insure installation of the trees if the sign is installed before the landscaping work has been completed.

Approval is based on the following findings:

1. *The plan proposes signs consistent in color, size and materials throughout the site.* The overall design of the sign will remain as is and not change with this proposal. The sign will remain a cabinet style sign with internally lit sign panels identifying Cenex. The gas price display sign will also remain the same. The exiting reader board will be removed and replaced with a smaller message center sign.
2. *Approving the deviation is necessary to relieve a practical difficulty existing on the property.* The Willow Creek Center is a multi-tenant shopping center approximately 14,500 square feet in floor area. With multiple tenants, there is a greater need for signage, especially for temporary sales or events. The replacement of the existing readerboard with the message center sign provides a more efficient and aesthetic method to display or advertise products, services and special events than the use of temporary signs.
3. *The proposed deviations from the standards of Section 208 result in a more unified sign package and greater aesthetic appeal between signs on the site.* The design and size of

the monument sign will not be changing as a result of this proposal. The replacement of the readerboard with an electronic message center sign is intended to visually improve the look of the monument sign, as well as improve identification and advertising for tenants. Temporary signage shall be prohibited for the property since the message center sign provides a vehicle for the tenants to advertise special sales or events.

4. *Approving the deviation will not confer a special privilege on the applicant that would normally be denied under the Ordinance.* The Code allows for a sign area increase in order to permit a message center sign to comply with the minimum required 20 sf area. The retail center is located on Lexington Avenue, an arterial road with a very high traffic volume. The high traffic volume, coupled with the multiple-tenant use supports this type of signage on the property. This approval will not confer a special privilege on the applicant given the history of past City approvals for signs here.
5. *The resulting sign plan is effective, functional, attractive and compatible with community standards.* Staff believes the sign proposes an effective and functional method to advertise the center, in a manner compatible with the commercial use. The intent of the sign is to provide a more efficient method of communicating products, specials or events for tenants within the center.

VOTE:

Ayes - 6

Nays - 9

MISCELLANEOUS

City Council Meeting Assignments

Commissioners Peterson and Thompson will respectively attend the January 4, 2016 and January 19, 2016 City Council meetings.

ADJOURNMENT

MOTION: by Commissioner Schumer, seconded by Commissioner McCool to adjourn the meeting at 7:51 p.m.

VOTE:

Ayes - 6

Nays - 0

ATTEST:

Kathleen Castle
City Planner

TO: Planning Commission

FROM: Niki Hill, Economic Development and Planning Associate

DATE: January 21, 2016

SUBJECT: File No. 2603-16-02-, Sidal Realty/Kowalski Companies, Inc - Planned Unit Development – Development Stage, Preliminary Plat and Comprehensive Sign Plan, 441 Highway 96

Introduction

Preliminary Plat, Planned Unit Development – Development Stage, and Comprehensive Sign Plan applications have been submitted by Kowalski Companies, Inc and Sidal Realty Co. LP, for the property at 441 Highway 96. The applicants propose to repurpose the vacant 68,000 square foot building for a 25,000 square foot Kowalski's Market grocery store, with the remainder of the building being used for a bakery/production area for all their stores.

Site History

The subject property has been zoned and used for commercial uses dating back to at least the 1960s. An old retail strip center (former roller skating rink) was torn down when the Rainbow Foods building was constructed in 1995. The City approved plans to develop a 68,000 square foot Rainbow Foods grocery store on the property, as part of a larger 15-acre PUD that included the adjoining funeral home, car wash/oil change facility and Dairy Queen properties. The PUD addressed the property exchanges, stormwater management and joint access needed to better serve the commercial quadrant. These plans were reviewed via the PUD process and approved using the regulations from the C1, Retail Service District as the underlying zoning district for the subject property. Conditions were imposed on the property relating to parking lot design, drainage, off-site snow storage, truck delivery hours, landscaping, lighting and signage. The site was developed in accordance with these plans.

Rainbow vacated the site in July of 2014 and the building has remained vacant. The re-investment in the property by the applicants will significantly enhance the appearance and provide uses that will serve a community need.

Site Characteristics

The 10.64 acre property is located on Highway 96 and also has frontage on Highway 49 (Hodgson Road), which is immediately to the east. Access to the property is gained from Highway 49 at a signalized intersection with Village Center Drive in North Oaks and from a restricted right-in/out access driveway from Highway 96. Existing site improvements include the retail building, off-street parking, stormwater ponding and landscaping/fencing.

The adjacent property to the north is developed with the Oak Hill Montessori School and has an O, Office zoning designation. The property to the west is zoned PUD – with a funeral

home and R1, Detached Residential - with single family homes. Immediately to the south across Highway 96 is Scandia Shores – a senior housing facility, which is zoned PUD. To the immediate east there is a car-wash, gas-station and Dairy Queen. Across Highway 49 further to the east, is the City of North Oaks which is developed with a variety of commercial and/or business related land uses.

Preliminary Plat

The property is currently platted as Block 1, Lot 2 as part of the four lot - Crossroads Pond Plat from the 1995 Rainbow Foods project. The parcel includes a drainage easement for the stormwater pond over the northwest corner along with a variety of other easements that encumber the site. The proposed subdivision would divide the property into two lots:

Lot 1 – consisting of 9.2 acres, containing the existing former Rainbow store, northern half of the parking lot and the wetland and storm pond area to the west.

Lot 2 – a new proposed lot of 1.5 acres, developed with parking but intended for future retail development.

This new parcel is located in the southeastern corner of the property and would share the existing access off Highway 96 and Highway 49. This parcel is currently developed with a parking area, identification sign and stormwater infrastructure. Easements and related agreements will be required to address the shared driveway, access, parking, signage and private infrastructure that will exist between proposed Lots 1 and 2.

The plat complies with the minimum standards of the C1 district (Section 205.042) and the subdivision standards (Section 204). Easements are being required along Highway 96 and Highway 49. An existing easement will be retained for the wetland and stormwater infrastructure related to the drainage on the property and the ponding area.

PUD – Development Stage Amendment

An amendment to the PUD has also been submitted to reflect changes associated with the proposed plat, property enhancements and changes related to Kowalski's Market daily operations.

Building Repurposing

The 68,000 square foot building will be broken out into different complimentary uses for Kowalski's Market. The grocery store will be approximately 25,000 square feet and also includes a wine shop, coffee shop and a culinary kitchen and cookware store. The remaining floor area will be used for a mixture of production space for their kitchen and bakery operations, a catering facility, a gift pricing and distribution facility, and freezer, cooler and warehousing.

In their agreement to purchase the property, Kowalski's will also obtain exclusive use and use restrictions on what is developed in the future for the proposed new retail pad (Kowalski's will not be the owner of Lot 2). It is anticipated that these use restrictions will be incorporated and further defined by the City in the agreements associated with the Final PUD.

Parking

Kowalski's Market does not intend to change the parking lot from its current configuration but will resurface and restripe the parking lot to improve the appearance and functionality. The existing globe style parking lot light fixtures will be replaced LED down light style fixtures. This will reduce energy cost and lessen light pollution, while providing more light in the parking lot.

The off-street parking lot provides 312 stalls south of the existing building. The proposed use of the existing building requires 223 stalls (Section 206.020). With the subdivision, the number of stalls located on the Lot 1(Kowalski's Market) is reduced to 159 stalls. There is additional parking located behind the building which is not included on the site plan. Staff estimates that there are about 40 stalls increasing the number of stalls to 199. This portion of the parking area will be used by employees. Kowalski's has indicated that the parking is sufficient to meet their needs. Parking ratio's at their other stores ranges from 4 to 4.89 stalls per 1,000 square feet of retail space.

Since shared parking will be available with the adjoining Lot 2, the staff believes parking for the proposed Kowalski's Market will be sufficient. Parking will be further analyzed when a development plan is received for Lot 2.

Access Improvements

While the primary access points to the site will remain, the project proposes improved access from Highway 96 via a new free left-turn lane from eastbound Highway 96 to serve the property. The developer, Oppidan Development, engaged a transportation engineering firm to provide design analysis and estimated traffic counts for the County to review. Ramsey County has sent the City a letter indicating that they will approve the left-turn lane and associated access, as has been proposed. As part of the City's financial assistance package for this project, the City will be managing the left turn lane construction as a public improvement project to be completed.

Landscaping

Kowalski's Market proposes to replace the majority of the plant materials to enhance the appearance of the property. The landscape island in front of the building in the southeast corner will be removed to open up the front sidewalk for outdoor seasonal seating and sales. The existing wood fence along the western edge of the parking lot will be replaced with a more durable and attractive decorative metal fence.

The proposed changes will provide landscape screening of the parking areas and preserve screening for the nearby residential neighborhood (Section 206.010). However, staff does have a concern about the potential removal of the screening along the north of the existing

access drive which screens the loading dock from Highway 49. It is suggested that the final landscaping plan be refined to provide more substantive evergreen landscape materials in this location or to supplement with a decorative fence behind the proposed vegetation to assist with screening.

Delivery Hours

Kowalski's Market is also asking that restrictions on the hours of deliveries imposed with the original PUD be modified. The condition prohibits truck traffic between 12:00 am and 6:00 am. These conditions were placed upon the PUD due to concerns expressed by nearby residents regarding noise. Vehicles using the docks enter from Highway 49 - drive around the east side of the building and pull back into the loading area.

Kowalski's has indicated that their regular store and production deliveries could occur in accordance with these hours, however, the production bakery operations require flexibility from this requirement. With the exception of Sunday night, products from the production bakery are shipped by truck at 1:00 am and 4:00 am to the other Kowalski Markets. The truck would utilize the loading dock and have the vehicle turned off while loading. Kowalski's believes this would not be disruptive to the adjoining neighborhood.

Generally, conditions restricting hours of operation or deliveries are imposed when commercial or business land uses are directly adjacent to residential land uses. In this case, the property immediately to the north is the Oak Hill Montessori School – which would not be impacted. There are single-family homes located to the west and are separated by a wetland area that serves as a buffer. The closest home is approximately 330-feet away from the loading area. With the loading docks oriented and located on the east side of the building, away from the residential uses, the Staff does not believe this will be a concern. Delivery hours will be further addressed in the Development Agreement.

Comprehensive Sign Plan

The applicants have submitted a Comprehensive Sign Plan package (Section 208.060) that identifies two new freestanding signs at the entrances of Highway 96 and Highway 49 as well as their proposed wall signs for the South and East elevations. It should be noted that an approved plan may vary from the design and dimensional standards set forth in the Sign Code without approval of a formal variance, provided it would result in attractive signage that is compatible with the premises and with adjoining development.

Free-standing signs (Section 208.040 (B12))

The existing freestanding sign locations along Highway 96 and Highway 49 would remain the same for the two new proposed signs with a height of 20' and width of 18'8". The sign face itself has a total area of 132 square foot. The proposed sign face exceeds the City's standards by 32 square feet. Staff believes that the proposed new monument signs are attractive in both design and materials, important for identification of a building that has a very deep setback from the highway, identifies the multiple uses within the building, and is proportionate to the size of the building.

These free-standing signs may be used to identify the future user (s) on Lot 2.

Wall Signs (Section 208.040 (B18))

Four wall signs are proposed along both the south and east sides – a primary sign identifying the Kowalski’s Market and 3 subsidiary signs that identify The Wine Shop, Starbucks and Cooks of Crocus Hill. This requires a deviation from the City’s sign standards as a maximum of one wall sign is permitted unless the structure faces two or more arterial roads. Deviations are needed for the total sign length on the East side as well as identified by the (*) in the table below.

Building Elevation	Sign Area	Sign Length
South (Front)	<i>340.6 square feet permitted</i>	<i>56 feet permitted</i>
Kowalski’s Market	139 square feet	23’2”
The Wine Shop	22.5	9’
Cooks of Crocus Hill	32.5	10’10”
Starbucks	22.56	9’8”
<i>Total</i>	<i>216.56</i>	<i>52.67 feet</i>
East (Side)	<i>255.6 square feet permitted</i>	<i>42.6 feet permitted</i>
Kowalski’s Market	139 square feet	23’2”
The Wine Shop	22.5	9’
Cooks of Crocus Hill	32.5	10’10”
Starbucks	22.56	9’8”
<i>Total</i>	<i>216.56</i>	<i>52.67 feet*</i>

The walls signs proposed are reasonable and will be attractive in their design. The additional signage on the south and east building walls will identify the other uses in the building for members of the public using Highway 96 or Hodgson Rd. The proposed signage size does not overwhelm the building elevation on the east and is consistent with the size wall signage on the south elevation.

Public Comment and Agency Review

The City notified property owners within 350 feet of the development. Legal notice of the public hearing was also published in the City’s legal newspaper. Comments from the Lake Johanna Fire Department were received; indicating no significant issues or concerns, and are attached. The Rice Creek Watershed District also indicated that a watershed permit may be required if over 10,000 square feet of area is disturbed when Lot 2 is developed. No other comments have been received.

Recommendation

The submitted plans were reviewed in accordance with the approved PUD and the City’s development standards, land use policies and sign regulations for this site. The continued use

of this site with a grocery store and support operations facility is consistent with underlying C1 zoning and the PUD. Staff is recommending the Planning Commission recommend the City Council approve the preliminary plat, amended PUD - Development Stage, and Comprehensive Sign Plan, subject to the following conditions:

Preliminary Plat

1. The applicant shall execute an agreement between the Lots 1 and 2 addressing the shared infrastructure including access, parking, signage, utilities and maintenance. Said agreements shall be submitted to the City Attorney for review and approval prior to the City's release of the Final Plat.
2. Executed and recorded copies of the required agreements shall be submitted to the City prior to the issuance of a building permit on Lot 2.
3. The Final Plat shall be submitted to the City for approval with the Final Stage PUD application.

Planned Unit Development – Amendment

1. This PUD amendment replaces the previous PUD approval from 1995.
2. A Development Agreement shall be executed and shall include applicable provisions from the previous PUD approvals referenced in Condition No. 1 above as well as any requirements associated with this PUD amendment.
3. Future development of Lot 2 shall require Site and Building Plan Review.
4. Kowalski's agrees to work with the City on refining the landscape plan that addresses better screening for the loading dock area on the northeast side of the property. Said plan shall be submitted with the Final PUD application.
5. The Development Agreement will amend the conditions regarding truck deliveries to correspond to the proposed uses.
6. Prior to submittal of a Final PUD application, Kowalski's shall verify the number of parking stalls provided on the property including the parking located north of the building. These stalls shall be identified on a site plan.
7. The City's prefers that the freestanding signs be shared with the future use of Lot 2.

Comprehensive Sign Plan

1. The signs on the property shall comply with the plans submitted for the Comprehensive Sign Plan application.
2. Signage shall be maintained in accordance with the City's Sign Code.
3. The applicant shall obtain a sign permit prior to the installation of the new signs on the property.

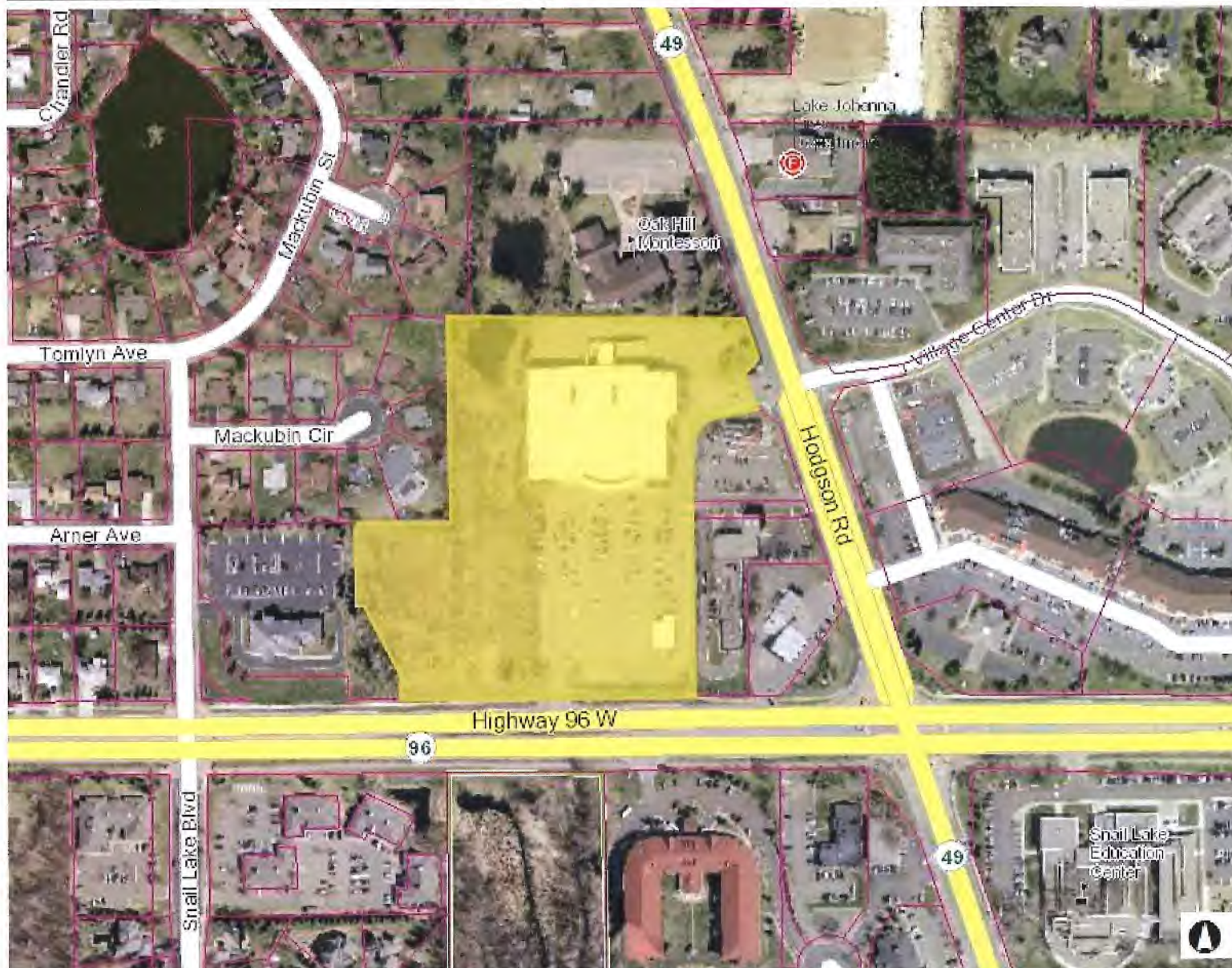
Attachments:

1. Location Map
2. Applicant's Statement and Submitted Plans
3. Comments Received
4. Motion Sheet

Legend



- City Halls
- Schools
- Hospitals
- Fire Stations
- Police Stations
- Recreational Centers
- Parcel Points
- Parcel Boundaries



599.8 0 299.92 599.8 Feet

NAD_1983_HARN_Adj_MN_Ramsey_Feet
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This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.

THIS MAP IS NOT TO BE USED FOR NAVIGATION

Notes

File No. 2603-16-02
441 Highway 96



To: Department of Community Development
City of Shoreview
C/O Kathleen Castle, City Planner

From: Kowalski's Market
Wilkus Architects, PA

Date: January 11, 2016

RE: Kowalski's Market
441 Highway 96 W.
Shoreview, Minnesota

Planned Unit Development Narrative

Kowalski's Market is requesting approval of the attached Planned Unit Development – Development Stage Application. The Kowalski's Market proposes to work within the previous existing and approved P.U.D./Development Code.

EXTERIOR

Kowalski's Market proposes to enhance the existing exterior walls through changes and additions of building materials and colors. With the addition of E.I.F.S. to the upper portions of the exterior wall, we'll add detail and contrast to the existing plain brick facade. The existing iconic Rainbow entry will be removed and replaced with Kowalski's branded entry similar to those found in other successful Kowalski's Market locations. Windows will be added to the South and East elevations to provide Architectural appeal and natural lighting to the interior spaces. Kowalski's Market proposes to provide a canopy at the main entry and exit to provide shelter from weather and identify to their future customers the entrance location. Fabric awnings are proposed over the new windows on the South and East elevations to add interest and bring the down the scale of the existing exterior walls to the customer level.

PROPOSED USES

Grocery store will be approximately 25,000 square feet and will be very similar to all of our other stores with a full service departments such as deli, sushi, imported cheese, bakery, meat & fresh seafood, produce, gift, floral, grocery, dairy & frozen, Cook of Crocus Hill culinary kitchen and cookware store, Starbucks Coffee and our Wine Shop. Our store will provide the best quality food possible that includes natural, organic, traditional products. We will also provide many great family recipe products, our signature Kowalski's products as well as some the most unique and trend setting products in the Twin Cities market. Our great employees will provide a very high level of service to make sure our customers really enjoy their shopping experience in our store including bagging and carry out service for all customers.

- Store hours will be 6:00 am – 11:00 pm every day, (Hours vary on holiday weeks)



- Wine shop hours will most likely be Mon – Saturday 9:00 am – 9:00 pm, closed Sunday.
- Starbucks hours will be 6:00 am – 8:00 p everyday
- Peak shift times for the grocery store are 11:00 am – 1:00 and 4:00 pm – 6:00 pm. There would be around 35 – 40 employees working at each peak time. Our busiest days are Saturday and Sunday.

Central Production Bakery will be approximately 12,000 square feet and is the primary producer of very high quality bakery product for all of our Kowalski stores such as cakes, specialty cakes, pies, fudge, pastries, breads and many more products. The bakery also focuses on research and development of new and unique items for our bakeries as well as developing items from family recipes. Peak shift time for the Production Bakery is 8:00 a.m. and they would have around 20 employees working. We do not operate our Production Bakery on Sundays.

Central Production Kitchen will be approximately 7,800 square feet and is the primary producer of very high quality deli and cheese product for all of our Kowalski stores such as ready to eat meals, pastas, salads, sandwiches, specialty products, cheeses and many more products. The kitchen also focuses on research and development of new and unique items for our deli's as well as developing items from family recipes. Peak shift time for the Production Kitchen is also 8:00 a.m. and they would have around 20 employees working as well. We do not operate our Production Kitchen on Sundays.

Catering facility will be approximately 2,155 square feet and will produce high quality products and full services for weddings, graduations, funerals, business events, meetings, home parties and many other events throughout the Twin Cities. Peak shift time for the Catering facility is around 2:00 pm and they would have around 10 employees working.

Gift pricing and distribution facility will be approximately 4,800 square feet and will receive, sort and price new gift items for all of our Kowalski stores. Once items are sorted and priced they are shipped to our stores with our deli and bakery deliveries. The peak shift for gift is around 11:00 am and does varies some but they never have more than 2 employees on at any one time.

We may have a meat processing facility which would be about 1,200 square feet in size and would produce smokehouse products, fresh sausage products, value added meat and seafood items and potential other meat items for our Kowalski stores. Peak shift time would be 9:00 and they would have around 3 employees working.

The remaining 15,000 square feet will be used for our transportation director, offices, holding coolers and freezers and grocery warehousing space for our Kowalski's signature products.

Any semi-trailers that are not being used that day will be safely stored behind the building.
(around 2 trailers)



The majority of employee parking will be behind the store and some employees will park in the front parking lot at the very far west end of the property.

We do not anticipate and problems with traffic because our employee and customer flows vary throughout the day and although we will have sporadic deliveries throughout the day and evening the majority of deliveries to and from the store are done between 5:00 am and 3:00 pm. The grocery store has very minimal deliveries on Wednesdays and Sundays.

LANDSCAPING

Kowalski's Market proposes to clean up the existing green spaces that have become over grown and unappealing. As initially discussed with City staff, much of the trees and shrubs will be removed, and the existing landscaping will be replanted. The landscape island in front of the building | the Southeast corner will be removed to open up the front sidewalk for outdoor seasonal seating and sales. The existing wood fence along the Western edge of the parking lot will be replaced with a more durable and attractive decorative metal fence.

PARKING LOT and LIGHTING

Kowalski's Markets does not intend to change the parking lot from its current configuration, but will resurface and restripe the parking lot to freshen its appearance. The existing globe style parking lot light fixtures will be replaced LED down light style fixtures. This will reduce energy cost and lessen light pollution, while providing more light in the parking lot.

SUMMARY

Kowalski's Market believes the proposed redevelopment of the existing Rainbow building and site is consistent with the existing P.U.D. and Development Code. Kowalski's has over 30 years of grocery experience, and looks forward to long relationship with the City of Shoreview and the neighboring communities

Thank you for your consideration of our request.

Sincerely,

Kowalski's Markets

Wilkus Architects, P.A.

CROSSROADS POND SECOND

R.T. DOC. NO. _____

PRELIMINARY

KNOW ALL MEN BY THESE PRESENTS: That Sidal Realty Co. Limited Partnership, a Minnesota limited partnership, fee owner of the following described property situated in the City of Shoreview, County of Ramsey, State of Minnesota:

Lot 2, Block 1, Crossroads Pond, according to the recorded plat thereof, Ramsey County, Minnesota

Abstract property

Has caused the same to be surveyed and platted as CROSSROADS POND SECOND and does hereby dedicate or donate to the public for public use forever, the public way, and also dedicating the drainage and utility easements as shown on this plat.

In witness whereof said Sidal Realty Co. Limited Partnership, a Minnesota limited partnership, has caused these presents to be signed by its proper officer this _____ day of _____, 201____.

Signed: Sidal Realty Co. Limited Partnership, a Minnesota limited partnership

_____, Its _____

STATE OF _____
COUNTY OF _____

The foregoing instrument was acknowledged before me this _____ day of _____, 201____, by _____ of KTJ 255, LLC, a Minnesota limited liability company, on behalf of the company.

Notary Public _____ County, _____
My commission expires _____

I, Eric A. Roeser, Professional Land Surveyor, do hereby certify that I have surveyed or directly supervised the survey of the property described on this plat; prepared this plat or directly supervised the preparation of this plat; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on this plat have been correctly set; that all water boundaries and wet lands, as defined in Minnesota Statutes Section 505.01, Subd. 3, as of the date of the surveyor's certification are shown and labeled on this plat; and all public ways are shown and labeled on this plat.

Dated this _____ day of _____, 201____.

Eric A. Roeser, Land Surveyor
Minnesota License No. 47476

STATE OF _____
COUNTY OF _____

The foregoing Surveyor's Certificate was acknowledged before me this _____ day of _____, 201____, by Eric A. Roeser, Land Surveyor, Minnesota License No. 47476.

Notary Public _____ County, _____
My commission expires _____

CITY OF SHOREVIEW

We do hereby certify that on the _____ day of _____, 201____, the City Council of the City of Shoreview, Minnesota, approved this plat. Also, the conditions of Minnesota Statutes, Section 505.03, Subd. 2, have been fulfilled.

_____, Mayor _____ Clerk

DEPARTMENT OF PROPERTY RECORDS AND REVENUE

Pursuant to Minnesota Statutes, Section 505.021, Subd. 9, taxes payable in the year _____ on the land hereinbefore described have been paid. Also, pursuant to Minnesota Statutes, Section 272.12, there are no delinquent taxes and transfer entered this _____ day of _____, 201____.

_____, Director By: _____ Deputy
Property Records and Revenue

COUNTY SURVEYOR

I hereby certify that this plat complies with the requirements of Minnesota Statutes, Section 505.021, and is approved pursuant to Minnesota Statutes, Section 383A.42, this _____ day of _____, 201____.

Craig W. Hinzman, Land Surveyor
Ramsey County Surveyor

REGISTRAR OF TITLES
County of Ramsey, State of Minnesota

I hereby certify that this plat of CROSSROADS POND SECOND was filed in the office of the Registrar of Titles for public record this _____ day of _____, 201____, at _____ o'clock _____ M., and was duly filed in Book _____ of Plats, Pages _____ and _____ as Document Number _____.

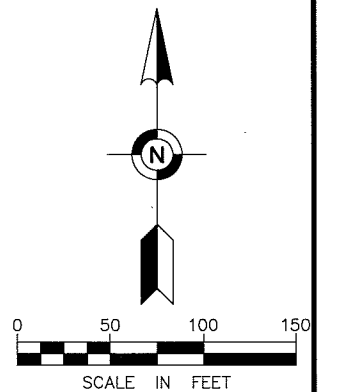
Deputy Registrar of Titles

COUNTY RECORDER
County of Ramsey, State of Minnesota

I hereby certify that this plat of CROSSROADS POND SECOND was filed in the office of the County Recorder for public record this _____ day of _____, 201____, at _____ o'clock _____ M., and was duly filed in Book _____ of Plats, Pages _____ and _____ as Document Number _____.

Deputy County Recorder

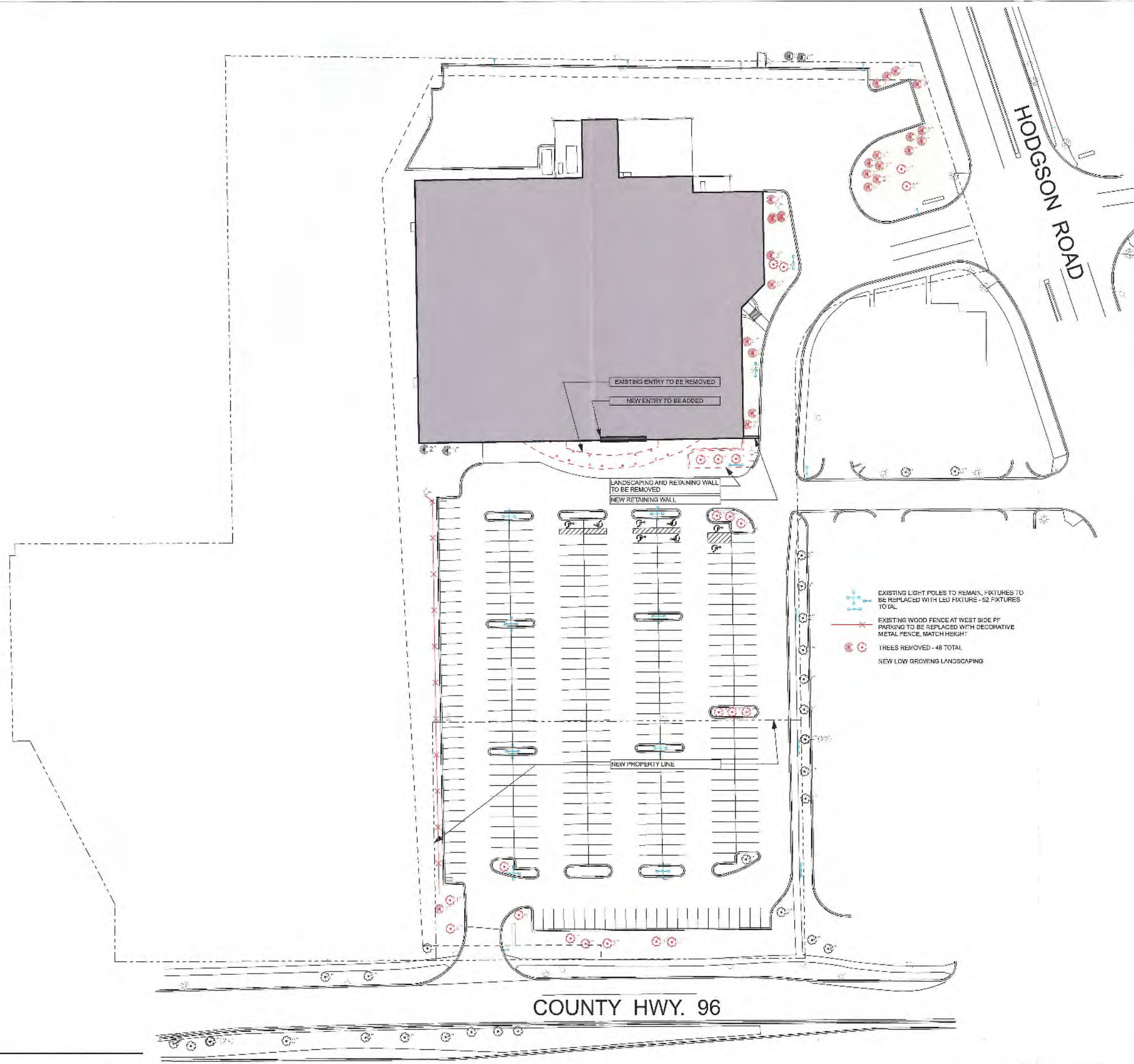
R.T. DOC. NO. _____



- DENOTES FOUND CAST IRON MONUMENT
- DENOTES FOUND GRANITE MONUMENT
- DENOTES FOUND 1/2 INCH IRON PIPE MONUMENT UNLESS OTHERWISE NOTED
- ✱ DENOTES FOUND CHISELED "X"
- ✱ DENOTES FOUND PK NAIL
- DENOTES 1/2 INCH BY 14 INCH IRON PIPE MONUMENT SET & MARKED BY LICENSE 47476

The orientation of this bearing system is based on the south line of the Southwest Quarter of Section 13, which is assumed to bear North 89 degrees 42 minutes 20 seconds East





CONSULTANT:



1127 Valley View Rd., Eau Claire, WI 54601
Telephone: 920.841.8369 www.wilkusarch.com

CLIENT:

PROJECT INFORMATION:

KOWALSKI'S MARKETS
SHOREVIEW, MINNESOTA

SEAL:
I HEREBY CERTIFY THAT THE PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

Print Name:

Signature:

License No.:

Expiration Date: 03-01-00

Date: 03-01-00

PROJECT NO.: 2015-1120

DRAWN BY: DLP

CHECKED BY: DLP

DATE	DATE
PRELIMINARY REVIEW	2015-11-11
PRELIMINARY REVIEW	2015-11-11
PRELIMINARY REVIEW	2015-11-11

REVISION: DATE:

PROJECT LOCATION:

SHOREVIEW, MINNESOTA

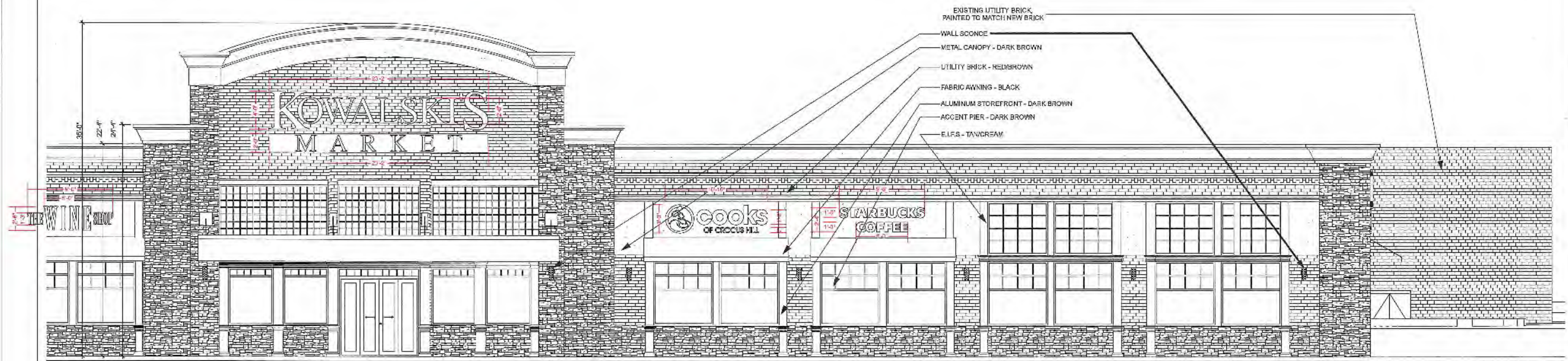
SHEET NUMBER/TITLE:

A1.0

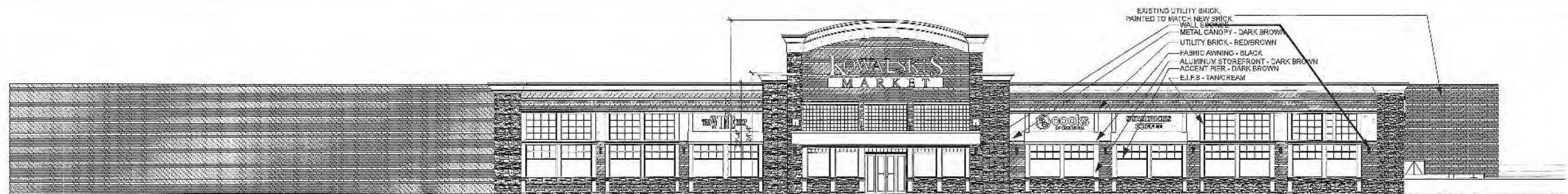
Architectural Site Plan



1 EAST ELEVATION
3/16" = 1'-0"



2 SOUTH ELEVATION
3/16" = 1'-0"



4 SOUTH ELEVATION
3/32" = 1'-0"

KOWALSKI'S MARKETS
SHOREVIEW, MINNESOTA

SEAL:
I HEREBY CERTIFY THAT THIS PLAN SPECIFICATION OR REPORT AND RECORDS BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A duly LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

Print Name:

Signature:

License No.:

Expiration Date: 03-01-02

Date: 03-01-02

PROJECT NO.: 2075-1728

DRAWN BY: CLP

CHECKED BY: CLP

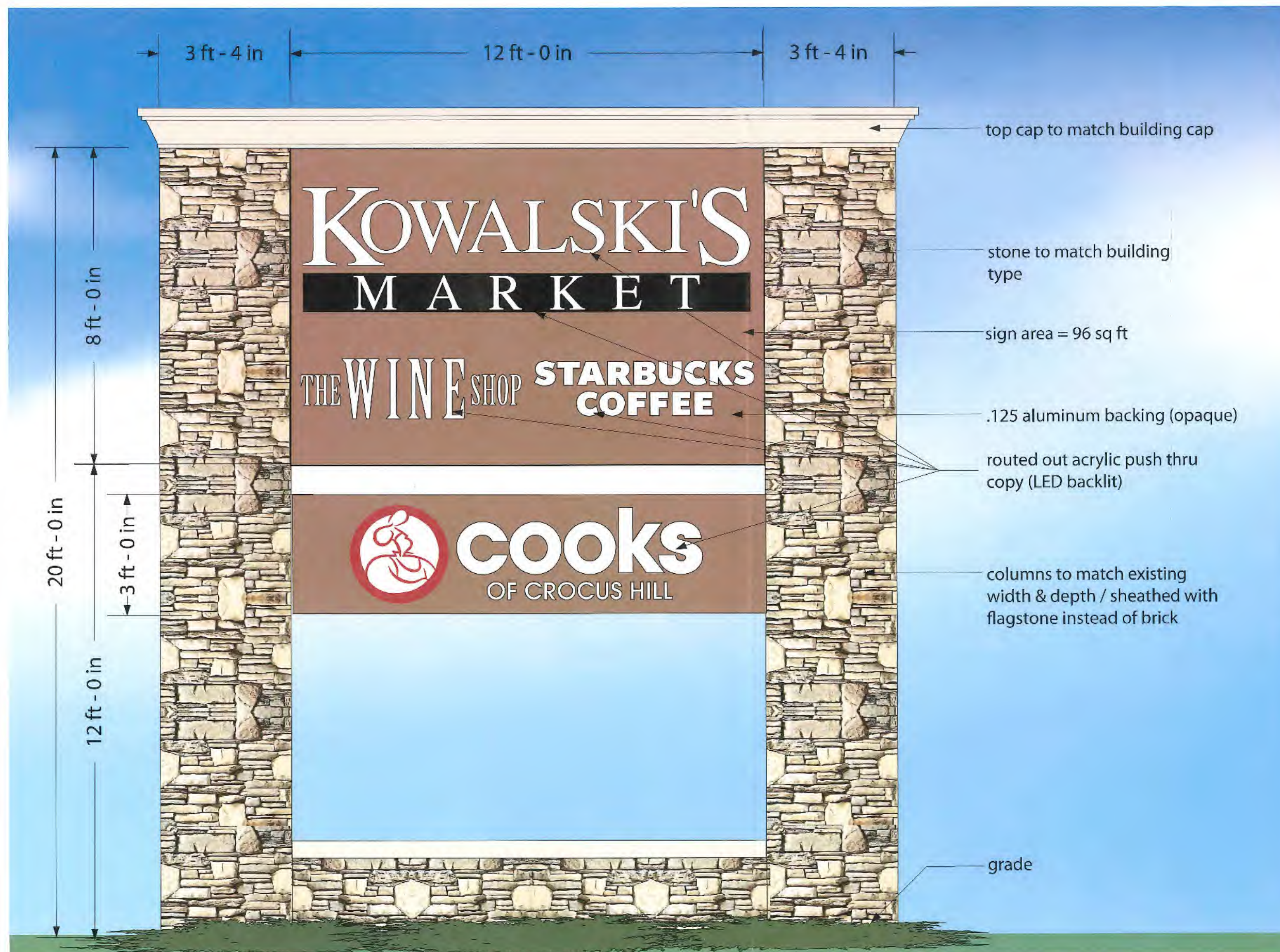
DATE	DATE
PRELIMINARY REVIEW	03/14/18
PRELIMINARY REVIEW	03/14/18
PRELIMINARY REVIEW	03/14/18

REVISIONS: DATE

PROJECT LOCATION:
SHOREVIEW, MINNESOTA

SHEET NUMBER/TITLE:
A7.0

Exterior Elevations



LEROY
SIGNS

6325 WELCOME AVE, N.
MINNEAPOLIS, MN 55429
Phone: 763-535-0080
www.leroy signs.com

DATE: 01/06/16 SCALE: 3/4" = 1'
FILE NAME: CKN_Shoreview_PS_V2C.ai
SALESPERSON: KAJ REITER
CLIENT: KOWALSKI'S MARKET
LOCATION: SHOREVIEW, MN
ARCHITECT:

CONTRACTOR:

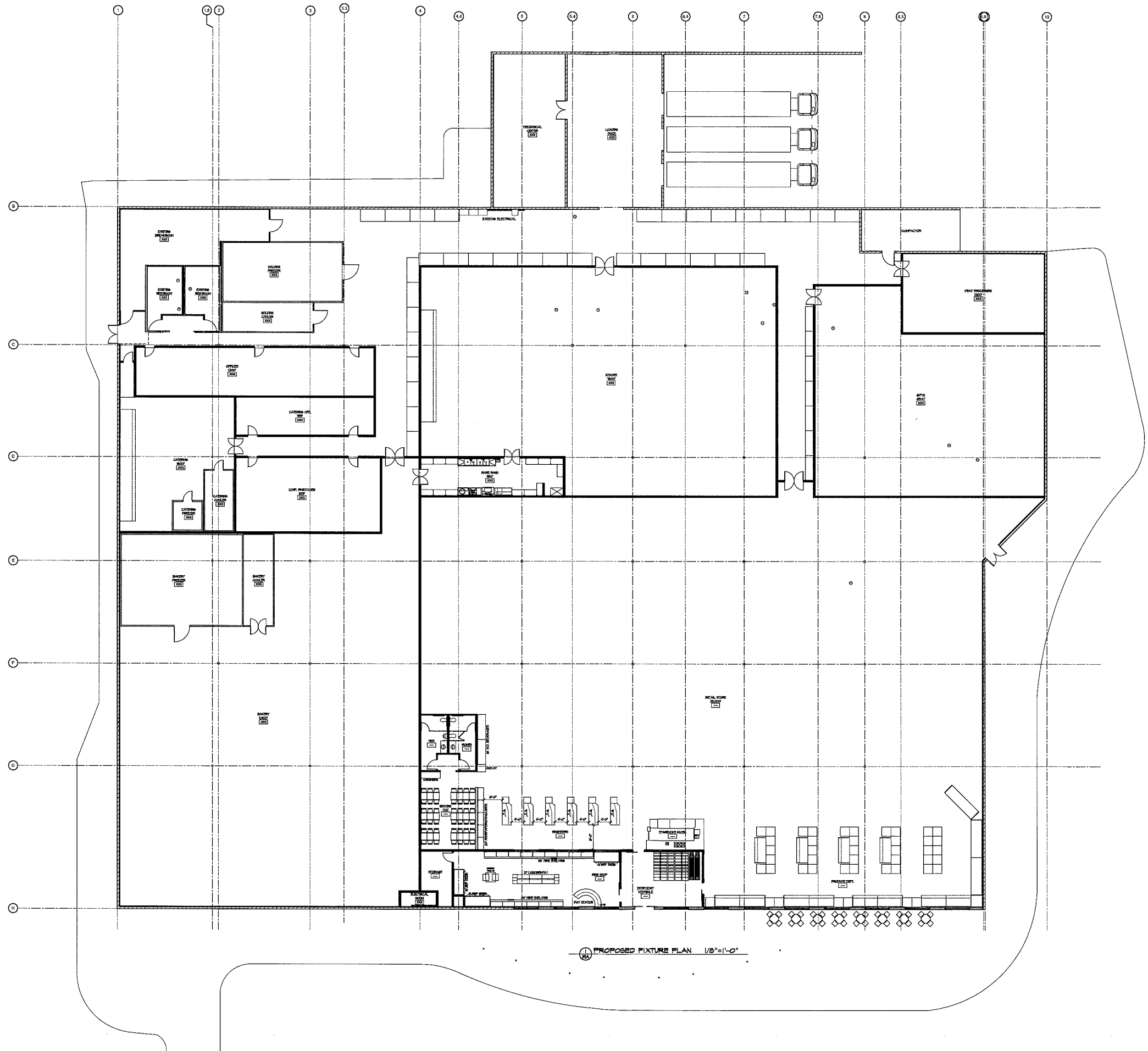
SIGN TYPE / DESCRIPTION:
ILLUMINATED FREESTANDING
SIGNS

GRAPHICS TYPE / DESCRIPTION:
ROUTED OUT PUSH THRU ACRYLIC

NOTES / REVISIONS:
PROPOSED CHANGES TO EXISTING
SIGNS (BOTH LOCATIONS)
HWY 96 & HODGSON RD


IMPORTANT NOTICE:

This is a proprietary design of Leroy Signs, Inc., designed specifically for this project. It is illegal and unethical to distribute to any other entity for copy or use. This design cannot be used without the written consent of Leroy Signs, Inc.



REVISIONS:

FOR REFERENCE ONLY!
NOT FOR CONSTRUCTION!



Robert Gorski
Associates, Inc.
planners • designers • artists

6208 HOGAN WAY, EDEN PRAIRIE, MN 55344
PHONE: (952) 945-0332
EMAIL: rgorski@rnetcom.com

DRAWING NAME:
PROPOSED
FIXTURE
PLAN

PROJECT NAME:
KOWALSKI'S
MARKET

PROJECT LOCATION:
SHOREVIEW, MN


PROJECT NUMBER:
2016-191-00

DESIGNER: RMG

DRAFTER: RMG/JPH

CHECKED BY: RMG

ISSUE DATE:
01-08-2016

SHEET:




Nicole Hill <nhill@shoreviewmn.gov>

Fwd: Kowalski's preliminary plat and PUD

Kathleen Castle <kcastle@shoreviewmn.gov>
To: Nicole Hill <NHill@shoreviewmn.gov>

Wed, Jan 20, 2016 at 1:52 PM

Kathleen Castle
City Planner
City of Shoreview
651-490-4682
kcastle@shoreviewmn.gov

----- Forwarded message -----

From: **Tom Wesolowski** <twesolowski@shoreviewmn.gov>
Date: Wed, Jan 20, 2016 at 12:57 PM
Subject: Kowalski's preliminary plat and PUD
To: Kathleen Castle <kcastle@shoreviewmn.gov>

Kathleen,

I do not have any comments on Kowalski's. Talked with Ramsey-Washington Watershed about the project and they do not think they would need a watershed permit, but they would like to be contacted just in case. Ramsey-Washington has some cost share programs, so if Kowalski's did want to make some stormwater improvements there may be funding.

I will talk with Neva and have her provide any comments she may have.

Let me know if you need anything else.

Thank you, Tom

Tom Wesolowski, P.E. | City Engineer
City of Shoreview
4600 Victoria St. N.
Shoreview, MN 55126
twesolowski@shoreviewmn.gov
Direct Tel: 651-490-4652
Fax: 651-490-4696



LAKE JOHANNA FIRE DEPARTMENT

5545 LEXINGTON AVENUE NORTH • SHOREVIEW, MN 55126

OFFICE (651) 481-7024 • FAX (651) 486-8826

January 11th, 2016

Kowalski's Market
Wilkus Architects, P.A.
11487 Valley View Rd
Eden Prairie, MN 55344
info@wilkusarch.com

File No. 2603-16-02

- All exterior awnings must be in accordance to Minnesota State Fire Code Section 2404

Sincerely,

Nate Berg
Fire Marshal
Lake Johanna Fire Department

MOTION
TO APPROVE PRELIMINARY PLAT, PLANNED UNIT DEVELOPMENT –
AMENDMENT AND COMPREHENSIVE SIGN PLAN FOR
SIDAL REALTY / KOWALSKI COMPANIES, INC– 441 HIGHWAY 96

MOVED BY COMMISSION MEMBER: _____

SECONDED BY COMMISSION MEMBER: _____

To recommend the City Council approve the preliminary plat and amendment to the planned unit development stage applications submitted by Sidal Realty and Kowalski Companies, Inc. for 441 Highway 96. Said approval is subject to the following:

Preliminary Plat

1. The applicant shall execute an agreement between the Lots 1 and 2 addressing the shared infrastructure including access, parking, signage, utilities and maintenance. Said agreements shall be submitted to the City Attorney for review and approval prior to the City's release of the Final Plat.
2. Executed and recorded copies of the required agreements shall be submitted to the City prior to the issuance of a building permit on Lot 2.
3. The Final Plat shall be submitted to the City for approval with the Final Stage PUD application.

Planned Unit Development – Amendment

1. This PUD amendment replaces the previous PUD approval from 1995.
2. A Development Agreement shall be executed and shall include applicable provisions from the previous PUD approvals referenced in Condition No. 1 above as well as any requirements associated with this PUD amendment.
3. Future development of Lot 2 shall require Site and Building Plan Review.
4. Kowalski's agrees to work with the City on refining the landscape plan that addresses better screening for the loading dock area on the northeast side of the property. Said plan shall be submitted with the Final PUD application.
5. The Development Agreement will amend the conditions regarding truck deliveries to correspond to the proposed uses.
6. Prior to submittal of a Final PUD application, Kowalski's shall verify the number of parking stalls provided on the property including the parking located north of the building. These stalls shall be identified on a site plan.
7. The City's prefers that the freestanding signs be shared with the future use of Lot 2.

Comprehensive Sign Plan

1. The signs on the property shall comply with the plans submitted for the Comprehensive Sign Plan application.
2. Signage shall be maintained in accordance with the City's Sign Code.

3. The applicant shall obtain a sign permit prior to the installation of the new signs on the property.

This approval is based on the following findings of fact:

1. The proposed land use is consistent with the designated commercial land use in the Comprehensive Plan and the previous Planned Unit Development.
2. The proposed subdivision complies with the subdivision standards identified in the City's Development Code.
3. The redevelopment/re-use of the property for retail is compatible with the adjoining land uses and will not have a significant adverse impact on surrounding properties.
4. The redevelopment/reuse of the property supports the City's land use and economic development goals.

VOTE:

AYES:

NAYS:

TO: Planning Commission

FROM: Kathleen Castle, City Planner

DATE: January 22, 2016

SUBJECT: Case File 2602-16-01 Comprehensive Plan Amendment, Rezoning, Preliminary Plat, Planned Unit Development – Development Stage, Elevage Development Group, LLC/Elevage Shoreview Holdings, LLC

Introduction

Elevage Development Group, LLC/Elevage Shoreview Holdings, LLC (EDG) has submitted several applications to redevelop the following properties: 157 County Road E, 185 County Road E, 3521 Rice Street and 3500 Rustic Place.

The submitted applications include:

- 1) Comprehensive Plan Amendment: Changing the land use from RL, Low Density Residential and C, Commercial/O, Office to MU, Mixed Use
- 2) Rezoning: Changing the zoning designation from C2, Commercial and R1, Detached Residential to PUD, Planned Unit Development
- 3) Preliminary Plat: Replatting the property from 4 lots to 2 lots
- 4) PUD – Development Stage: Redeveloping the property with a mixed-use consisting of high-density multi-family residential and commercial development

Project Summary

The redevelopment site includes four properties located at the intersection of Rice Street and Country Road E, north of Interstate 694. Existing land uses include a one-story 34,887 square foot shopping center built in 1957, and three small single-family residences that were built in 1939 and 1940. The redevelopment site has approximately 4.2 acres with frontage on Rustic Place, Rice Street and County Road E. The existing commercial and residential land uses would be replaced with a development consisting of a 5-story mixed use building that has 134 market rate apartment units and 6,800 square feet of commercial space on the first floor. This structure is designed as an “L” shaped building located in the southeastern corner of the property oriented towards the Rice Street/County Road E intersection. Access to an off-street parking lot and below grade parking structure is provided from County Road E. Fourteen townhomes are also proposed on the western part of the redevelopment site. Access to the townhomes is also proposed off County Road E.

The proposed development includes amenities for the tenants’ use such as an outdoor pool area, enclosed dog area and walking paths. Please see the attached statement and submitted plans.

Previous City Review: PUD – Concept Stage

The Planning Commission and City Council previously reviewed a Concept Stage – PUD application submitted by the applicant last year. This plan also included commercial and high-density residential land uses but was designed as a horizontal mixed use project rather than the vertical mixed use being proposed. The plan required deviations from the City's code requirements pertaining to building height, building setbacks, parking lot design and setbacks. Planning Commission and City Council members recognized the need for the retail center's redevelopment and generally supported a mixed use residential/commercial development. If the project was to move forward to the Development Stage, members asked that the following items be addressed: mixture of land uses, development intensity/density, traffic, architectural design, building height and site design. The primary concern related to the potential redevelopment's impact on the adjoining residential neighborhood and the applicant's were encouraged to incorporate site and building design features that mitigate these impacts. Minutes from these meetings are attached.

In response to the comments received, the applicant has made revisions to the plan. The plan has changed from a horizontal mixed use to a vertically mixed use project with the commercial uses integrated into the residential apartment building. The apartment building has been shifted to the southeastern portion of the property to create a better presence at the road intersection and reduce impacts on the adjoining residential neighborhood. In addition, townhomes have been included on the western portion of the property and are intended to be a transitional land use from the higher density residential to the lower density residential neighborhood. Site amenities such as an outdoor pool/patio area, walking paths and dog area have also been added.

Prior to the submittal of these applications, the applicant did hold several neighborhood meetings to present the revised plans and receive comments from property owners in the neighborhood.

Staff Review

The submitted applications have been reviewed by staff in accordance with the Comprehensive plan and Development Code. The key issues associated with each application are addressed.

Comprehensive Plan Amendment

Attachment A summarizes the Comprehensive Plan policies related to this site as well as other planning efforts the City has undertaken to address redevelopment near the Interstate 694/Rice Street interchange.

The applicants are requesting an amendment to the Comprehensive Plan changing the existing C, Commercial/O, Office and RL, Low Density Residential land use designations to MU, Mixed Use. Due to the mixed use nature of the development, the relationship of the proposed uses and density of the apartment building, the staff believes a MU, Mixed Use land use designation is appropriate. The MU designation permits a variety of land uses, including vertical mixed residential and commercial land use.

In accordance with Section 203.053 (D), the Planning Commission and the City Council need to consider the following when reviewing a Plan amendment:

- (1) The site and the characteristics of adjoining planned land uses;
- (2) Probable building mass differences;
- (3) Traffic generation;
- (4) Separation to dissimilar land uses;
- (5) Carrying capacity of the site (sewer, water, access, topography, etc.), and
- (6) Buffering potential of dissimilar but adjoining land uses

The City Council may grant or deny the amendment based on 4/5ths majority vote of the Council.

The planned land uses of the adjoining property in Shoreview include RL, Low Density Residential (0-4 units per acre) to the north and west, and C, Commercial to the south. Interstate 694 is also located immediately to the south of the development site. To the east, in the City of Vadnais Heights, land uses include commercial, office and low density residential.

The majority of the development site (157 County Road E) is currently guided for commercial and office land uses. The redevelopment of the property with a commercial or office use could have similar or more significant impacts than a mixed-use development whose primary use is residential. Changing the low-density residential designation of the existing single-family residential properties to mixed-use that will have a greater impact on the nearby residential properties.

In staff's opinion, it is reasonable to change the land use designation of the two residential properties on County Road E due to their orientation, adjacency to the existing commercial center and proximity to the Interstate. Further, staff believes it is also reasonable to change the land use designation for the remaining property on Rice Street due to its adjacency to the commercial center and frontage on Rice Street. Existing language in the Comprehensive Plan anticipates potential land use changes in the Rice Street corridor. The consolidation of these properties provides additional land area where site design elements can be utilized to preserve the integrity of the adjoining low density residential land uses.

The MU land use designation includes both commercial and residential land use and serves as a land use transition and buffer between the Interstate, the Rice Street corridor and nearby commercial land uses. When looking broadly at the City's land development patterns, higher density residential developments are generally found along arterials in the City and adjacent to lower density residential land uses. Residential land uses, regardless of housing type, occupancy or density, are generally considered compatible land uses. While staff believes the proposed land use designation is compatible, site and architectural design strategies should be used to address the review criteria identified above.

Rezoning

The applicant's are requesting the properties be rezoned from C2, Commercial and R1, Detached Residential to PUD, Planned Unit Development.

In accordance with Section 203.052 (C), the Planning Commission and City Council need to consider the following criteria (*italics*) when reviewing a rezoning request. Staff's review also follows each of the individual criteria.

- 1) *That the proposed rezoning is consistent with the policies of the Comprehensive Plan and with the general purpose and intent of the development regulations.*

EDG is seeking a Comprehensive Plan Amendment to change the designated RL, Low Density and C, Commercial/O, Office land use to MU, Mixed Use. The preceding section provides information on the plan amendment.

- 2) *That the development facilitated by the proposed rezoning will not significantly and adversely impact the planned use of the surrounding property.*

In staff's opinion, the rezoning of this property to PUD for the mixed use development will not significantly and adversely impact the planned use of the surrounding property. Significant and adverse impacts are evaluated by reviewing the effect the proposed rezoning and subsequent development will have on the transportation system, stormwater management, municipal sewer and water service and the environment.

The primary concern relates to the impact of the development on the adjoining low density residential neighborhood. The submitted plans indicate that the site can be redeveloped in a manner that will not adversely impact these adjoining residential properties. While the proposal consists of a mixed use project, the primary land use is residential and has less commercial space than currently provided on the site. Staff considers this high-density residential use a transition from the adjoining arterial road network and commercial land uses to the low density residential land use. Architectural and site design features have been incorporated into the redevelopment project to minimize the impact on the nearby single-family residential homes. The relocation of the mixed use building towards the intersection increases the separation of this use, including the commercial, from the adjoining land uses and provides land area for buffering and screening.

Rezoning to the PUD District is also seen as desirable since the City has the ability impose conditions that mitigate impacts and influence the architectural design, site design, and land uses. The majority of the site is currently zoned C2, Commercial, which permits a variety of commercial land uses that could potentially have a greater impact on the adjoining single-family residential neighborhood.

- 3) *The developer is willing to enter into a rezoning/development agreement with the City.*

As a condition of approval, the developer will be required to enter into a development agreement with the City.

Preliminary Plat

The development site includes four tax parcels, three of which are currently used as single-family residential and one which is the commercial property. Elevage has submitted a preliminary plat application that will combine these properties into two parcels. Lot 1 has been designated for the townhome use and Lot 2 for the mixed use building.

The preliminary plat has been reviewed in accordance with the City's subdivision standards (Section 204). The block and lot layout are defined by the different land uses with the townhomes located on one parcel and the mixed use building located on a second parcel. Drainage and utility easements will be provided over stormwater infrastructure and along parcel lines as required by the City Engineer. Municipal utilities are currently present and will serve the redevelopment.

The plat does include dedicated and vacated right-of-way for County Road E. The Final Plat will need to identify the County Road E easement as platted right-of-way.

A public use dedication fee, based on the density, will be required and addressed in the Development Agreement.

Planned Unit Development – Development Stage

Development of this site will be reviewed via the Planned Unit Development process. Planned Unit Development (PUD) process is used to encourage or provide flexibility, creativity, and innovation in the planning and design of development to achieve a variety of objectives related to the Development Code and the City's land use and housing goals.

The PUD Development Stage is the second stage in the PUD process. At this stage, the detailed site development and subdivision plans are presented to the City for public review. A 4/5 majority vote is required for approval by the City Council. The third and Final Stage of the PUD is reviewed by the City Council and provides the City with the opportunity to review the plans for consistency with the Development Stage approval. Development agreements are also drafted and executed at this stage.

Planned Unit Development Review Criteria

The proposed development needs to satisfy certain objectives in order to be approved through the PUD process. Objectives met by this proposal includes; Comprehensive Plan consistency, high-quality building design, enhancement of public infrastructure, innovative stormwater management, sustainable design, housing choice, elimination of a blighted structure, land use compatibility and natural resource preservation.

Land Use and Development Issues

EDG has submitted the development plans for review by the Planning Commission, City Council and public. Code flexibility is essential for this project to move forward, specifically regarding building height, structure setbacks and parking. The project has been reviewed in accordance with the R3, Multiple Dwelling Residential District (Lot 2) and the R2, Attached Residential

District (Lot 1) which will be the underlying zoning districts for the PUD. The underlying zoning for the commercial component will be C1, Retail Service for the commercial uses.

Placement of Structures

The applicants have made significant revisions to the plans since the conceptual plans were presented to the Planning Commission. The mixed use structure was shifted to the southeast and oriented toward the Rice Street/County Road E intersection. The intent is to minimize the impact of this use on the adjoining residential neighborhood and enhance the visibility of the commercial activity to the transportation network. Townhomes are proposed on the western portion of the property and intended to provide a transition from the higher density residential use to the lower density single-family residential neighborhood. With this new layout, green space is provided along the northern boundary of the property and serves as a buffer. These revisions do address some of the concerns previously expressed in the Concept Stage PUD review.

Building Height and Setbacks

Mixed Use Building – Lot 2

The mixed use structure is designed as flat roofed 5 story building that has a height of 55' and exceeds the maximum 35-feet permitted in the R3 district (Section 205.084 (C3)). This height, however, can be exceeded provided: 1) It does not exceed the firefighting capabilities of the Fire Department (Section 206.040 (A)) and 2) An additional 1-foot of setback is provided for every additional foot in height over 35'(Section 205.084 (C3)).

Lake Johanna Fire Department has reviewed the proposed concept and indicated that the proposed height is not a concern as the Department has trained staff and the equipment needed to respond to a fire in a taller building. The building is also required to have a fire suppression system. Comments from Lake Johanna Fire Department are attached.

The placement of the structure complies with the required setbacks from the adjoining single-family residential uses to the north. Flexibility from the required minimum building setbacks is needed from property lines along Rice Street and County Road E and the western property line adjacent to the proposed townhomes. The following table compares the required and proposed setbacks and identifies the deviations being requested.

	R3 Code Requirement	Proposed Setback
Front property line - Rice Street	60'	41'*
Side property line County Road E	50'	32'*
North (adjacent to single-family residential)	50'	78.2'
Rear property line - West	50'	14'*

**Deviation Required*

Townhomes – Lot 1

The townhomes are located in two structures each with 7 units. They are designed as two-story units with a tuck under garage that is accessed from the rear of the building. The height of the townhomes when measured to the midpoint of the roof is 31 feet and 37 feet when measured to the peak. The height complies with the maximum 35-feet permitted.

A deviation from the setback requirement adjacent to Rustic Place is needed. The structures have been located closer to County Road E in an effort to increase the setback from the adjoining single-family residential land uses and minimize impacts. The following table summarizes the required and proposed setbacks:

	R2 Code Requirement	Proposed Setback
Front property line County Road E	30'	32.4'
Side property line Rustic Place East	30' 10'	25.3'* 23''
Rear property line North (adjacent to single-family residential)	-	54.3'

**Deviation Required*

Visual Impact

The applicant has modified the site plan and structure design to address the concerns raised regarding the visual impact the proposed mixed use structure will have on the adjoining single-family residential properties. A flat roof design was chosen to lower the profile and height of the 5-story building. The structure setback from the adjoining single-family residential uses were also increased. The northern portion of the development site is reserved as green space and provides the needed land area for a landscape buffer.

A shadow study was conducted and depicts the structures' shadow on the adjoining residential properties at different times throughout the year. Shadow appears to have an effect in December when the sun is lowest on the horizon. A reduction in building height or different building configuration could reduce this impact. The applicants have tried to minimize this effect by placing the building in the southeast corner of the property and exceeding the minimum setback required from the north property line.

Residential Density

In the MU land use designation, a density up to 45 units per acre is permitted. Density is calculated by using the gross site area of the property. The overall density of the development is 33.6 units per acre.

One issue that has been raised relates to the impact higher density developments can have on lower-density residential neighborhoods. The proposed location is on the edge of a lower density neighborhood but is also adjacent to an arterial roadway, Interstate 694 and commercial land uses. This type of development pattern is not unique and other examples include Southview

Senior Living, The Shores, Summerhouse and Applewood Pointe. Although the densities of the two residential land uses have a large variation, these densities can co-exist provided site and building design strategies are used to minimize and mitigate impacts on the lower density residential neighborhood.

While the number of units has increased, concerns regarding the intensity of development have been addressed by relocating the structure, utilizing below grade parking, increasing green or open space and providing site amenities for future residents.

Traffic Impacts

Currently, access to the development site is gained from driveways off Rice Street and County Road E. With the proposed redevelopment, two access driveways will be located off of County Road E with one accessing the parking lot for the mixed use building and the other accessing the driveway for the townhome units.

A traffic impact study was prepared comparing forecasted traffic levels generated by the site's redevelopment with a no-build condition. The study acknowledges that congestion does occur at the Rice Street/County Road E intersection during the morning peak rush hour. Redevelopment of the property will have a negligible traffic impact on the road network. The level of service for this area will remain relatively unchanged from the conditions presently experienced.

Ramsey County transportation staff has reviewed the traffic study and found that it has been completed in accordance with industry standards. In an earlier version of the plan, the developer had proposed all access to the apartment and retail uses access solely from Rice Street in response to neighborhood concerns. This was rejected by the County, and the plan now shown proposes all traffic associated with the project be directed to the signalized intersection of County Road E and Rice Street.

The existing traffic congestion occurring on Rice Street is primarily attributed to the operational deficiencies of the I694/Rice Street interchange. The reconstruction of this interchange is a top priority and the County is working on a legislative funding package for the needed interchange improvements. The cities of Little Canada, Shoreview, and Vadnais Heights, along with Ramsey County have funded the preliminary design work for the new Rice Street bridge. If funding is received, the improvements may be completed in 2018.

Parking

Mixed Use Building – Lot 2

The City's Development Code requires a minimum 2.5 stalls per unit in the R3 zoning district with one stall fully enclosed (Section 206.020 (B1g)) and 5.5 stalls per 1,000 square feet of net floor area in the C1 district (Section 206.020 (B1a)). Restaurants require 1 stall per 3 seats based on the maximum design capacity of the building (Section 206.020 (B2f)).

Off-street parking for the mixed use building is provided in an underground parking structure and surface parking lot. The surface parking area is designed with 77 stalls and shared between the residential and commercial uses. One-hundred sixty-eight stalls are provided in the underground parking structure increasing the total number of stalls provided on-site to 235. In

accordance with the Development Code, a minimum of 365 stalls are required. The applicant has indicated that the number of stalls provided is sufficient to meet the demand due to the unit mix and target demographic.

The Development Code does provide some flexibility with respect to parking standards. The number of parking stalls constructed may be reduced to a number less than the minimum provided parking management techniques are used. Techniques that may be considered include proximity to transit (transit is provided on Rice Street), shared parking and proof of parking (Section 206.020 (C)).

The proposed parking ratio is 1.7 stalls per unit and is consistent with what was previously approved with the Lakeview Terrace development. Staff does have some concerns regarding the deviation since this project does not have the opportunity to share parking with adjoining land uses and the development site does not have proof of parking. While the staff believes that some level of flexibility may be reasonable due to the demographic market and unit mix, additional information is needed to support the applicant's statement that the proposed parking will meet the demands of the facility.

Landscaping is also required to screen the parking areas, provide visual relief and shade (Section 206.060 (A)). The setback of the parking lot from the northern property line adjacent to the single family residential use is 31'7" exceeding the minimum 20-foot setback required. Landscaping is provided along the northern property line to screen the parking lot from view of the neighboring properties.

Townhomes – Lot 1

Access to the townhomes is also gained via a driveway off of County Road E. The proposed driveway location exceeds the minimum separation required from an intersection (Rustic Place/County Road E). Each of the townhome units has a two-car tuck under garage and driveway area that can accommodate two vehicles.

Grading

The topography of the property will be modified to accommodate the proposed development. The grade elevation will be raised slightly for the mixed-use building and parking lot. The existing shopping center has a floor elevation of 927.8' which is slightly lower than the proposed 929' first floor elevation of the mixed use building.

The western portion of the property has a lower topographical elevation of the existing shopping center site. The grading plan identifies that the garage floor elevation for the townhomes will be set at 924 which is similar to the elevation of the existing homes. This grade elevation will then be raised to accommodate and access the main floor of the townhome. This increase in grade elevation will have a visual impact on the adjoining properties to the west and north. While landscaping is proposed to mitigate this impact, additional measures should be explored such as berming or fencing.

Stormwater Management

In accordance with Section 205.084 (C4), the maximum impervious surface coverage permitted is 70% provided best management practices are used. The existing impervious surface coverage of 55.1% will increase to 57% with the proposed development. The impervious surface coverage for Lot 1 is 63.5% and for Lot 2 is 56.1%.

The property is located in the Ramsey Washington Metro Watershed District. The District has the permitting authority for stormwater management. The stormwater management plan will need to comply with the District guidelines for rate and quality control.

The stormwater management plan has been reviewed by the City Engineer. Stormwater will be managed through the use of underground infiltration chambers that will manage the rate of runoff and water quality. Stormwater runoff from the site will be directed to these chambers through a series of catch basins and pipes before it is discharged into the City's storm sewer system. Modifications to the plan may be required by the Watershed District since the development site is within the St. Paul Water Utility Drinking Water Supply Management Area.

Vegetation, Woodlands and Landscaping

The majority of tree cover is located on the three existing residential properties while there is little vegetation on the commercial property. Redevelopment of the property will result in the removal of 52 landmark trees and the preservation of 7 landmark trees. The number of replacement trees required is 317.

The proposed landscape plan identifies the installation of 116 trees and is less than the 371 trees required. While additional plant materials are proposed as part of the landscape plan, they do not meet the tree replacement requirements. In the event replacement trees cannot be provided on the development site, a financial contribution is required to the City's forestry fund.

The landscape plan identifies trees planted along the roadways enhancing the streetscape along County Road E and Rice Street. Landscaping is also proposed along the north property boundary to buffer the development from the adjoining single-family residential homes. Plant materials along the boundary include evergreens and deciduous shrubs.

Public Comment and Agency Review

Property owners in the Rustic Place neighborhood and those on Rice Street in the City of Vadnais Heights were notified of the redevelopment proposal and the public hearing. A development notification sign was also placed on the property.

The City has received comments from a number of property owners expressing opposition and concerns regarding the proposed development. These concerns generally relate to land use compatibility with the adjoining single-family residential neighborhood, density, public safety, traffic, visual impact, architectural design/scale, and environmental impacts. These comments are attached.

Joe Lux, Ramsey County Public Works Department, provided comments regarding impacts on the transportation network. The County has reviewed the traffic study and concurs the Study's finding that traffic generated by the proposed redevelopment will have a negligible impact on the transportation network. Current roadway deficiencies are attributed to the Rice Street/Interstate 694 interchange.

The Minnesota Department of Transportation also provided comments regarding the proposed plat and permitting requirements.

The Lake Johanna Fire Marshall also provided comments regarding Fire Code requirements and access.

Comments from the City Engineer are also attached. These comments address a variety of items such as utilities, stormwater management, trail connections and tree preservation.

Recommendation

The submitted plans were reviewed in accordance with the City's development standards and previous direction received from the Commission and Council. The staff believes the proposed development is consistent with Shoreview's land use and housing policies and meets the criteria for the Comprehensive Plan Amendment, Rezoning and PUD. This mixed-use residential and commercial development should serve as a transitional land use between the arterial transportation network, commercial land uses and the adjacent single-family residences. Impacts of the proposed development are mitigated through the site design including building placement, green space and landscaping. This mixed use product will also support community needs by diversifying the rental housing options available in the community and providing additional commercial development.

The Staff is recommending the Planning Commission forward a recommendation of approval to the City Council with the following conditions attached.

Comprehensive Plan Amendment

1. The amendment changes the land use designation from C, Commercial/O, Office and RL, Low Density Residential to MU, Mixed Use.
2. Review and approval of the amendment by the Metropolitan Council.
3. The amendment will not be effective until the City grants approval of the Final Plat and PUD - Final Stage requests and the development agreements are executed.

Rezoning

1. This approval rezones the property from C2, General Commercial and R1, Detached Residential to PUD, Planned Unit Development.
2. The underlying zoning district for this PUD is: Lot 2 – R2, Attached Residential, Lot 3- R3, Multi-Dwelling Residential for the apartment units and C1, Retail Service for the commercial uses.
3. Rezoning is not effective until approvals are received for the Final Plat, PUD - Final Stage and development agreements executed.

Preliminary Plat

1. A public use dedication fee shall be submitted as required by ordinance prior to release of the final plat by the City.
2. The final plat shall include drainage and utility easements along the property lines. Drainage and utility easements along the roadways shall be 10' wide and along the side lot lines these easements shall be 5' wide. Other easements shall be dedicated as required by the Public Works Director.
3. Private agreements shall be secured between the parcels in the subdivision regarding the maintenance of shared facilities. Said agreements shall be submitted to the City Attorney for review and approval prior to the City's release of the Final Plat.
4. Comments received from the State of Minnesota and Ramsey County shall be addressed in the Final Plat submittal.
5. The Final Plat shall be submitted to the City for approval with the Final Stage PUD application.

Planned Unit Development – Development Stage

1. This approval permits the redevelopment of 157 County Road E, 185 County Road E, 3521 Rice Street and 3500 Rustic Place with a mixed use development consisting of a 5-story building that has 134 market rate apartment units and 6,800 square feet of commercial space on the first floor. Fourteen townhomes are also planned.
2. Access to the site shall be provided via the driveways off County Road E as indicated in the approved plans.
3. The items addressed in the City Engineer's memo dated January 20th shall be addressed in the Final PUD submittal.
4. Approval of the final grading, drainage, utility, and erosion control plans by the Public Works Director is required, prior to submittal to the City of applications for Final Plat and PUD – Final Stage. Final plans shall identify site construction limits and the treatment of work (i.e. driveways, parking areas, grading, etc.) at the periphery of these construction limits.
5. The applicant shall secure a permit from the Ramsey Washington Metro Watershed District prior to commencing any grading on the property.
6. The proposed mixed use structure shall be of a 5-story design as depicted on the plans submitted with this application. Said building shall include the architectural enhancements and high-quality building materials as identified. The structure shall not exceed the 55-foot height as identified in this report and on the submitted plans.
7. The applicant shall review options to enhance landscape screening along the north property line such as berming or fencing.
8. A financial contribution to the City's Forestry fund is required since the number of required tree replacements cannot be accommodated on the development site.
9. The applicant shall provide additional information pertaining to the parking supply and demand prior to the City Council's consideration of the Development Stage application.
10. The applicant is required to enter into a Site Development Agreement and Erosion Control Agreement with the City. Said agreements shall be executed prior to the issuance of any permits for this project. The Development Agreement shall address:

- a. Construction management and nuisances that may occur during the construction process, including parking for contractors. No parking is permitted on Victoria Street.
 - b. Best Management Practices for Water Quality improvement
 - c. Landscape maintenance
 - d. Maintenance of stormwater management facilities
11. This approval shall expire after two months if the Planned Unit Development - Final Stage application has not been submitted for City review and approval, as per Section 203.060 (C)(6).

Attachments

1. Attachment A
2. Planning Commission Minutes – July 28, 2015
3. City Council Minutes – August 17, 2016
4. Memo dated July 20, 2015 Mark Maloney, Public Works Director and Tom Wesolowski, City Engineer
5. Location Map
6. Pictometry photos
7. Zoning Map/Planned Land Use Map
8. Applicant's Statement and Submitted Plans
9. Correspondence from Joe Lux, Ramsey County Public Works – November 10, 2015, January 6, 2016 and January 11, 2016
10. Letter - Nate Berg, Fire Marshall, LJFD
11. Letter– Karen Scheffing, MNDOT
12. Public Comment
13. Motion

ATTACHMENT A

COMPREHENSIVE PLAN/PLANNING EFFORTS

The Comprehensive Plan (weblink - <http://www.shoreviewmn.gov/government/comprehensive-plan-test>) designates the majority of this property as C, Commercial and the existing residential parcels as RL, Low Density Residential.

The planned land use of this property (Chapter 4) is C, Commercial and this property is also included in Policy Development Area #18, Rice Street Crossings and Vicinity. Policy Development Areas (PDA's) present opportunities or pose significant concerns for development or redevelopment. Through the use of the PDA concept, the City has established more specific land use policies that address the individual circumstances found within the PDA. In accordance with the PDA #18, the commercial parcel has a land use designation of Commercial and O, Office. The C designation recognizes and allows for commercial uses while the O designation, suggests this area may also be suitable for office use due to the proximity to the highway and adjoining residential neighborhood. The Plan does recognize that the large residential lots adjacent to this should retain the RL, Low Density Residential Designation until a redevelopment application is received. The application includes three residential properties that are adjacent to the shopping center. The submitted application is requesting an amendment to change the land use of these property to MU, Mixed Use.

Chapter 6, Economic Development, also identifies the commercial property as a Targeted Redevelopment Area, TRA #2. One of the key issues identified relates to the appropriate land use for this property because it adjoins a low density residential neighborhood.

Chapter 7, Housing, include goals that address redevelopment and housing. Since there are limited opportunities for development, the plan recognizes that redevelopment is vital to meet the life-cycle housing needs of the community and diversify the housing supply. The Plan also recognizes that existing conditions and neighborhood character need to be considered when reviewing infill and redevelopment proposals.

Other Planning Efforts

Since the Rice Street/I694 interchange area was identified as both a PDA and TRA, this area was included in the Highway Corridor Transition Study which was recently completed. The recommendations of this study include updating the PDA and expanding potential uses to include high density residential as well as buffer requirements for the adjacent single-family neighborhood. In addition, the creation of a Mixed Use district was recommended to allow a mixture of uses within a development project including commercial, office, high-density residential and live-work units.

The Economic Development Authority also included the Rice Street/I694 interchange area in their 2015/2016 workplan. This plan calls for continued efforts to address the transportation

infrastructure issues for the interchange area as well as looking at options for the redevelopment of the commercial center.

Shoreview's Housing Action Plan recognizes that new rental housing opportunities are needed to expand housing choice and attract younger households (weblink - <http://www.shoreviewmn.gov/home/showdocument?id=1294>).

MOTION: by Commissioner Schumer, seconded by Commissioner Doan to move item 4E on the agenda to be the first item of consideration.

VOTE: Ayes - 5 Nays - 0

PLANNED UNIT DEVELOPMENT-CONCEPT REVIEW

FILE NO: 2584-15-27
APPLICANT: ELEVAGE DEVELOPMENT GROUP, LLC
LOCATION: 155-173 WEST COUNTY ROAD E, 185 WEST COUNTY ROAD E,
3500 RUSTIC PLACE AND 3521 RICE STREET

Presentation by City Planner Kathleen Castle

This application proposes redevelopment of the subject properties located at I-694 and Rice Street. A Concept Plan with mixed use residential and commercial is being presented. The properties combined would consist of approximately 4.14 acres with frontage on Rice Street, Rustic Place and County Road E. Currently, the property has three single-family homes, zoned R1, Detached Residential; and a retail center of 34,887 square feet, zoned C2, Commercial. Surrounding land uses include low density residential to the north and west, commercial to the east and south, and the City of Vadnais Heights to the east. The property zoned Commercial allows restaurants, gas stations, banks, liquor stores, hardware stores and general retail. These uses could be proposed with only a Site and Building Plan Review. A proposal that complies with City standards would mean granting approval of the Site and Building Plan Review.

The mixed use development proposed consists of a 100-unit high density market rate apartment building on the western portion of the site. The apartment building would be L-shaped with the north/south leg consisting of 5 stories and the east/west leg consisting of 4 stories. The building would be of a sustainable design with parking provided in a surface lot as well as below grade. Commercial uses are planned on the east portion of the site. This includes a retail building of 1,858 square feet and a restaurant of 6,576 square feet.

The applicant is requesting Planned Unit Development (PUD) zoning to allow some flexibility from the Development Code. The building height maximum is 35 feet, which may be exceeded under a PUD with increased structure setbacks. The minimum setback is 30 feet from all property lines. Building setbacks for commercial buildings is a minimum of 50 feet when located adjacent to a residential land use. A parking lot adjacent to a residential land use is required to have a 20-foot setback.

The residential density is proposed at approximately 41 units per acre, less than the allowed 45 units per acre with the Mixed Use land designation. Design strategies will be used minimize impacts to the low-density residential neighborhood to the north. Access is proposed off Rice Street and County Road E. Both Ramsey County and Mn/DOT require a traffic impact study for review at the Development Stage Review.

Surface parking includes 162 stalls. There will be underground parking for the apartment building. Additional information is needed to determine whether the proposed parking is in compliance. A minimum of 2.5 stalls per unit is required for apartment buildings. A minimum 20-foot setback is required from the street and residential lot lines.

The Concept Stage is the first of a three-stage process in a PUD. The purpose is to determine the appropriateness of the development and land use compatibility. Public input is taken to identify concerns to address at the Development Stage PUD review.

This property is located in the Policy Development Area (PDA) No. 18 of the City's Comprehensive Plan. The commercial site is designated for commercial and office uses. The low density residential zoning for lots immediately to the north is to remain in place until a redevelopment application is received. A Comprehensive Plan Amendment will be needed to change the residential and commercial land use to Mixed Use. This site is also identified as a Targeted Redevelopment Area (TRA) No. 2. TRAs focus on underutilized business and commercial properties in the City.

The Highway Corridors Transition Study recently completed discusses expanding uses to include high density residential and mixed use for areas north of I-694. The 2015/2016 Work Plan of the Economic Development Authority (EDA) focuses on options for redevelopment and continued efforts to address infrastructure for the I-694/Rice Street interchange. Mixed use with high density residential may be compatible with the adjacent low density residential use if design strategies, such as setbacks, height and buffers, minimize impacts to nearby residential properties.

Staff finds that the location of proposed uses makes sense for the site characteristics, proximity to the transportation corridor and adjacent low-density residential neighborhood. The Lake Johanna Fire Department has no concerns regarding the apartment building height. The main concern is visual impact to the single-family residential to the north.

Notices were sent to property owners within 350 feet of the site. Comments oppose the proposal for the following reasons: land use compatibility, density, public safety, traffic, visual impact, architectural design and scale; and environmental impacts. The developer held a neighborhood meeting. Unfortunately, only one resident attended.

Michael Mergens, Elevage Development Group, 4470 W. 78th Street Circle, Bloomington, MN, stated that site is a premium corner, and his firm is committed to a premium development for this gateway development to Shoreview. They are working hard to meet goals of the City with what they have learned from experts and develop a project that will fit in with the neighborhood. The east/west leg of the apartment is shown on the north property line because it was thought it would provide a nice break from parking and retail. However, it would be just as easy to locate it on the south boundary and push the parking closer to the adjacent neighborhood. The restaurant is planned to be high end. It will not be a chain or a bar. It is his belief that the building is less of an impact than parking, but the building can be pushed further south. A berm will be added along the north property line with trees to increase privacy for adjacent residents. This site is in need of redevelopment. With the anticipated new interchange and bridge, this is a

good site for mixed use. This was also the conclusion of the City's Highway Corridor Transition Study. Rather than focusing on one commercial property, his firm wanted to build something better with the combination of multiple properties. In considering design alternatives, plans are to make the apartment building special. The fifth floor will be floor-to-ceiling glass with premium units for corporate use that are fully furnished. Just the infrastructure for floor-to-ceiling glass will be \$1.8 million. There is an atrium and a sustainable green roof. Rain storage capture will be stored underground.

Regarding traffic flow, he plans to work with Ramsey County. There are currently two access points from Rice Street. One is being eliminated. It is his belief that a right-in/right-out access could work.

Commissioner McCool asked when notices were sent for the neighborhood meeting and if the PDA includes the residential properties. Ms. Castle stated she believes notices were mailed approximately six days before the meeting took place. It was held in the neighborhood shopping center. The PDA is only for the commercially zoned site. Commissioner McCool asked if the restaurant is still viable, if Ramsey County does not allow access off Rice Street. Further, he asked the setback from the property line to the north. Mr. Mergens responded that a more complete traffic study will be needed to make the internal flow work. The restaurant is destination oriented, not a convenience. That means customers will use the access available to reach this restaurant. His preference would be an in-and-out access with two-way traffic. He estimated a 30- to 40-foot setback. Commissioner McCool asked if there will be apartments both sides. Mr. Mergens answered, yes.

Commissioner Doan asked if there are planned improvements to the interchange at I-694 and Rice Street. Ms. Castle explained that the traffic impact study will be completed by the developer and submitted to Mn/DOT, Ramsey County and the City for review. There will be improvements to the interchange and to the bridge. The interchange has been scheduled, but the bridge has not yet been designed or scheduled. Commissioner Doan noted a letter from Ramsey County stating that the existing access off Rice Street must be closed. Access can only be off County Road E. Ms. Castle stated that the developer is aware of the County comments. The developer will be working with the County to find out options.

Commissioner Doan asked for more background information on Elevage. Mr. Mergens stated that he is a Land Use Attorney. He has worked with prestigious land use attorneys, such as Bob Hoffman who developed the Mall of America. He has two partners who have experience in retail development. Elevage owns a development in Blaine that has a strip center and plans to develop an outlot.

Chair Solomonson asked how the density for the apartment building is calculated, as that portion of the proposal is 2.4 acres and there was discussion about shared parking. Ms. Castle responded that the gross site area is used to calculate density and this site area was estimated. The surface parking will be designed for the uses to share that parking, whether apartments, restaurant or retail. Chair Solomonson asked the distance to the adjacent north residential property. Ms. Castle answered, 30 to 40 feet.

Commissioner Ferrington asked the size of buildable area with the setback requirements. Ms. Castle stated that as there is no proposed building height, the setbacks are not known.

Commissioner McCool asked the height and setback of the berm. **Mr. Mergens** estimated the setback to be 30 to 40 feet. Commissioner McCool asked

Commissioner Ferrington asked how the new Rice Street bridge will impact the project. **Mr. Mergens** stated that the new bridge will ease traffic, but he does not want to hold up redevelopment because this is a premium corner.

Chair Solomonson stated that the building is contoured on the south end and the entrance to the garage is shown to the north. He asked how that would impact the entrance to the garage. **Mr. Mergens** responded that it depends on what can be worked out with Ramsey County. At this time, he believes the best development would be three stand-alone buildings. If there is interest in more retail, that could be achieved. If the development changes to a strip center, then it would not be possible bring in a restaurant.

Chair Solomonson opened the discussion to public comment.

Ms. Marcia Figus, 3538 Rustic Place, stated the notice for the neighborhood meeting arrived two days before the meeting. The notice was unsigned, and she did not attend. This area is a triangular island surrounded by Vadnais Lake, Grass Lake and I-694. There have been many issues in this neighborhood. Introducing 100 adults, teens and children into this small neighborhood will mean that children will have no outlet but the yards, streets and railroad space. Security would be compromised by the sheer number of people. The property owners at 3520 Rustic Place will not see the sun from October to March, but the renters will have a direct view of all space. It is a reasonable expectation to be safe in one's home and yard. This invasion would take away her security in her home. The sheer number of new residents would make her vulnerable. Residents in the apartment building would be able to see her living which would be very uncomfortable to the point of mental anguish. Many in the neighborhood are seniors who have established equity. Planning policy is to not change the character of neighborhoods. She gave examples of decisions where added screening and landscaping were required to preserve the character of a neighborhood. The proposed development does not fit a neighborhood bordered by Vadnais Lake, Grass Lake and I-694. Trees do not grow on the north side of a 5-story building. The development is all asphalt and structure. Traffic will be significantly impacted with an apartment building, restaurant and drive-through coffee building. Congestion already exists at the intersection of Rice Street and County Road E. This is not the right concept plan for this site.

Ms. Kristi Tomas, 3584 Rustic Place, stated that neighbors are not against development but want to be sure it is a development that does not kill the neighborhood. This is an encapsulated neighborhood where residents know each other. There are no sidewalks leading in or out of the neighborhood and no access to a park. Residents use the streets for walking, children playing, biking, gathering. The community cares about its homes and residents. There are two group homes and two developmentally challenged children in the neighborhood. People have been hit by cars turning onto Rustic Place because the pedestrian traffic is not visible. Children in a

proposed apartment building will only have the parking lot, street or railroad property as choices for outdoor play. This development would kill the community and all that has been done by the residents to make it safe.

Mr. Mark Kapszak, 3628 Rustic Place, stated that he is a police officer. Currently, there are approximately 50 houses on Rustic Place and St. Marie. At an average of two cars per household, there are 100 cars. At 2.5 parking stalls per apartment unit, that would be an additional 250 cars to the neighborhood. That increase is dangerous for children. Because of the congestion, cars cut through the neighborhood during rush hour. Residents have had to step out into the middle of the street to slow cars down. Adding 250 cars will only add to the danger. The parked cars in an open parking lot along I-694 will become an open invitation for criminals. The neighborhood is dark at night and would be a prime target for criminals who don't find what they want in the parking lot. This development will drain resources of Shoreview. An apartment building on Lexington, Shoreview Hills, had 278 emergency calls in one year. Midland Terrace had 181 emergency calls in one year. In all of Shoreview, there were 8,883 emergency calls. Those two apartment buildings account for 5% of emergency calls in one year. In his neighborhood, there is one call in an entire year. Neighbors can look out at a car passing and know who it is. The increased calls to the Sheriff's Department will be a drain on resources. The buffer berm with trees will not work. Trees will not grow because on the north there will be no sun. If the trees do grow, they will be good hiding for criminals coming into the neighborhood.

Mr. Curt Levitt, 3636 Rustic Place, referred to an article in the *Star Tribune*, July 21, 2015, by Jeremy Olson. A copy of the article was distributed to each Commissioner. The article talks about Minnesota being rated at the top nationally for children's well being. The article specifically reports the danger to children who do not have safe access to a park, schools, healthy food and health care. Mr. Levitt expressed his thoughts regarding the article. There may be deaths as a result of this new apartment building. The apartment would not have safe access to the Owasso Beach or Owasso Beach Park, where children will surely go. The route to Owasso Park will be one of the busiest streets in the County with the entrances and exits of County Road E, Vadnais Boulevard, a gas station, liquor store, Taco Bell. There is also a gas station on the east side of Rice Street with the on and off ramps to I-694 East and West, Caribou Coffee and a bar. Any teen from the apartment building on a bike going to the park will contend with all this traffic. They will have to finally cross the Soo Line Railroad. These types of conditions lead to deaths of children on bikes.

Mr. Nathan Anderson, 3565 Rustic Place, stated that he is a licensed building and remodeling contractor, specializing in historical restoration and problematic insurance claims. This proposal violates height, density, essential characteristic, reasonable use, privacy, public safety and setbacks. His calculations do not find anything imaginable below 75 people per acre. If there were to be maximum usage of space, there could be as many as 150 or 175 people in this proposal. The proposal is misleading with setbacks. The bottom corner key states one inch equals 20 feet. There is approximately one-half inch between the apartment building and the west property line, which may be 10 to 15 feet. That is not close to the 30 or 40 feet stated. Add one foot for each inch of added height and the proposal will not fit the site. It would be shortsighted to label the opposition heard at this meeting as a "Not In My Back Yard" issue. It

is about adherence to written regulations development laws and planning that the City has in great detail. It is about the Planning Commission and City Council not allowing this proposed illegal non-compliant plan to move forward. This proposal is initiated inquiry with private attorneys and multiple legal resources. This proposal's level of non-compliance is so overt that approval will create suspicion of official misconduct. If necessary, the neighborhood is prepared to file a complaint. Interrogatories and depositions of Commissioners and Councilmembers will seek to find out if anyone was unduly influenced to approve this proposal. Anyone who is an unwitting participant in the undermining of due process should withdraw from this decision to avoid any semblance of bias or cast a nay vote. Evidence to work around the codes and ordinance shall be met with extreme scrutiny and consequences. Having reviewed City Code, the only appropriate decision is to deny this proposal. The only recommendation should be to study the impact of a PUD on the neighborhood and provide accurate, unbiased data to further deliberate and draft an amendment to the Comprehensive Guide Plan. On behalf of the people of Shoreview and the Cardigan neighborhood, Vadnais Heights and citizens of Shoreview, a moratorium is requested on all noncompliant proposals until a study is conducted by an independent third party of acceptance by the City and its residents. This study should be conducted in a variety of modalities to include a person to person, door to door canvass of the neighborhood, which include an overview of acceptable density. Additionally, this study should include architectural design requirements that address a comfortable transition to the existing neighborhood. All development documentation and study needs to address and adhere to the City of Shoreview's laws, ordinance and Comprehensive Guide Plan. This is not an opposition to development but that development occur with the proper legal consideration that this PUD lacks. Misguided or misinformed approval of this development will bring an uproar of neighborhood response. The neighborhood is prepared to hold the Planning Commission, City Council and any regulatory committee accountable to the laws set forth. Please do not allow your decisions to tarnish, blemish or compromise the high standards that our officials and residents have instilled in our community.

Ms. Wendy Olson, 2094 Cedar Avenue, White Bear Lake, stated that she has been a customer of Pet Junction of many years. It is a training center pets for people all over the state. There are clubs that use the venue. Her concern is that a new building would be difficult for this business to afford and remain. It may put them out of business. That would be a big loss for the area and this community.

Ms. Hilary Fox, 181 St. Marie Street, stated that she echoes the comments of her neighbors. Further, she encouraged a thorough cross examination of Mr. Mergens' assertions. The building proposed sounds expensive. She asked the City to look into the type of professional being asserted is not someone who will want to be in a building that will overlook the interstate. The building will either end up empty, or low occupancy, or college students. She asked that the developer be held to the assertions being made.

Mr. Pat Foley, 295 Cottage Place, stated that his concern is traffic. Right now it is approximately one-half hour to reach his home. That is compounded with the train. He is not happy about this proposal. There is a high end restaurant in Little Canada and a Lindy's Steak House nearby. He is not convinced of the need for a high end restaurant. He noted that the

shopping center at this location took a hit with the development on Lexington with Target and Trader Joe's. An apartment building is not the solution.

Ms. Kay Sorgatz, 3800 Rustic Place, stated that she is very concerned about traffic because their property is on the curve. Her driveway is blind. She and her husband have been almost hit many times. A petition was raised to dead end the street, but there was not enough room for turn-around plows. Signage for right turn only means nothing. She is also concerned about crime. It is a comfort to know who drives past her house. She does not want to see the small town atmosphere of Rustic Place changed.

Mr. Anhil Toro, Rustic Place, stated that he is new to the neighborhood. He has seen this type of development before. People have no attachment or respect for the neighborhoods which get broken down. It cannot be controlled once it happens. He urged the Commission to stop it before it happens.

Commission Comments

Commissioner Ferrington expressed her appreciation for the overwhelming participation of this neighborhood. Some redevelopment of this area is necessary and will happen. The plan presented is problematic. She cannot imagine this much development in such a small area. A modified version may work, but 100 apartments is too many. The restaurant is appealing, but it would take too many variances for it to be built in that location.

Commissioner McCool expressed his appreciation for all the comments. He agreed that this site will be redeveloped. There has been a lot of study already completed with the Comprehensive Plan, the Corridor Study, which indicate redevelopment will happen. He also has concerns about this development. He encouraged that development team to do a traffic study as soon as possible. He would like to see a comparison of trip numbers in a residential and commercial alternative. He anticipates that commercial development will generate more trips than 200 apartment residents. The density and height feels too much. The building is too large a transition. He is intrigued with reversing the L so that one leg is along I-694. However, he had not considered the safety issues that could present. If that change is made, he would want to see lighting and crime prevention measures. He also likes the idea of a restaurant but is not sure if this is the right location. The heights presented are too close to the property to the north. No nearby recreation for children is troubling. While the parcel is limited, he would like to see some effort to provide a tot lot or some recreation to make it feel like it will fit in the community. It is important to understand that this site will be redeveloped and it will be done as responsibly as possible.

Commissioner Schumer stated he is impressed by the unity of the neighborhood. He did not appreciate the one speaker who was threatening if this development is approved. This is a concept design. Something is going to happen on that site. The City wants to hear ideas from the residents. The size of what is presented is too big. The developer is answering the desire of Shoreview for a new restaurant.

Chair Solomonson thanked everyone for coming. He stated that he agrees with the previous Commissioners' comments. This development does meet a need of the City. It would replace a blighted mall. It is very commercial to the south. But what is presented is too dense, too tall, too close to residential, and it adversely impacts the character of the neighborhood. He agreed that there is a problem without access to parks, which means new residents would spill into the existing neighborhood. He noted Midland Terrace apartments, a six-story building, which replaced a strip mall, but is 1000 feet from residential properties. A restaurant could go in with the current zoning. His biggest objection is the high density.

Commissioner Doan thanked everyone who listened to staff, the developer and neighbors. He understands the desire to keep the good parts. The question is what is the best this could be in the future and how can it compliment the neighborhood? Something needs to happen on this site. The question is whether this development is the best option for the City. There needs to be more vetted as to what could work on this site. There is definite concern with regard to height. There is a lot of asphalt. He would like to see more open space, more green space that is accessible at ground level for those not living in the building. There is a concern about the setback. If the L shape was flipped and inverted, there would be a lot more distance from the neighborhood. What the developer has characterized is what he believes Shoreview to be. He urged the developer to consider owner occupied units.

Chair Solomonson explained that no action is taken on a Concept Plan. This proposal will be considered at the August 17, 2015 City Council meeting.

Chair Solomonson called a five-minute break and then reconvened the meeting.

VARIANCE

FILE NO: 2581-15-24
APPLICANT: LUBOMIR & HANA KOUDELKA
LOCATION: 874 WESTVIEW DRIVE

Presentation by Economic Development and Planning Associate Niki Hill

The property is located at the south end of Westview Drive. The rear property line abuts Wilson Park. The trapezoid-shaped property has an area of approximately 16, 117 square feet. This application is to construct a four-season porch addition of 15 feet by 10 feet, which would extend into the rear setback. Currently, the closest point of the house is at the 30-foot rear setback. The new porch addition would reduce the rear setback by 10 feet to 20 feet. Therefore, a variance request is submitted.

The applicant states that the existing four-season porch serves as the only dining room in the home. With 10 people in the family, it is inadequate to accommodate everyone. Extending the dining room east or west are not options without major structural design to the house. To the west is the kitchen and all major utilities to the home; to the east is the living room and fireplace chimney. Widening the dining room east or west would also be cost prohibitive because of pitch of the roof and placement of the second story windows. The south or rear property line borders a

Mr. Espe introduced Ms. Terry Heaton from Springsted to answer any questions.

Mayor Martin asked if the difference in interest rate is a significant difference. Ms. Heaton stated that the City received a premium bid that provides some cash. The amount between the premium and bond issue must be under \$10 million. The City will receive the same amount of money, but the bond issue awarded at this meeting is in the amount of \$9,690,000 because more cash was paid up front.

On this bond issue, the difference between the estimated 2.7605% interest rate and the interest rate received of 2.4698% amounts to approximately \$300,000.

Mayor Martin asked the difference between Standard & Poor's "strong" rating and "adequate" rating. Ms. Heaton explained that a grid is used to look at total debt to total expenditures. There are larger payments that are paying off retiring bonds. That pushed the City into the "adequate" category. The most important measure is if the City has the money to make all the payments.

Mayor Martin explained that the Water Treatment Plant is the biggest infrastructure project ever undertaken by the City, which is the reason for issuing bonds. The Water Treatment Plant will benefit everyone by removing iron and manganese from the water.

Councilmember Wickstrom commended Mr. Espe and his department for the continued excellent work and preserving the AAA bond rating with the change in staff during the last year and a half.

MOTION: by Councilmember Wickstrom, seconded by Councilmember Johnson to adopt proposed resolution #15-69 authorizing issuance, awarding sale, prescribing the form and details and providing for the payment of \$10,000,000 general obligation bonds, Series 2015A.

ROLL CALL: Ayes: Springhorn, Wickstrom, Johnson, Quigley, Martin
Nays: None

PUD CONCEPT STAGE - ELEVAGE DEVELOPMENT GROUP, 155-173 COUNTY ROAD E, 185 COUNTY ROAD E, 3500 RUSTIC PLACE, 3521 RICE STREET

Presentation by City Planner Kathleen Castle

Elevage is presenting a Concept Stage PUD to redevelop the subject properties with mixed use residential and commercial development. The property is north of I-694 and west of Rice Street; it consists of 4.14 acres with frontage on Rice Street, Rustic Place and County Road E. Existing improvements include three single-family homes and a commercial shopping center of 34,887 square feet. Adjacent land uses include low density residential, commercial and commercial development in the City of Vadnais Heights. The commercial shopping center is zoned C2, which allows restaurants, fuel stations, banks, liquor stores, hardware stores and general retail.

The proposed project is to redevelop the four properties with a high density market rate apartment building with 100 units. The building is L-shaped running along the north and west

side of the property with five stories on the west leg and four stories on the north leg. A restaurant of 6,576 square feet is located in the northeast corner and a small retail building of 1,868 square feet on the corner of Rice Street and County Road E. Access is being considered off Rice Street and County Road E. The applicant is seeking flexibility on requirements relating to setbacks, height, and parking lot design.

The existing land use designation is C, Commercial and RL, Low density residential. The applicant seeks to rezone the site to MU, Mixed Use, which requires a Comprehensive Plan Amendment. The property, with the exception of 3521 Rice Street, is within Policy Development Area (PDA) #18. PDA's identify underutilized sites in the City and establishes policies and goals for that site. The commercial parcel is zoned C, Commercial and O, Office. This is also Targeted Redevelopment Area #2, which recognizes adjacent low density residential and the need for redevelopment to have a low impact on that neighborhood.

The City's Highway Corridor Transition Study identifies this property for potential expansion that would include high density residential and mixed use. The Economic Development Authority (EDA) lists this property in its 2015/2016 Work Plan for redevelopment and notes the need to address infrastructure for the new interchange. Mixed Use allows up to 45 units per acre. The approximate density proposed is 41 units per acre. Mixed use with high density residential and commercial may be compatible if there are design strategies to minimize impact to adjacent residential properties. This means adequate structure setbacks, a buffer and attention to the height of any planned building.

Staff finds that the proposed high density apartment building would be best located on the western portion of the site due to site characteristics, proximity to the transportation corridor and the low-density residential area to the north. A five-story building would exceed the City's limit of 35 feet and would have a visual impact on the residential neighborhood to the north. Added height would be allowed with increased setbacks from the property lines. The Lake Johanna Fire Department had no issue with the proposed building height but did comment on requirements for access.

Access is proposed off Rice Street and County Road E. A traffic impact study would be required from both Ramsey County and MnDOT. Surface parking is planned for 162 stalls. Underground parking is planned for the apartment building. There is not enough information at this time to determine if parking requirements are met. A minimum 20-foot setback is required from residential lot lines and the street right-of-way.

Property owners within 350 feet were notified of the proposal. Comments of opposition focus on land use compatibility, density, public safety, traffic, visual impact, architectural design/scale and environmental impacts. The developer held a neighborhood meeting in July. The Planning Commission reviewed this proposal at its July 28, 2015 meeting and heard public testimony. The Commission found that the proposed uses are appropriate with the proximity to arterial roadways but expressed concern about impacts to the residential property to the north. There was also expressed concern about traffic and access to parks, trails and green space.

The Council is asked to review the proposal and identify issues that need to be addressed. Should the application move forward, the following will be required: 1) Comprehensive Plan Amendment; 2) Rezoning; 3) Preliminary/Final Plat; 4) Planned Unit Development - Development and Final Stage reviews.

Mr. Michael Mergens, Co-Owner Elevage Development Group, stated that the Concept Stage PUD is optional to receive public comment and develop a good project. Elevage plans to close on the shopping center property by the end of the month and move forward with development. That parcel is zoned Commercial. Without rezoning, development could have been proposed with allowable uses. However, Elevage desires to build a quality development of Mixed Use. Rather than just rezoning to Mixed Use and proposing allowable businesses, the need for more apartments in Shoreview was recognized. The building proposed would have a green roof, atrium and top floor with floor-to-ceiling glass, which come at significant cost. Four parcels have been consolidated for the best layout. Design details will be part of the next review. What is being looked for is feedback on placement of the proposed components and whether the area planned as a restaurant should be a restaurant or retail.

Planning Commission Chair Steve Solomonson stated that the biggest concern is the intensity of what is proposed--a restaurant with a high density apartment building. There are concerns about the number of people filtering into the neighborhood, height of the buildings and access to green space. The question is how to buffer a five-story building. There was discussion about shifting the apartment building toward County Road E, but public comment expressed concern about creating a cove effect that would be a public safety problem. There is a lot of intensity for the size space with the apartment building, a large shared parking space and restaurant.

Mayor Martin opened the discussion to public comment.

Mr. Dennis Hamilton, 3633 Rustic Place, stated that the neighborhood believes this is a good site for redevelopment. However, the proposed development is too big with no real transition to the neighborhood. At this time, there are 76 landmark trees between the neighborhood and the site that have worked for years as a buffer. Those trees would be removed. Increased setbacks will not allow privacy. If the green space has to be on the roof, the project is too big. Green space needs to be on the ground. Residents would like to see some commercial space in the area with access for pedestrians and bikes. The viability of such a large project is in doubt. A luxury apartment building on this site will not have the amenities of access to parks and trails. Whatever is built will be there for a long time. As a gateway to the City, careful planning is needed. Residents want to see the right size development for the site and provide a good transition to the neighborhood. There is concern about rental and whether such a large building can be successful. A previously built apartment tower is only 20% occupied. The proposal is not close to what it should be but off by a lot.

Mr. Nathan Anderson, 3565 Rustic Place, stated that residents do understand that this site will be redeveloped, but residents feel like bystanders. This is a very complicated problem. He asked the meaning of flexibility under a PUD. He stated that flexibility can be a devastating word in fostering livability for residents. He referred to Highway Corridor Transition study recently contracted by the City to determine best potentials for redevelopment of highway

corridor sites. He noted that the study cautions development of his neighborhood to include consideration of impacts of noise, traffic and speeds may have on homes along the arterial roadways. The study refers to a windshield survey of the community. He asked how a survey of a community can be done by driving down the street of a neighborhood. The study is a blueprint for local government to have a developer come in. The study does not include any assessment of the impact to the neighborhood. There is no input on what will happen to the Rustic Place neighborhood. Mixed Use is proposed next to large (almost estate size) neighborhood lots full of trees. There is not enough room to buffer what is proposed. The study is in direct opposition to Shoreview's Comprehensive Plan and codes. The study does not mean the development should be high density. There needs to be flexibility in interpretation of the study. Continued study is needed of detrimental impacts to the neighborhood.

Mrs. Becky Harshauer, 3562 Rice Street, stated that her zip code is Shoreview, but technically she lives in Vadnais Heights. However, she received a notice and wanted to note that they are being told that an apartment building will have less traffic than retail. Retail would have distributed traffic. Restaurant and apartment traffic would impact the rush hours when there is already a struggle with traffic at that time. More apartment buildings are not needed in Shoreview, not even two stories. Residents would prefer retail because buildings would be one story and there would only be business traffic. Upscale retail such as Kowalski's, women and children's clothing, Boston Market. There is already a restaurant planned at the Rainbow site. Elevage has two business registrations with the Minnesota Department of Commerce. Both businesses are listed at the same address, which is different from the Bloomington address given by Mr. Mergens, which is troubling. In the Stillwater newspaper, it was mentioned that Elevage was fired by the city for missed deadlines. New apartment buildings are required to have a percentage of low income units. Also, there is no access shown to Owasso Beach or Snail Lake Beach. Residents do not want an apartment building.

Mr. Jim Shea, 123 Radisson Road, NE Blaine, stated that he works for Pet Junction, one of the current retail stores in the existing shopping center. He would like to see this pet facility remain. It is upscale and provides training. There are people that come from many towns outside the Twin Cities to train their pets. The business is very lucrative and has been on this site for 20 years. It is important to look at what is good for the economy. He would like to see a restaurant and other retail. There is little in the area with good retail services.

Mayor Martin closed the public comment period at 9:00 p.m.

City Planner Castle responded to the question about flexibility within a PUD. She explained that a PUD is used to encourage creativity and innovation. With redevelopment, it is difficult to conform to Code requirements. The PUD process provides flexibility. In return, the City needs to receive a benefit from the project, such as meeting City goals for economic development or housing, or providing transportation improvements for the community. Those are the types of things staff looks for in a PUD.

Mayor Martin asked about the possibility of moving the east/west wing to the south, which is the shorter of the two segments. Mr. Mergens indicated that location on the map. He further stated that Elevage Development Group is registered with the Secretary of State of Minnesota with a

registered address. The address he provided is the address of his law firm. Elevage was not fired by Stillwater.

Councilmember Johnson asked Mr. Mergens to share some success developments he has been involved with. **Mr. Mergens** stated that he was the lead attorney on a six-story apartment building in downtown Minneapolis that was a \$50+ million project, the lead attorney on a project in Golden Valley and another project in Blaine. He emphasized that the intent of Elevage is to work with the City. Being told the development won't work is not constructive. Factual feedback is needed to refine it. It is not viable to redevelop the entire site with retail. The apartment building is a vital part of making the retail viable.

Mayor Martin asked who the clientele would be for the apartment building. **Mr. Mergens** stated that there are different requirements that can require low income apartments, such as TIF Districts. The proposal is for a market rate apartment building. There will be a variety of incomes. Their market study shows more than a 1000 units needed with incomes over \$40,000 by 2020 in Shoreview. At this time, no such projects are planned.

Councilmember Wickstrom asked the pros and cons of moving one portion of the apartment building to the south. MnDOT mentioned noise and she asked if there would be a noise wall. People on the south end of the apartment building will see the freeway or a noise wall and her concern is if that impacts the desirability of the building. **Mr. Mergens** stated he does not believe the apartments on the south end will impact the desirability of the building. There are those who will like the quick accessibility to the freeway, and there is a dynamic view to downtown St. Paul. Mr. Maloney added that MnDOT is not planning a noise wall on that side of the freeway with the lane addition.

Councilmember Wickstrom stated that the proposed building does not look residential. It has a box look, which is not like other buildings in Shoreview. She asked if more features can be added to the building. **Mr. Mergens** stated that is the type of information he is looking for. The next stage will have such details.

Councilmember Wickstrom expressed other concerns about traffic. She asked the pros and cons of closing off Rustic Place. She asked if more green space can be incorporated. She asked what types of retail would be viable and if retail would be viable on the first floor of the apartment building. **Mr. Mergens** stated that there is more potential for retail on Rice Street, but not in the back of the site on the first floor. There tends to be tension between residents and retailers, and most apartment buildings are not including retail. He further stated that market studies have been done. The apartment building would not be proposed if they did not think there would be close to 100% occupancy.

Councilmember Wickstrom stated that younger people want amenities, but there are not many at this site. **Mr. Mergens** stated that one of the desirable factors is that people want to live in Shoreview. He added that it is not just transient people who move into apartments. Apartment living has become a lifestyle, and many move into apartments with the intent of living in them for a long time.

Councilmember Quigley stated that he sees the main issues as buffering and height. He asked about flipping the building so that one leg is on the south side. The history of small retail at that site has not been successful. Added traffic will be minor to the traffic already there. With good lighting he does not see public safety as an issue. Added buffering would be well received. One difficulty is that there is no playground, but he noted another building at Victoria and County Road E without a playground. It will take flexibility to fit the development well on this site.

Mayor Martin asked about the possibility that the project does not work. **Mr. Mergens** responded that it is important to do all due diligence, rely on expert projections and decide what can be developed to meet needs and projections. With the \$20 million being invested, a lot of time is spent on such studies and making those decisions. There is a risk to any development. Everything he has studied points to Mixed Use for more residents that would support a smaller retail project.

Mayor Martin responded the City's Corridor Study was a followup to a Policy Development Area study that was established before the last Comprehensive Plan. The City has always done long-range planning. This strip mall is a blighted area. Many different types of shops in that strip mall have not succeeded. She agreed with Mr. Shea that Pet Junction is a success, and the City is not rejecting Pet Junction. The owner has put the strip mall up for sale. Her concerns are about green space. The height could be modified by moving it to the south. The current height would need greater setbacks. Trees are important and how trees would be replaced. The apartment building would serve as a bit of a sound barrier to the neighborhood. There are nuisance properties to the west and north. Those problems would not be solved with just a retail development. If this development proceeds, it would solve the problems with some of the nuisance properties. Further, the Corridor Study is an indicator of what could happen but it is not cast in stone.

Councilmember Springhorn also urged finding a place for Pet Junction. He expressed appreciation for all the comments. He does not like to see statements that renters do not care about the land. There are places where there are problems with renters and places where renters are great neighbors. He does not want renters to not be welcomed into Shoreview. If the building is moved south, he suggested trying to fit in a small playground.

Councilmember Johnson agreed with Councilmember Springhorn. The community continues to want more restaurants. She asked that component be made a priority. She suggested using crime prevention through environmental design and use that as a tool to see what crime prevention mechanisms can be put in place.

Mayor Martin stated that no decision is made at this meeting. The next review will also be a public hearing. She added that when she built her dream home in 1967, the City rezoned for an area with 986 apartments that completely surrounded her home. What she learned is that setbacks are very important, height is important, green space is enormous and today she can say that it is not a bad thing to have those apartments in the area.

ADJOURNMENT

MOTION: by Councilmember Johnson, seconded by Councilmember Springhorn to adjourn the meeting at 9:35 p.m.

VOTE: Ayes - 5 Nays - 0

Mayor Martin declared the meeting adjourned.

THESE MINUTES APPROVED BY COUNCIL ON THE 8th DAY OF SEPTEMBER 2015.

Terry Schwerm
City Manager

Date: January 20, 2016

To: Kathleen Castle

From: Mark Maloney, Public Works Director
Tom Wesolowski, City Engineer

Subject: Preliminary Plat – PUD Development Stage
Elevage Development Group








The City of Shoreview Engineering staff has reviewed the preliminary engineering plans submitted for the proposed mixed redevelopment at the intersection of Rice Street and County Road D. The Engineering staff has the following comments regarding the proposed development:

1. The proposed project is located within the Ramsey-Washington Metro Watershed District (RWMWD). The project will disturb more than 1-acre and will require a permit from the RWMWD. The City requires that all information that is submitted to the RWMWD, as it relates to the proposed development, also be sent to the City of Shoreview.
2. The developer and their engineer met with staff from the RWMWD and the City to discuss stormwater treatment on the site. Based on the meeting the engineer for the developer will submit a revised stormwater management plan and permit application to the RWMWD. Infiltration is proposed on the site, which is located within the St. Paul Regional Water System Drinking Water Supply Management Area (DWSMA). Based on the City's MS4 permit and RWMWD requirements, if infiltration is proposed within a DWSMA the developer must evaluate the vulnerability of the aquifer to determine if infiltration is acceptable. The engineer was directed to contact St. Paul Water to discuss the proposed infiltration. If infiltration is not allowed the stormwater on the site would need to be treated by filtration prior to discharging into the City's stormwater system.
3. The developer shall determine if any wells are located on the properties and determine if the wells have been abandoned as per the requirements of the Minnesota Department of Health. If that has not been done the Developer shall abandon the wells as per the State requirements. The developer shall provide information to the City that the wells have been properly abandoned.
4. The piping for fire protection and the domestic supply shall be separated prior to entering the building and shut-off valves installed on each line.
5. Water and sanitary sewer services for the townhomes are not shown on the plans. For the water service the City prefers a meter be installed on the main service line prior to the individual services to each townhome. Any configuration of private water service should be designed to insure that all potential water use on the site is metered.
6. Shoreview's Comprehensive Plan shows the need for a paved, multiuse trail along the west side of Rice Street with a connection to the neighborhood to the north side of the proposed development. The reconfiguration of the I-694/Rice Street interchange will also modify pedestrian options for the area. The trail along Rice Street should meet the City's

requirements for an 8-foot wide bituminous trail and be owned and maintained by the City.

7. The developer has submitted a traffic study that estimates the effect of the proposed mixed use development on the County Road E and its intersection with Rice Street. The study was prepared by a licensed engineer from the State of Minnesota and was reviewed by both the City and Ramsey County. The study indicates that there is heavy traffic at the intersection and that the Level of Service (LOS) of the intersection will remain the same until improvements can be completed on Rice Street and the Rice Street/I694 interchange. Ramsey County, who has jurisdiction over Rice Street/County Road E intersection, is currently working to receive funding for improvements in 2018. If funding is not received the improvements will most likely be delayed. While the Level of Service (LOS) of the intersection certainly isn't optimal, the existing public roadway infrastructure is adequate for the anticipated increase in vehicle trips and the overall operation of the intersection will remain relatively unchanged. In addition, we agree with the findings of Westwood Professional Services memo dated January 20, 2016, that states that "the potential is virtually nonexistent for site-generated traffic to divert onto Rustic Place and "cut through" the neighborhood."
8. Cash Escrows will be required for any utility, trail or driveway work in the public right-of-way. An erosion control cash deposit will be required for the development.
9. A tree preservation surety shall be included at the time of the Development Agreement to ensure proper tree protection is installed and maintained throughout construction.
10. Please notate tree species and DBH for trees to be removed to determine replacement number. Tree replacement will be a 6:1 ratio for landmark trees per city code.
11. Please refer to City Code Section 209.050(B)(2) for tree replanting guidelines. Replacement guidelines for non-landmark trees is 1:1. Shrubs such as the proposed Miss Kim Lilacs are not considered suitable vegetation for tree replacements.
12. Within the urban forest of Shoreview Spruce and maple are over abundant at 21% and 14% respectively. The overabundance of any one species significantly increases its risk to pests and disease. Staff advise the developer to consider alternative tree species to help diversify the landscape and make it more resilient to future tree diseases and pests. Some alternatives may include Hackberry, Lindens, Birch, Coffeetree, Ginkgo, Beech and Dutch Elm disease resistant elms.
13. The preliminary plans will be presented to the Environmental Quality Committee for their review and comment at the January 25th meeting.



-  City Halls
-  Schools
-  Hospitals
-  Fire Stations
-  Police Stations
-  Recreational Centers
-  Parcel Points
-  Parcel Boundaries
-  Airports



Notes

Mixed Use Redevelopment
File No. 2602-16-01



Print Date: 07/23/2015
Image Date: 03/25/2012
Level: Neighborhood

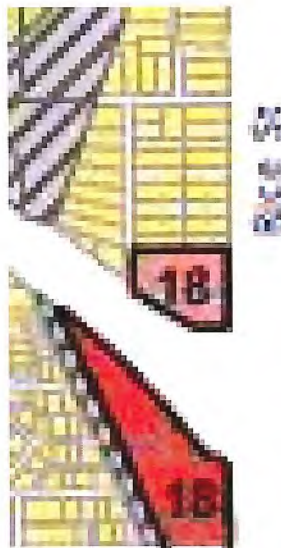
Zoning Map



Zoning Classifications

- RE - Residential Estate
- R1 - Detached Residential
- R2 - Attached Residential
- R3 - Multi-Dwelling Residential
- R4 - Mobile Home Residential
- C1 - Retail Service
- C2 - General Commercial
- OFC - Office
- I - Industrial
- T - Tower
- OS - Open Space
- PUD - Planned Urban Developme
- UND - Urban Under Developed
- BPK - Business Park
- Water

Planned Land Use - 2008



Legend

PDA Boundaries

Planned Land Use

- Residential (up to 4 units/acre)
- Residential (4 - 8 units/acre)
- Residential (8 - 20 units/acre)
- High Density Senior Residential
- Office
- Commercial
- Mixed Use
- Business Park
- Tower
- Light Industrial
- Institutional
- Park
- Recreational Open Space
- Natural
- Railroad
- Open Water

December 28, 2015

Mayor Sandy Martin and City Councilmembers
CITY OF SHOREVIEW
4600 Victoria Street North
Shoreview, Minnesota 55126

Dear Mayor Martin and Councilmembers:

On behalf of ELEVAGE DEVELOPMENT GROUP, LLC ("**EDG**") and its wholly-owned subsidiary ELEVAGE SHOREVIEW HOLDINGS, LLC ("**ESH**"), I submit this narrative in support of the applications for a Development Stage PUD, Major Subdivision/Preliminary Plat, Rezoning, and a Comprehensive Plan Amendment (collectively, the "**Applications**").¹

In the Applications, EDG seeks to redevelop four blighted parcels (collectively the "**Property**") into a coordinated mixed-use project comprised of 14 market-rate rental townhomes, 134 units of market-rate apartments, approximately 6,800 square feet of resident-focused retail, high-end amenities, and significant open space (the "**Project**"). We expect that a majority of the retail space will house a locally-owned, high quality restaurant. The Project is expected to require an investment exceeding \$30 million.

The Property resides immediately north of I-694 on Rice Street and includes the commercial lot ("**Commercial Lot**") on the northwest corner of Rice Street and County Road E. It also includes two residential properties to the immediate west and one residential property to the north along Rice Street. Together, the four parcels contain nearly 4.5 acres of land and provide a tremendous opportunity to meet the City's needs of today and tomorrow.

THE PROPERTY'S CURRENT USE

Before providing greater detail on the Project as a whole, I first turn to the Property in its current state. The Commercial Lot is currently "improved," in the loosest sense of the word, with a shopping center that was built in 1957 and has suffered from high vacancy for years. Currently, more than 17,000 square feet of the approximately 29,000 square feet of net rentable area sits empty (a vacancy rate of nearly 60%). Perhaps not surprisingly for a center with nearly 60% vacancy, the shopping center is dilapidated. The roof has outlived its useful life and presently suffers from significant leaks. The interior is rundown and old. Contrary to modern tenant needs, the shopping center suites are built narrow and deep. Any attempt to resurrect the existing center would require more than \$1,000,000 in basic infrastructure repairs. And, even if those repairs **could** be made, the design of the existing center is unworkable for modern retail tenants. The building is far too deep with far too little storefront to bring in rental rates sufficient for profitability as is, let alone justify \$1,000,000 in repairs, plus at least that much more in tenant improvements. Quite simply, the nearly

¹ The undersigned is an owner, officer, and governor of EDG.

fifty-year-old shopping center has outlived its lifespan. It is now functionally obsolete and blighted.

Replacing the existing center with a new retail center is likewise unsustainable. First, there is no demand to justify developing the Property as solely commercial nor does the Property justify such interest. Unlike residential, retail requires that at least the main end-user be identified before it is constructed. That demand quite simply does not exist—and has not existed—for the Property for quite some time. Ironically, the previous ownership group included a commercial developer with millions of square feet in retail development. Despite a clear need for redevelopment, he was unable to identify a commercial-only redevelopment option.

The reason there is no demand to redevelop the Property as solely commercial is clear. The land and demolition costs make a small retail building unfeasible. But large retail was equally problematic. Even combined with the residential properties, the Property lacks adequate space to meet the needs for parking and storefront. Most importantly, retail needs an adequate customer base and, as history has shown, the Property does not attract enough customers to support large retail as it sits. Of course, there are few, if any, options that would have a greater traffic impact on the surrounding neighborhood than a large commercial redevelopment.

The looming Rice Street bridge replacement bears mentioning, as well. Unlike mixed use, no commercial tenant (and hence no commercial development) could withstand the uncertainty of the bridge reconstruction. We all know it is coming, but no one yet knows when or in what form. It is another rationale in an already long list of reasons that commercial-only use is a non-starter for the Property.

The Property also includes two residential parcels to the immediate west (3500 Rustic Place and 185 Rice Street). Both properties are blighted, with 185 Rice Street suffering from such significant blight that it likely should be declared uninhabitable. The final parcel within the Property is 3521 Rice Street, which was a rental property prior to ESH's acquisition and is also blighted. The homes are unquestionably old, poorly-maintained, and in need of replacement. In sum, these four parcels are all ideally suited for redevelopment. Indeed, they are in desperate need of it.

THE PROJECT IS A UNIQUE OPPORTUNITY TO MEET THE CITYWIDE NEEDS SET OUT IN THE COMPREHENSIVE PLAN

As described below, the Project takes four severely blighted properties and replaces them with a coordinated, community-driven, mixed-use development. In this regard, the Project is exactly what the City's 2008 Comprehensive Plan (the "**Comp Plan**") identifies to meet the City's future needs. Of particular relevance, the Comp Plan states:

The Metropolitan Council has found there is a need for more housing within the next 30 years to accommodate the projected population growth while recognizing a change in unit preference type due to demographic changes. In past years, single family detached housing has been the preferred type of home but is now being outweighed in preference to

attached housing that is more desirable for empty nesters, singles, young couples with no children.

The evolution in housing preferences to high density, rental units is especially true for the under-45-year-old demographic. As noted in the Comp Plan, more than 30% of those in the 25- to 34-year-old age range rent their homes and nearly 20% of those in the 35- to 44-year old range. This is the same age range that the Comp Plan identifies as a point of emphasis for the City. Of course, Shoreview is not the only city that desires to increase its population of 25 to 45 year olds, especially those with household incomes of more than \$40,000.00. It is the key demographic for supporting existing businesses and for rejuvenating the City's housing stock. It is also the demographic that will comprise most of the employment force for the City. For businesses in the City to attract these qualified candidates in an ever-tightening labor market, appropriate housing options are critical.

This critical demographic is currently vastly under-served by the City. A housing demand assessment prepared by VIEW POINT CONSULTING GROUP, LLC (the "**Housing Study**") examined housing demand for persons under 65 with an income greater than \$40,000 within Shoreview and surrounding communities, an area defined as the Primary Market Area (or "**PMA**").² The Housing Study noted that the PMA's population experienced a slight decline last decade (-1.9%) because the PMA was "fully developed with little land available to accommodate new housing." Over the next five years, however, the PMA will see significant growth and a corresponding projection of 3,754 new housing units needed across the PMA. On the specific question of market-rate rental needs, the Housing Study concluded that "[h]ousehold growth, pent-up demand, and turnover combine to create demand for **1,097 additional market rate units** in the PMA over the next five years."

The Housing Study also found that if the PMA is to meet its projected housing growth this decade, "many multifamily housing redevelopments, such as the subject Site, will need to occur because of the limited supply of land for the construction of lower-density housing, such as single-family homes." Despite the need for market-rate rental units in Shoreview and the surrounding communities, the Project, as of March 2015, was the **only** market rate rental project that was proposed, and no other projects were under construction. Since any new housing complex of this size will take many months if not years to open, the Project is a critical component to meeting this five-year housing demand.

As noted in the Housing Study, the proximity to employment is a key factor when deciding where to rent. And available housing is an important dynamic for attracting and keeping elite employers. There are several major employers near the Property, including API GROUP INC., BOSTON SCIENTIFIC, DELUXE CORPORATION, H.B. FULLER, LAND O' LAKES, MEDTRONIC, SMITHS MEDICAL, and ST. JUDE MEDICAL INC. The Project is designed with these businesses in mind. It will provide the amenities and finishes that the young professionals attracted by the businesses want and expect.

As noted in the Comp Plan, the City faces two conflicting realities: 1) by the year 2020, there

² The study's primary market area included Arden Hills, Little Canada, Mounds View, New Brighton, North Oaks, Roseville, Shoreview, and Vadnais Heights. A copy of the Housing Study is enclosed for your convenience.

will be significant housing needs, especially in the rental category; and 2) the City has very limited opportunities for new housing development. The Comp Plan spends significant time on certain potential redevelopment areas as Policy Development Areas (PDAs) or Targeted Redevelopment Areas (TRAs). It notes that, in at least some of these areas, “the City anticipates housing will play a vital role and will provide an opportunity to address the community’s housing needs.” The Property, and more specifically the Commercial Lot, is identified in the Comp Plan as both a PDA and TRA.

Not surprisingly, multiple studies and the City’s Comprehensive Plan have identified the Commercial Lot as a target for redevelopment and have suggested redevelopment as mixed-use. Equally unsurprising, particularly given the need for more housing within the City of Shoreview and the lack of vacant property, the Property has been highlighted as good location for high-density residential. Specifically, when the City commissioned land-use experts to review the areas around the Property, the resulting report, the March 2015 Highway Corridors Transition Study (the “**Corridor Study**”), identified a key element for the area of north of I-694 was the “[p]otential for horizontal mixed-use development with higher density residential and a reduced amount of commercial space as compared to today.” The Corridor Study goes even further. The Commercial Lot along with 185 County Road E and 3500 Rustic Place were reviewed in particular and identified as “Project A” in the study. The study suggests Project A should be developed as “horizontally-mixed development.” The Corridor Study suggests updating the Comp Plan discussion of the area around the Property to “include high density residential” and “[c]reate a Mixed Use district... that can apply to Projects A and/or D.”

In short, the Project is not only a chance to remove blight, but to do so in a manner that will provide citywide benefit by directly meeting a critical housing need, as identified in the Comp Plan, for the specific demographic that Comp Plan emphasizes as a focus point. Of course, there is also the citywide benefit that comes from replacing four blighted properties, some severely blighted, with a new \$31,000,000+ development.

THE PROJECT IS THE CULMINATION OF THE BREADTH OF EXPERIENCE OF THE DEVELOPMENT TEAM AND COMMUNITY/EXPERT INPUT

With that background, I now turn to the Project currently before the City, which was essentially redone from square one following the feedback we received through the Concept Stage PUD process. Development of the Project began shortly after the input of the neighborhood, Planning Commission, and City Council we received through the PUD Concept Plan. As described below, it is now a highly-analyzed and polished proposal that is the result of many hours of meetings, research, reworking, and review.

Following the City Council meeting, the development group made the decision that rather than revising the existing plan, we needed to explore a complete reworking of the layout. In the days that followed, we interviewed multiple architectural firms and requested that they each provide their vision of what should be developed in light of the City’s comments and demonstrate their experience with mixed-use developments of this scale. The clear winner to us was ELNESS SWENSON GRAHAM ARCHITECTS, INC. (“**ESG**”). The breadth of ESG’s experience with market-rate apartments and mixed-use projects was quite simply remarkable and beyond compare. As a recent Star Tribune article noted, ESG “dominate[s] the Twin

Cities market in the design of new housing and hotels.” They have a well-deserved reputation for knowing what the market wants and for knowing not only the big concepts, but all the details needed to ensure their projects respond to what the market demands. For the Project, they understood the need for it to be a part of the community and a lasting positive impact on all of the City of Shoreview. It certainly did not hurt that the lead architect lives in the Shoreview area.

ESG immediately went to work on designing a completely new concept for the Property. ESG, after a review of the City and neighborhood comments, responded with an initial concept. For ESG, its experience confirmed what the neighbors and City suggested: the apartment buildings need to be moved from the northwest corner to the southeast corner of the Property. The initial concept called for a 6-story, 75-foot tall apartment building that included approximately 10,000 square feet of retail. Looking at options to transition to the existing single-family homes and to enhance to holistic design of the Project, ESG proposed a series of townhomes on the western portion of the Property. Critical components of the new design were an emphasis on much greater underground parking and a design of the apartment building aimed at providing the greatest set back from the existing homes. The design added substantial open space and a sense of community. At 6-stories, the concept also allowed the apartment buildings to be set back from the northern property line by more than **120 feet**.

Since we were working with a completely new vision and concept, we held a neighborhood meeting to share the new idea with the neighbors. Recognizing that the new concept was still in the preliminary stages, we held a second meeting with neighbors to discuss the concept. Between the neighborhood meetings, we also met with representatives of the Ramsey County Public Works Departments. We also consulted multiple experts and have held weekly meeting to revise and refine the Project. Those meetings and additional discussions with Ramsey County and private consultants resulted in significant refinement of the initial concept to what is now before the City.

As if often the case in a project of this scope, there have been nearly as many opinions as there are people we spoke with about the Project. While there were advocates for the 6-story, 155-unit apartment complex, after gathering all available information, we reviewed every aspect of the Project to consider alternatives to respond to a vocal neighborhood group that opposed the 6-story height. We again returned to the drawing board to analyze alternative designs. In reviewing the options, we had to remain aware that the greatest benefit and best projection for long-term success is that the Project is a coordinated design where multiple components each work to enhance the others.

As discussed below, we were able to reach a design that, while losing some positive features, responds to the desire for lower height. The apartment building is now more than 20 feet lower and reduced to five stories with a total height of 53 feet, 4 inches. The unit mix has likewise dropped to 134 units and the retail space was reduced to approximately 6,800 square feet and relocated. The Project as it now sits is the culmination of ESG’s experience across decades and hundreds of projects refined by nearly countless hours of expert analysis, all designed to create a project that delivers a superior, timeless, memorable, and holistic design.

THE PROJECT'S HOLISTIC AND INTEGRATED DESIGN, CREATES A BENCHMARK FOR FUTURE DEVELOPMENT

The Project has been designed to ensure a sense of community, to meet the needs of City as a whole, and to mitigate any impacts to existing homes. The apartment building is set back from the northern property line more than 75 feet. To the west, ESG has created a transition from high-density apartments to the existing single-family homes with 14 market-rate rental townhomes. Like the apartments, the townhomes are designed to attract “renters by choice.” The townhomes will be built with all the exterior and interior amenities that renters-by-choice demand. The entire Property will be linked by a trail system and provide considerable outdoor amenities such as a large dog run, a pool area, outdoor game areas, and significant open space.

The Project will also contain modern sustainability features. It will have underground cisterns to hold and gradually release all rainwater captured on the Property. The green spaces will be developed using fescue grass rather than sod, which will dramatically reduce water needs. The design will offer a pedestrian- and transit-oriented community that allows residents to live, work, and play without the dependence on daily automobile usage. The development team is committed to the sustainable design principles reflected in the Comp Plan. Our sustainable design mission is to promote livable communities through the use of energy efficient systems, green building practice, reduced dependency on automobiles, creative density, high-quality pedestrian and bicycle options, and preservation of natural resources. This mixed-use project will feature a series of green elements including green construction and demolition practices, green material specifications, thermal high-efficiency windows, and exterior envelope systems, and participation in the Xcel Energy Design Assistance Program.

A key feature of the Project is the link between the residential density and the retail component. As designed, the “right” retail is critical to maximize the success of the apartments and the residential density is key to ensuring the retail tenants will become vibrant and successful members of the community. In the concept design, we called for stand-alone retail that would be outward focused and designed to pull customers into the Property.

A key aspect of ESG's concept was to change the retail focus to be supportive of the residential, not exclusive of it. The expected restaurant is a focal point of a vibrant and exciting corner at Rice Street and County Road E that will enhance the community feel. It will benefit from—and be an amenity for—both the apartment component and the townhome component. As demonstrated by their long line of successful projects, where retail space is property integrated into and serves as an amenity to the residential component of a mixed-use development, both aspects thrive. In our analysis, the combined link is critical to a successful project. Neither aspect can reach its potential on the site without the other. What ESG was also able to show based on the multitude of projects they have designed, is that the “right” retail is critical to an elite market-rate apartment complex. They were able to show that the difference between vibrant mixed-use projects with first floor retail and those projects with a dark empty first floor is design and foresight. The projects that succeed are those that have the right ratio of residential density to retail floor space and the retail is focused on providing a benefit to the residents that the project seeks to attract.

In the initial design, the higher residential density supported a full 10,000 square feet of retail and, because of the coordinated nature of the Project, we could charge a lower per square foot rent for the retail space. To address the “height” comments, however, we closely reviewed options to adjust the residential-retail interaction. After considerable effort, we concluded that we could reduce the retail space to approximately 6,800 square feet by reducing the size of the expected restaurant and eliminating portions of the other retail area. With the reduced space and a change in the retail rent structure, we were able to reduce the required density and parking needs.

Next we looked at what could be done to reduce the residential density in the initial design. It was not a simple step, as ESG had designed a comprehensive concept where each component meshes with and supports the others. We analyzed the options to consider impacts to the retail customer base and what fewer apartments meant to the retail rent rates. Based on that analysis, we were able to reduce the unit count to 134 and lower the apartment building to 5 stories. We also looked at construction options to lower the height and reduce the overall massing of the building. Through creative building design, we were able to lower the actual height of the building from 75 feet to less than 54 feet and dramatically reduce the visual impact of the building. Unfortunately, one of the positives lost in the full-story height reduction, particularly in light of the need to maintain the minimum density that expert analysis shows is necessary for the retail to thrive, we could not maintain the enormous set backs from the neighboring homes.

For the Project, we all agreed that it must be designed to attract “renters by choice” with household incomes in excess of \$40,000 per year. But we did not simply rest on our beliefs or ESG’s experience. As discussed above, the Housing Study shows that the Shoreview market area has a significant deficiency for this housing need. There is need for 1097 new apartments designed for renters with more than \$40,000 in income and under the age of 65. It is a demographic that can, and does, pay the rents the Project will demand. While the market will dictate the rents, we expect the bottom of the rent range to be \$1,000. Of particular interest was the Housing Study’s conclusion that “32% of the age-/income-qualified households will seek to rent their housing.” This conclusion is wholly consistent with what the market has demonstrated: today, many people of financial means choose to rent not because of a lack of options but out of a preference to rent.

Our research has also confirmed that the renter-by-choice market is not limited to downtown Minneapolis and St. Paul. In fact, a recent Star Tribune article noted, “The Twin Cities apartment boom, which is transforming Minneapolis and St. Paul, is now thundering into the suburbs, where rentals are full... And after five years of intense development in the core cities, the action is shifting to the suburbs where there’s been relatively little building since the 1980s.” As the Housing Study confirms, Shoreview (like so many other suburbs) is part of this growing demand for high-end rental units and (like so many other suburbs), is not currently meeting that demand.

Because so-called renters-by-choice have adequate financial resources, they demand and expect quality from their apartment choices. Today’s market-rate rental units, including those in the Project, are a far cry from the past. The finishes inside the units are commensurate with high-quality homes including things like marble countertops and hardwood flooring.

These renters also demand and expect a sense of community. The apartment they live in is not a temporary option out of necessity, but one they view and treat as their home. They will expect and demand gathering areas, security and safety, and outdoor amenities.

To attract these renters and provide a citywide benefit, everyone agreed that the retail should be designed for a restaurant to be included in the Project. Of course, with substantial restaurant experience within the team, we have dramatically refined that area of the Project to set the expected restaurant up for success including the layout, the exterior seating options, the expected seating area, the rent, and the immediate residential density.

No architect understands or tracks the renter-by-choice demands more thoroughly or aptly than ESG. Among other things, ESG tracks the amenities of **all** Metro market rate apartment complexes, whether it was ESG-designed or not. With that knowledge and experience, ESG knows precisely what features must be incorporated into a market-rate apartment building. The Project is designed to provide those amenities. It has all the interior finishes that renters by choice demand, plus the sense of community. The Project offers significant open space and walking trails. We have added a dog run and open area on the north end of the Property. The outdoor amenity area offers the latest in community building. In addition to the pool, there is a community room, a grilling area, and outdoor exercise/play areas.

The Project and its 158 total units (134 apartment and 14 townhomes) is also fully supported by the Housing Study, which among other things, concludes that:

- The Site has excellent freeway access, is within close proximity to retail and employment, and the adjacent land uses would be compatible with multifamily housing. Overall, the Site is well suited for multifamily housing.
- It is reasonable that the Site in Shoreview, with good access to Interstate 694, can capture between 15% and 20% of the unmet demand potential in the PMA. This results in the support for about 165 to 220 units on the Site through 2020.

The Project, with its holistic and integrated design, directly meets the City's stated needs. And the Comp Plan recognizes that there are few opportunities to meet those stated needs. This Project does not simply **meet** the City's needs, it does so in a stunning manner that builds on the community. It will be the benchmark that sets a high standard for future redevelopment and will foster and accelerate that needed development.

THE PROJECT IS CONSISTENT WITH THE FEEDBACK FROM THE CONCEPT STAGE

As noted above, the Project is not a simple revision to the Concept Stage proposal, but a complete reworking of the development with an entirely new architectural firm. The Project is superior in every way to what was submitted and addresses each heading in the City's letter of August 25, 2015. We explore those issues below.

Mixture of Land Uses

As noted above, the Property has both hurdles and opportunities. We looked myriad options, including commercial-only use. Based on all available information, commercial-only is not viable, is not consistent with the Comp Plan, and would have far greater impacts on the surrounding homes. In contrast, a mixed-use development is supported by the Corridor Study and the Housing Study, and ESG has a long track record of success that shows how integrated designs, such as the Project, will and do thrive.

Intensity of the Development

In developing the Project as a coordinated unit, the team spent considerable time analyzing the impacts of all options and comments. With the retail incorporated into the apartment building and designed for much more of an internally-driven retail tenant, the appropriate amount of residential density is critical. Not only does it generate potential customers, but the retail rent is directly linked to the overall project density.

More importantly, the density is consistent with the Comp Plan, which calls for density of up to 45 units per acre for mixed unit developments. The Property contains 191,600 square feet, or 4.4 acres. At 45 units per acre, the Comp Plan calls for up to 198 units on the Property. The Project in total has 158 (134 apartments and 14 townhomes), or 33.6 units per acre.

With respect to set backs, the City Code does not have a “mixed use” district and the Project is submitted as a PUD, which is designed to avoid strict application of the City Code. That said, we have looked to the R3 district as guidance for set back, height, and similar setbacks.

The apartment building complies with the height requirements. Under City Code Section 205.084(C)(3), the City Code allows for heights greater than 35 feet so long as the set back is increased one foot for each additional foot of height. The setback requirements under City Code Section 205.084(c)(2) is 30 feet with the caveat that “[z]ero lot line development shall be permitted.” The Apartment building is 53 feet 4 inches, which would be permitted with an additional 18 feet, 4 inches of setback or a total setback of 48 feet, 4 inches. The apartment building is set back from the northern property line by more than 75 feet, which is well beyond the setback needed to justify the height. As shown in the shading study included in the application, the overly large setback also creates little impact on the existing single-family homes. On the Rice Street and County Road E side, it is both desirable and appropriate that the apartment building to have smaller setbacks; nevertheless, the apartment building meets the requirements here, too, with nearly a 50 setback on both sides.

The Project also complies with the lot coverage limit for R3 of City Code Section 205.084(c)(4), which allows a maximum of 65% with a possible increase to 70% if best management practices are used. We would note that the standard for General Commercial is 75% and up to 80% if best management practices are used. The Project comes in at 55.7%, which is well below the 65% threshold. It should also be noted that the Project increases the existing impervious surface cover of 101,500 to 106,770 after the Project is completed, a mere 5,220 square feet of additional coverage as part of a complete redevelopment.

The Project is also adequately parked. The apartment and retail building provides 168 below grade units and 77 surface parking. The townhomes each have tuck-under garages with driveway parking.

Residential Density

The Project is markedly different in terms of layout and design from the Concept Stage proposal. The apartment building has shifted to the opposite corner of the Property to move it as far as possible from the existing residences. To create a further transition to single family residences, the western portion of the Property will be developed with townhomes. The overall density for the Project is 33.6 units per acre, which is significantly below the Comp Plan's guidance of up to 45 units. More importantly, the site density is necessary for the retail component to thrive. The density allows the retail rent to be manageable and provides the necessary customer base for retail that is now integrated into the building rather than stand-alone.

The Comp Plan also reveals that this level of density is in the best interests of the City as a whole. Specifically, the Comp Plan notes a need for substantially more housing within the City, especially housing that responds to changing housing preferences. At the same time, the Comp Plan recognizes the reality that there simply are few options with the City to meet the increased need. This reality is highlighted by the Housing Study, which shows Shoreview and surrounding communities currently have a 1,097-unit deficit in market-rate rental units to meet the demand of the next five years. It must also be noted that while there has been some concern raised about the impacts "renters" might have on the existing neighborhood, it simply is not a legitimate or factually correct statement. First, high density residential is compatible with other residential uses under the Comp Plan. Second, the Project is a market-rate rental unit designed for renters-by-choice. The rental rates will start at approximately \$1,000 per month and rise from there. Quite simply, the residents of the Project will be the very demographic that any city would be thrilled to attract.

The Property and the Project create a dynamic opportunity to meet the very housing needs that the City has identified in the Comp Plan. It is in the immediate vicinity (and in some places abuts) of a major freeway. Today, nearly half of the Property is zoned General Commercial, which would allow a more intense use and one that would have greater impacts on the existing single-family homes. The Property, or at least a very significant part of it, has been identified as TRA and each expert/consultant that has looked at the Property has come to the same conclusion: it should be developed as high-density mixed use.

Architectural Design/Building Height

How the Project responds to this comment likely needs no further explanation. For the sake of clarity, we will note that the design as submitted addresses this very point. The apartment building has been shifted away from the existing single-family homes with a 75-foot setback from the northern property line. The shading study shows that the current configuration has minimal impact on the home to the immediate north and little, if any, impact on the other existing homes. It should also be noted that the apartment building is mostly on a parcel currently zoned General Commercial. Under GC zoning regulations, we would be entitled to a construct a building with similar, if not greater, height.

Site Design

The Project directly responds to the comments about site design. All setbacks are met or greatly exceeded. Substantial green space has been incorporated and the Project is well below the 65% coverage limit.

Traffic

To respond to residents' traffic concerns, we retained Vernon Swing of WESTWOOD PROFESSIONAL SERVICES and consulted with the Ramsey County Public Works department. Ramsey County flatly stated that it would not allow an access point directly onto Rice Street, but it believed without question that Rice Street has the capacity to handle the Project. It also responded to the concern of "cut through" traffic on Rustic Place and agreed that the Project would not generate any such traffic.

Westwood's traffic study likewise finds that the Project will not have a significant impact. That is not to say that Rice Street is not busy and does not today face congestion. However, the Project is not going to significantly alter that fact. This is especially true in light of the reality that some development **must** occur on this corner. Comparing the Project's traffic generation to traffic generation from a hypothetical commercial-only use on the Commercial Lot (assuming the non-existent demand came to exist), the neighborhood and Rice Street are far less impacted by the Project than it would be under commercial only redevelopment.

CONCLUSION

As described in this letter the Applications, and supporting documents, EDG seeks the City's blessing to redevelop the Property into a coordinated mixed-use project that has been vetted, analyzed, reworked, and refined to a state-of-the-art project with market-rate rental townhomes, market-rate apartments, resident-focused retail, high-end amenities, and significant open space. We believe it is a project the City can (and should) be excited about for countless reasons. We certainly are, and we look forward to working with the City to make the plans a reality.

Of course, if you have any questions, comments, or concerns about this matter, please do not hesitate to contact me at your earliest opportunity. We are delighted to provide anything you need.

Very truly yours,



Michael J. Mergens

³ A copy of the County's November 10, 2015 letter is included for your convenience.

November 10, 2015

Kathleen Castle
City Planner
City of Shoreview
4600 N. Victoria St.
Shoreview, MN 55126

**ELEVAGE DEVELOPMENT GROUP LLC- PROPOSED REDEVELOPMENT OF THE NORTHWEST
QUADRANT OF RICE STREET (CSAH 49) AND COUNTY ROAD E**

Dear Kathleen:

We met earlier this week with representatives of the Elevage Development Group to discuss access issues related to their latest proposal for the site. The proposal shown to us split access between County Road E and Rice Street, with access to the proposed 14 townhomes being from County Road E and the remainder of the site, 154 apartment units and approximately 10,000 square feet of retail space, served by a direct access to Rice Street. The proposed access location on Rice Street is approximately 300 feet north of County Road E, which is unacceptable to Ramsey County.

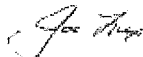
The proposed access location on Rice Street is too close to County Road E to fit in the turn lanes which would be necessary to provide safe access from a high-volume, relatively high-speed arterial roadway. More importantly, at peak traffic times, there would not be adequate gaps in traffic for drivers to enter Rice Street from this location. Ramsey County's position is that the intersection of County Road E and Rice Street, which is controlled by a traffic signal, will provide the capacity to safely serve this development. We do not see any reason why this would create any "cut-through" traffic in the neighborhoods to the north.

We also discussed whether a more northerly location for access to Rice Street would be acceptable. If an access point could be located at least 500 feet north of County Road E, there would be adequate space to construct left and right turn lanes outside of the influence area of the County Road E intersection. Full-movement access could be considered there, and access to the east could be considered, if adjacent properties in Vadnais Heights were redeveloped. A Traffic Impact Study would be needed to determine what accommodations are necessary for this intersection to function acceptably. All necessary improvements would need to be funded by the developer or the cities.

Elevage has made a significant investment in properties adjacent to the Rice Street/County Road E/Vadnais Boulevard intersection and we are encouraged by their commitment to making access to their properties functional and safe. We expect to see a revised concept from them soon and are confident it will address our concerns.

Please let me know if there are questions or concerns. We look forward to working with the city and the developers as this important project progresses.

Sincerely,



Joseph Lux
Senior Planner

C: Carie Fuhrman, Vadnais Heights Community Development Director

January 6, 2016

Kathleen Castle
City Planner
City of Shoreview
4600 N. Victoria St.
Shoreview, MN 55126

**REVISED ELEVAGE DEVELOPMENT GROUP LLC- PROPOSED REDEVELOPMENT OF THE
NORTHWEST QUADRANT OF RICE STREET (CSAH 49) AND COUNTY ROAD E**

Dear Kathleen:

We have reviewed the latest Elevage Development group proposal for the northwest quadrant of Rice Street and County Road E that you sent on January 5th, along with the traffic study that was done for the proposal.

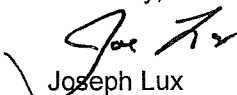
The proposed development and the access proposed to serve it are acceptable to Ramsey County. The traffic study that was submitted is very complete and useful in evaluating the effects of the proposed development as well as in evaluating potential traffic signal timing improvements.

We have the following comments:

- The traffic study clearly identifies that the existing issues in serving Rice Street traffic are primarily attributable to operational deficiencies at the Rice Street/I-694 interchange. Ramsey County, in cooperation with the Cities of Shoreview, Vadnais Heights, and Little Canada, has proposed a legislative funding package to complete the reconstruction of that interchange. We may also pursue federal Surface Transportation Program (STP) funding. Reconstruction of this interchange is the County's top transportation priority and we are committed to completing it. The traffic study correctly states that the earliest probable date for its reconstruction is 2018, if funding is obtained. If we are unsuccessful in getting funding this year, the project could be delayed.
- The traffic study evaluates two different development scenarios. Though the trip generation potential of each differs slightly, their impact on the Rice Street/County Road E/Vadnais Boulevard intersection would be of similar magnitude. We appreciate that the traffic study used the slightly higher AM peak hour volumes of Scenario I for its evaluation of AM peak hour operations.
- We recommend that the City evaluate sight distance from the proposed westerly access to County Road E to confirm that it is adequate to the west/northwest.

Thank you for the opportunity to review this proposal. We look forward to working with the City and with the developers as the project moves forward.

Sincerely,



Joseph Lux
Senior Transportation Planner

C: Mark Maloney, Shoreview Public Works Director
Carie Fuhrman, Vadnais Heights, Community Development Director
Jim Tolaas, Ramsey County Public Works Director
Erin Laberee, Ramsey County Traffic Engineer



Kathleen Castle <kcastle@shoreviewmn.gov>

Application Comment Form - Elevage Redevelopment

Lux, Joseph <Joseph.Lux@co.ramsey.mn.us>
To: Kathleen Castle <kcastle@shoreviewmn.gov>

Mon, Jan 11, 2016 at 8:25 AM

Hi, Kathleen:

Since no other access was shown on the drawings, I didn't comment on the possibility of any. No, we don't think another access would be acceptable- even with a right-turn lane, it would function as an in-only during the morning peak hours. We'd have to see a proposal to comment on it. We'd consider that a major change to the proposal and would need to review it separately from what was presented with these documents.

Regarding the sidewalk, I should have included in the comment letter that all work within the Rice Street right of way will require permits. Connecting the sidewalk to the short stretch of existing trail or replacing the trail with a new sidewalk would be a City decision and we'd defer to you on how you want that constructed. There's only about 175' of trail in place now, so whatever you think is appropriate, would be fine with us.

Joe Lux

Joseph Lux

Senior Planner

Ramsey County Public Works

1425 Paul Kirkwold Drive

Arden Hills, MN 55112-3933

651-266-7114

<http://www.ramseycounty.us/>



From: Kathleen Castle [mailto:kcastle@shoreviewmn.gov]

Sent: Friday, January 08, 2016 2:35 PM



LAKE JOHANNA FIRE DEPARTMENT

5545 LEXINGTON AVENUE NORTH • SHOREVIEW, MN 55126
OFFICE (651) 481-7024 • FAX (651) 486-8826

January 11th, 2016

Elevage Development Group, LLC
10901 Baltimore St NE
Blaine, MN 55449

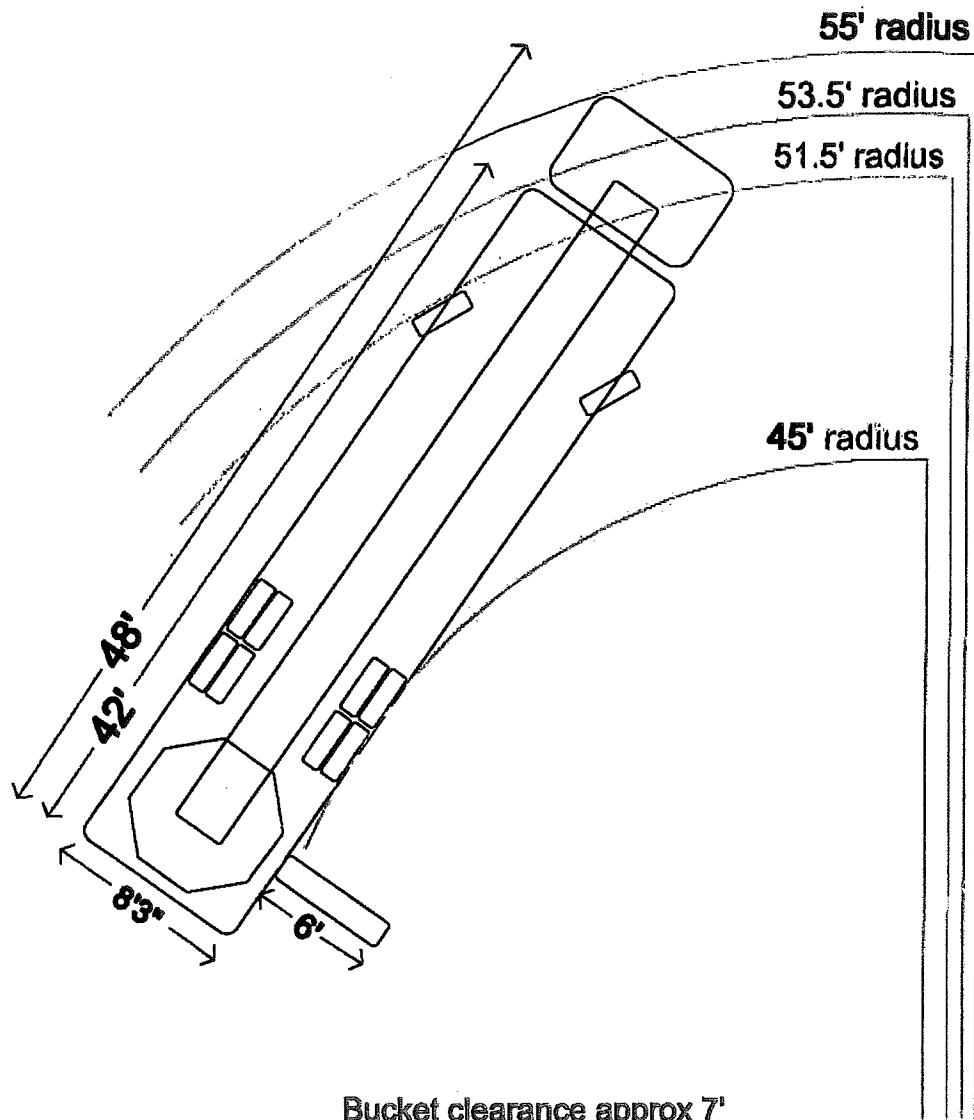
File No. 2602-18-01

- Lot 1
 - Provide an approved turnaround per Appendix D 103.4 of the Minnesota State Fire Code or a drive through to Rustic Place to alleviate the dead end (Appendix D provided)
 - Provide a hydrant at dead end
- Lot 2
 - Verify height for drive from street to lot 2 through building
 - Provide hydrant at front triangle island in lot
 - Verify radius for trucks in lot 2 according to specs provided
 - Hydrant located within 150' of FDC at front lobby location

Sincerely,

Nate Berg
Fire Marshal
Lake Johanna Fire Department

843 Statistics



Bucket clearance approx 7'
Bumper clearance approx 1.5'
Overall vehicle height approx 12' 1"
Overall vehicle weight approx 69,000 lbs

APPENDIX D

FIRE APPARATUS ACCESS ROADS

The provisions contained in this appendix are not mandatory unless specifically referenced in the adopting ordinance.

SECTION D101 GENERAL

D101.1 Scope. Fire apparatus access roads shall be in accordance with this appendix and all other applicable requirements of the *International Fire Code*.

SECTION D102 REQUIRED ACCESS

D102.1 Access and loading. Facilities, buildings or portions of buildings hereafter constructed shall be accessible to fire department apparatus by way of an approved fire apparatus access road with an asphalt, concrete or other approved driving surface capable of supporting the imposed load of fire apparatus weighing at least 75,000 pounds (34 050 kg).

SECTION D103 MINIMUM SPECIFICATIONS

D103.1 Access road width with a hydrant. Where a fire hydrant is located on a fire apparatus access road, the minimum road width shall be 26 feet (7925 mm). See Figure D103.1.

D103.2 Grade. Fire apparatus access roads shall not exceed 10 percent in grade.

Exception: Grades steeper than 10 percent as approved by the fire chief.

D103.3 Turning radius. The minimum turning radius shall be determined by the fire code official.

D103.4 Dead ends. Dead-end fire apparatus access roads in excess of 150 feet (45 720 mm) shall be provided with width and turnaround provisions in accordance with Table D103.4.

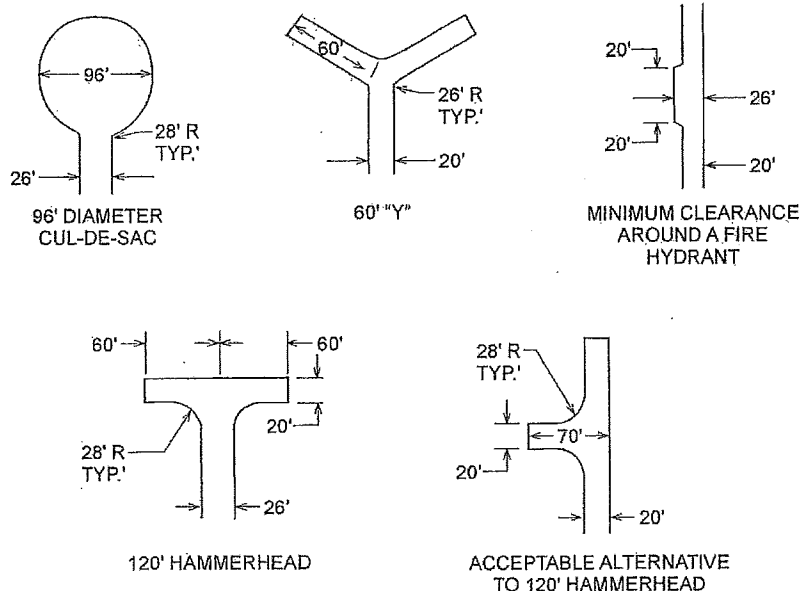
TABLE D103.4
REQUIREMENTS FOR DEAD-END FIRE
APPARATUS ACCESS ROADS

LENGTH (feet)	WIDTH (feet)	TURNAROUNDS REQUIRED
0-150	20	None required
151-500	20	120-foot Hammerhead, 60-foot "Y" or 96-foot-diameter cul-de-sac in accordance with Figure D103.1
501-750	26	120-foot Hammerhead, 60-foot "Y" or 96-foot-diameter cul-de-sac in accordance with Figure D103.1
Over 750		Special approval required

For SI: 1 foot = 304.8 mm.

D103.5 Fire apparatus access road gates. Gates securing the fire apparatus access roads shall comply with all of the following criteria:

1. The minimum gate width shall be 20 feet (6096 mm).



For SI: 1 foot = 304.8 mm.

FIGURE D103.1
DEAD-END FIRE APPARATUS ACCESS ROAD TURNAROUND

2. Gates shall be of the swinging or sliding type.
3. Construction of gates shall be of materials that allow manual operation by one person.
4. Gate components shall be maintained in an operative condition at all times and replaced or repaired when defective.
5. Electric gates shall be equipped with a means of opening the gate by fire department personnel for emergency access. Emergency opening devices shall be approved by the fire code official.
6. Manual opening gates shall not be locked with a padlock or chain and padlock unless they are capable of being opened by means of forcible entry tools or when a key box containing the key(s) to the lock is installed at the gate location.
7. Locking device specifications shall be submitted for approval by the fire code official.

D103.6 Signs. Where required by the fire code official, fire apparatus access roads shall be marked with permanent NO PARKING—FIRE LANE signs complying with Figure D103.6. Signs shall have a minimum dimension of 12 inches (305 mm) wide by 18 inches (457 mm) high and have red letters on a white reflective background. Signs shall be posted on one or both sides of the fire apparatus road as required by Section D103.6.1 or D103.6.2.

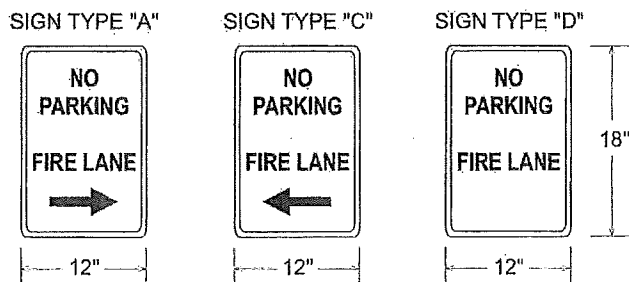


FIGURE D103.6
FIRE LANE SIGNS

D103.6.1 Roads 20 to 26 feet in width. Fire apparatus access roads 20 to 26 feet wide (6096 to 7925 mm) shall be posted on both sides as a fire lane.

D103.6.2 Roads more than 26 feet in width. Fire apparatus access roads more than 26 feet wide (7925 mm) to 32 feet wide (9754 mm) shall be posted on one side of the road as a fire lane.

SECTION D104

COMMERCIAL AND INDUSTRIAL DEVELOPMENTS

D104.1 Buildings exceeding three stories or 30 feet in height. Buildings or facilities exceeding 30 feet (9144 mm) or three stories in height shall have at least three means of fire apparatus access for each structure.

D104.2 Buildings exceeding 62,000 square feet in area. Buildings or facilities having a gross building area of more than 62,000 square feet (5760 m²) shall be provided with two separate and approved fire apparatus access roads.

Exception: Projects having a gross building area of up to 124,000 square feet (11 520 m²) that have a single approved fire apparatus access road when all buildings are equipped throughout with approved automatic sprinkler systems.

D104.3 Remoteness. Where two access roads are required, they shall be placed a distance apart equal to not less than one half of the length of the maximum overall diagonal dimension of the property or area to be served, measured in a straight line between accesses.

SECTION D105

AERIAL FIRE APPARATUS ACCESS ROADS

D105.1 Where required. Buildings or portions of buildings or facilities exceeding 30 feet (9144 mm) in height above the lowest level of fire department vehicle access shall be provided with approved fire apparatus access roads capable of accommodating fire department aerial apparatus. Overhead utility and power lines shall not be located within the aerial fire apparatus access roadway.

D105.2 Width. Fire apparatus access roads shall have a minimum unobstructed width of 26 feet (7925 mm) in the immediate vicinity of any building or portion of building more than 30 feet (9144 mm) in height.

D105.3 Proximity to building. At least one of the required access routes meeting this condition shall be located within a minimum of 15 feet (4572 mm) and a maximum of 30 feet (9144 mm) from the building, and shall be positioned parallel to one entire side of the building.

SECTION D106

MULTIPLE-FAMILY RESIDENTIAL DEVELOPMENTS

D106.1 Projects having more than 100 dwelling units. Multiple-family residential projects having more than 100 dwelling units shall be equipped throughout with two separate and approved fire apparatus access roads.

Exception: Projects having up to 200 dwelling units may have a single approved fire apparatus access road when all buildings, including nonresidential occupancies, are equipped throughout with approved automatic sprinkler systems installed in accordance with Section 903.3.1.1 or 903.3.1.2.

D106.2 Projects having more than 200 dwelling units. Multiple-family residential projects having more than 200 dwelling units shall be provided with two separate and approved fire apparatus access roads regardless of whether they are equipped with an approved automatic sprinkler system.

**SECTION D107
ONE- OR TWO-FAMILY RESIDENTIAL
DEVELOPMENTS**

D107.1 One- or two-family dwelling residential developments. Developments of one- or two-family dwellings where the number of dwelling units exceeds 30 shall be provided with separate and approved fire apparatus access roads and shall meet the requirements of Section D104.3.

Exceptions:

1. Where there are more than 30 dwelling units on a single public or private fire apparatus access road and all dwelling units are equipped throughout with an approved automatic sprinkler system in accordance with Section 903.3.1.1, 903.3.1.2 or 903.3.1.3.3, access from two directions shall not be required.
2. The number of dwelling units on a single fire apparatus access road shall not be increased unless fire apparatus access roads will connect with future development, as determined by the fire code official.



Minnesota Department of Transportation

Metropolitan District

Waters Edge Building
1500 County Road B2 West
Roseville, MN 55113

January 13, 2016

Kathleen Castle
City Planner
4600 North Victoria St
Shoreview, MN 55126

SUBJECT: P16-002 Shoreview mixed use development
NW quad of I694 and Rice Street
Shoreview, Ramsey County
Control Section: 6285

Dear Ms. Castle:

The Minnesota Department of Transportation (MnDOT) has reviewed the above referenced plat in compliance with Minnesota Statute 505.03, subdivision 2, Plats. Before any further development, please address the following issues:

Right of Way:

The MnDOT right of way should be shown and labeled specifically on the plans. A small portion of County Rd. E. and also Rustic Place are within the existing MnDOT right of way. This information needs to be identified because any work performed within the MnDOT right of way will require a permit. A portion of the existing right of way has been depicted on MnDOT right of way Plat No. 62-67. The remainder of the right of way can be found on R/W Map No. 17-60.

For questions on these comments, please contact Matt Aguirre at 651-234-7599.

Permits:

Any use of or work within or affecting MnDOT right of way requires a permit. Permit forms are available from MnDOT's utility website: <http://www.dot.state.mn.us/utility/forms.html> . Please include one 11x17 plan set and one full size plan set with each permit application. Please direct any permitting questions to Jeff Dierberger (651-234-7916) of MnDOT's Metro Permits section.

Review Submittal Options:

MnDOT's goal is to complete the review of plans within 30 days. Submittals sent in electronically can usually be turned around faster. There are four submittal options. Please submit either:

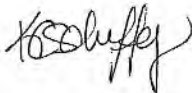
1. One (1) electronic pdf. version of the plans. MnDOT can accept the plans via e-mail at metrodevreviews.dot@state.mn.us provided that each separate e-mail is under 20 megabytes.
2. Three (3) sets of full size plans. Although submitting seven sets of full size plans will expedite the review process. Plans can be sent to:

MnDOT – Metro District Planning Section
Development Reviews Coordinator
1500 West County Road B-2
Roseville, MN 55113

3. One (1) compact disc.
4. Plans can also be submitted to MnDOT's External FTP Site. Please send files to: <ftp://ftp2.dot.state.mn.us/pub/incoming/MetroWatersEdge/Planning> Internet Explorer doesn't work using ftp so please use an FTP Client or your Windows Explorer (My Computer). Also, please send a note to metrodevreviews.dot@state.mn.us indicating that the plans have been submitted on the FTP site.

If you have any questions concerning this review, please contact me at (651) 234-7784.

Sincerely,



Karen Scheffing
Principal Planner

Copy sent via E-Mail:

Buck Craig, Permits
Jeff Dierberger, Permits
Bruce Irish, Water Resources
Matt Aguirre, Right of Way
Gayle Gedstad, Traffic
Clare Lackey, Traffic
Mark Lindeberg, Area Engineer
Greg Gund, Ramsey County Surveys, Gregg.gund@co.ramsey.mn.us
Russ Owen, Metropolitan Council



Kathleen Castle <kcastle@shoreviewmn.gov>

Elevage

1 message

Elizabeth Decker <tazdeck@hotmail.com>

Thu, Jan 21, 2016 at 12:16 PM

To: "kcastle@shoreviewmn.gov" <kcastle@shoreviewmn.gov>

I am a lifelong Shoreview resident, having returned to the area after college. It has always been quiet, pleasant, desirable. Upon my return 6 years ago I have been disappointed in some of the changes that significantly lower the standard of living and home values here. If you want Shoreview to remain desirable with decent people, higher home median value (tax revenue for you), and a high quality of life then you in particular have a chance to keep it that way.

New concerns:

- 1) rail noise, pollution of our water shed, traffic i.e. switching
- 2) new multi-family builds (like Co.Rd.E/Victoria)
- *3) the incorrect PC thinking that "mixed use family" developments are good for anyone

A friend of mine designed and built one in Mpls just off 94/55 it is full of people who destroy the property, disrespect each other, and of course no one who can afford to own wants to pay more to live near them. We already have a low rent apartment complex in our area at Rice/Gramsie, we do not need to invite more trouble.

It is obvious that this will fail over time, cost Shoreview \$, and make everyone near the project miserable. We the residents are unhappy about this decision. Unless you yourself plan to move there do not ask the rest of us to live next to it and put up with it. Please consider the long term cost of what you are entertaining and do the right thing for those of us who already live here, pay you tax revenue, and take good care of the city we know and love!!

Sincerely Verklempt!



Kathleen Castle <kcastle@shoreviewmn.gov>

Elevage Development Group Proposal

1 message

Keith Johnson <k.cardigan.johnson@gmail.com>

Thu, Jan 21, 2016 at 6:55 AM

To: kcastle@shoreviewmn.gov

Ms. Castle,

Concerning the plan for development of County Road E and Rice Street (File No. 2602-16-01), we have the following comments:

The plan originally submitted to the Planning Commission in the fall of 2015 called for a four and five story building with just over 100 units plus commercial space. One of the commissioners at the first hearing called that plan "too intense" for the site.

The latest plan calls for a building entirely of five stories with 137 units plus commercial space. 14 townhomes have been added as well. We fail to see how this plan is less intense than the original plan.

The original plan had two access points, one to Rice Street, and one to County Road E. Although this plan has two access points, both are to County Road E, and one of the two is for the exclusive use of the 14 townhomes. This arrangement will undoubtedly cause traffic problems at the Rice/County Road E exchange. We have seen frequent violations of traffic laws in the area by drivers frustrated with the already heavy traffic. This plan will aggravate the situation.

At the first hearing, a police officer who lives in the neighborhood warned that any building placement that screens the parking area from Rice Street and County Road E would attract crime. This plan places the building poorly from that perspective.

For these reasons we request the Commission to deny the plan.

Keith and Carolyn Johnson
3695 Rustic Place



Kathleen Castle <kcastle@shoreviewmn.gov>

Elevage Development Concerns for meeting

1 message

chris@truebluebuilders.org <chris@truebluebuilders.org>

Thu, Jan 21, 2016 at 10:33 AM

To: kcastle@shoreviewmn.gov

Cc: Teri Downing <Teri32168@gmail.com>

To Whom it may concern:

Everyone has concerns about this proposal for a multitude of reasons, our focus is safety. Safety for our family, our neighbors, and our guests.

Rice st is already a crazy busy street teaming with impatient commuters, large trucks, buses, bicyclists, pedestrians and animals, all fighting for a precious piece of pavement. The street is saturated. Drivers do not obey speed signs, road restriction, or street signs about shoulder passing. We are constantly swerved around and honked at as we pull into our driveway, and we take our lives in our hands crossing the street. Our kids have to catch the school bus across the street on Rustic place and that in itself is a daily adventure.

People and pets have been killed right in front of our homes, and I can only imagine the effect a high density development will have on the safety of our family and guests. There needs to be major road changes and restrictions, bike and walking paths, increased lighting etc... before even a scaled down version of this proposal should be considered.

A jogger, dog walker, biker, skateboarder, or child waiting for a bus will be seriously injured or worse if this proposal passes without significant changes to the volume of land use, and major road restrictions and improvements. This current proposal is irresponsible, shortsighted, and dangerous.

We look forward to any development that enhances the quality of life for ourselves, our neighborhood, and our city.

Thank You,

The Downings
3590 Rice St

Dec 8, 2015

Mayor Sandy Martin

RE: Elevage proposal for Shopping Center(Co Rd E/Rice St)

Enclosed is a copy of the concerns that Sharon & I have with this project. We feel this revised proposal will not improve our area that we are all so proud to be a part of. Please review these concerns and ask yourselves if this high density shopping center proposal blends in with this low density neighborhood and with all the surrounding areas – including Shoreview, Vadnais Heights, and Little Canada. Add to this the horrific traffic problem this proposal would create for not only the residents commuting problems but all who use Hwy49/Rice St/694 intersection as part of their commute.

Thank you for taking the time to review our comments.

Richard & Sharon Braun

3535 Rustic Pl

Shoreview, MN 55126



ELEVAGE'S SHOPPING CENTER RENOVATION PROPOSAL – 2ND REVISION

NOV 24, 2015

OUR MAIN CONCERNS WITH THIS 2ND REVISION:

1) A PROPOSED HIGH DENSITY 6 STORY APARTMENT COMPLEX & 2&1/2 STORY TOWNHOUSES DOESN'T FIT THE LOW DENSITY RESIDENTIAL NEIGHBORHOOD

A) 150 APARTMENT UNIT WITH 172 UNDERGROUND PARKING STALLS & 85 SHARED PARKING SPACES W/RETAIL & RESTAURANT:

- 1) 1ST FLOOR 20FT HIGH CAR/TRUCK PORTAL NECESSARY FOR RETAIL BUSINESS DELIVERIES – MAKING THIS A 7 STORY BUILDING (NOT 6).
- 2) ALL PARKING (UNDERGROUND & SURFACE) WILL FUNNEL INTO ONE IN-&-OUT ROAD UNTO OC RD E – POSSIBLE CONGESTION – NEEDS TO BE ADDRESSED.
- 3) REVISED PROPOSAL WILL BLOCK DIRECT SUNLIGHT (LATE NOVEMBER- EARLY FEBRUARY) – NEEDS TO BE ADDRESSED
- 4) TOWNHOUSE RENTER RD LEADING TO CO RD E IS CLOSE TO STOP SIGN ON CO RD E & RUSTIC PL – AQ PARTIAL BLIND SPOT – NEEDS TO BE ADDRESSED.
- 5) 14 TOWNHOMES – PARALLEL TO RUSTIC PL (7 FACING EAST & 7 FACING WEST ON RUSTIC PL) – RENTERS PARKING IS PROVIDED FOR ALL RENTERS ON THE EAST SIDE. PROBLEM: SINCE 7 FACE RUSTIC PL, RENTERS OR VISITORS PARKING ON RUSTIC PL HAVE TO ABIDE BY THE SAME RULES AS RESIDENTS (NO PARKING ON STREET BETWEEN 2AM -5AM). WHO WILL ENFORCE THIS? ARE RESIDENTS GOING TO BE RESPONSIBLE FOR THIS?

NOTE: ADD TO ALL OF THIS, THE SHOPPING MALL HAS AN 8FT RETAINING WALL & A PRIVACY FENCE BEHIND THE WALL. THIS WALL PLUS TO 3520 RUSTIC PL'S YARD MAKES TOWNHOUSES 3&1/2 (NOT 2&1/2) STORIES HIGH. JUST FOUND OUT THAT THERE IS A DRAIN IN THE BACK OF 185 CO RD E (HOME WAS PURCHASED BY ELEVAGE). BOTH THINGS NEED TO BE ADDRESSED & CLARIFIED.

B) DOES ELEVAGE HAVE THE APPROVAL FOR DEMOLITION OF THE SHOPPING CENTER YET? WE UNDERSTAND THAT THIS CAN BE DONE BEFORE THE APPROVAL OF THE PROJECT IS GRANTED – IS THAT CORRECT?

2) THIS PROPOSED PROJECT CREATES MAJOR TRAFFIC PROBLEMS:

A) RAMSEY CTY IS AWARE OF OUR CURRENT RICE ST TRAFFIC PROBLEMS – CO RD E/RICE ST LIGHT PATTERNS NEED CHANGING. THE TRAFFIC INCREASE OF THIS PROPOSED SHOPPING CENTER PROJECT WILL AFFECT OUR NEIGHBORHOOD'S MOBILITY.

B) WHEN SHARI, MYSELF, & 2 NEIGHBORS MET WITH MICHAEL MERGENS & ASKED ABOUT A TRAFFIC STUDY, MICHAEL INDICATED THIS STUDY WOULD BE DONE BY THE NEXT NEIGHBORHOOD MEETING. AT THE NOV 24TH MEETING, WHEN I ASKED THEM ABOUT THE TRAFFIC STUDY, THEY SAID THIS STUDY HAS TO BE DONE BEFORE THIS PLAN IS SUBMITTED TO THE PLANNING COMMISSION (NOT REALLY ANSWERING WHY IT WASN'T DONE).

C) AT THAT SAME MEETING, ELEVAGE POINTED OUT THE HOMES PURCHASED WERE IN HORRIBLE CONDITION (COMMENTS WERE DOWNGRADING OUR AREA – NOT APPRECIATED). THEY WERE SARCASTIC TO ANYONE ASKING QUESTIONS SO THAT FEWER PEOPLE ASKED QUESTIONS. ELEVAGE SAID THEY DIDN'T NEED TO HAVE THESE NEIGHBORHOOD MEETINGS BUT WANTED OUR INPUT!!

D) THERE ARE OTHER PROJECTS THAT SHOULD BE ADDRESSED THAT MAY AFFECT THIS PROJECT:

- 1) SOUTHEAST CORNER OF RICE ST/COUNTRY DRIVE, LITTLE CANADA – BUILDING OF SENIOR HOUSING IS IN PROCESS.
- 2) NORTHEAST CORNER OF RICE ST/VADNAIS BLVD IN VADNAIS HEIGHTS ELEVAGE IS WORKING ON A BUILDING PROPOSAL.
- 3) DEVELOPMENT OF SOUTHWEST CORNER (694&N OWASSO BLVD IF RAMSEYCTY SELLS THIS AREA.
- 4) MAJOR 694/RICE ST ROAD & BRIDGE PROJECT (HAS NO DEFINITE PROJECT START DATE).

NOTE: 3 OF THE 4 PROJECTS AFFECT TRAFFIC PERMANENTLY- THE 694 ROAD & BRIDGE PROJECT IS THE 694 FREEWAY CORRIDOR EXPANSION.

FINALLY, WE (SHARON & I) FEEL THIS PROJECT PROPOSAL OF ELEVAGE WILL DESTROY OUR NEIGHBORHOOD AS WE KNOW IT AND REDUCE THE SELLABLE VALUE OF OUR HOMES. THIS NEIGHBORHOOD IS NOT A NORMAL 8 YEAR TURNAROUND ONE. AS YOU KNOW, THIS IS A VERY UNIQUE AREA – ONCE ANYONE BUYS A HOME HERE, THEY STAY HERE. WHEN A HOUSE GOES UP FOR SALE, FRIENDS & RELATIVES USUALLY BUY INTO THIS AREA. SHARON & I HAVE BEEN HERE FOR 42 YEARS!! WE PARTICIPATE IN SHOREVIEWS ACTIVITIES – USE THE COMMUNITY CENTER, SUPPORT THE FARMERS MARKET, GO TO SUMMER CONCERTS AND HAVE BEEN NEIGHBORHOOD WATCH CAPTAINS FOR MANY YEARS. WE LOVE IT HERE & DON'T WANT TO LOSE OUR NEIGHBORHOOD!!!!

NOTE: I WENT ON THE INTERNET & FOUND A CHECKLIST FOR HOME BUYERS – PRIORITIZED AS FOLLOWS – 1) HOME FEATURES YOU'RE LOOKING FOR, 2) NEIGHBORHOOD, 3) SCHOOLS, & 4) CONVENIENCE TO SCHOOLS, WORK, SHOPPING, ETC. "NEIGHBORHOOD" RATES TRAFFIC, NOISE LEVEL, SAFETY, MIX OF INHABITANTS, PARKING, ZONING REGULATIONS, POLICE, FIRE PROTECTION, ETC. THE POTENTIAL BUYER SHOULD RATE EACH HOUSE & COMPARE BEFORE MAKING THEIR DECISION.

THIS SHOPPING CENTER PLAN (AS IT NOW IS) WILL AFFECT OUR FUTURE HOME VALUES WHEN WE WANT TO SELL. THAT IS ALSO OUR (SHARON & MY) CONCERN!!!

ELEAVAGE'S SHOPPING CENTER RENOVATION PROPOSAL – 3rd REVISION JAN 14, 2016

OUR MAIN CONCERNS WITH THIS 3rd REVISION:

1) A PROPOSED HIGH DENSITY 5 STORY APARTMENT COMPLEX & 2&1/2 STORY TOWNHOUSES DOESN'T FIT THE LOW DENSITY RESIDENTIAL NEIGHBORHOOD

A) 134 APARTMENT UNIT WITH 172 UNDERGROUND PARKING STALLS & 85 SHARED PARKING SPACES W/RETAIL & RESTAURANT:

- 1) 1ST FLOOR 20FT HIGH CAR/TRUCK PORTAL NECESSARY FOR RETAIL BUSINESS DELIVERIES – 5 STORY BUILDING
- 2) ALL PARKING (UNDERGROUND & SURFACE) WILL FUNNEL INTO ONE IN-&-OUT ROAD UNTO OC RD E – POSSIBLE CONGESTION – NEEDS TO BE ADRESSED.
- 3) REVISED PROPOSAL WILL BLOCK DIRECT SUNLIGHT (LATE NOVEMBER- EARLY FEBRUARY) – NEEDS TO BE ADDRESSED
- 4) TOWNHOUSE RENTER RD LEADING TO CO RD E IS CLOSE TO STOP SIGN ON CO RD E & RUSTIC PL – A PARTIAL BLIND SPOT – NEEDS TO BE ADDRESSED.
- 5) PARKING WILL OVERFLOW ONTO RUSTIC PL. IN REALITY THIS WILL HAPPEN—HOW WILL THAT BE ADDRESSED? OUR STREET IS NARROW AND WITHOUT SIDEWALKS. THIS WILL CAUSE A PROBLEM FOR SCHOOL BUSES, EMERGENCY VEHICLES, AND PEDESTRIANS. IT WILL BLOCK THE NORMAL TRAFFIC-FLOW FOR OUR RESIDENTS.
- 6) DOES A TRAFFIC STUDY NEED TO BE DONE BEFORE THE PLANNING COMMISSION MEETING ON THE 26th? IF ONE HAS BEEN DONE, IT WILL PROVE THAT THIS AREA WILL BE VERY CONGESTED. WE ASK THE PLANNING COMMISSION NOT CONSIDER THE DEVELOPMENT PLAN UNTIL A TRAFFIC STUDY HAS BEEN COMPLETED.
- 7) WE ARE AGAINST;
 - A. REZONING TO MIXED USE. AS THE PLANNING COMMISSION STATED BEFORE, ANY PROPOSAL WILL NOT ALTER THE ESSENTIAL CHARACTER OF OUR RESIDENTIAL NEIGHBORHOOD. IF ZONING IS CHANGED TO MIXED USE, IT WILL ALLOW A HIGH DENSITY DEVELOPMENT IN A LOW DENSITY NEIGHBORHOOD.
 - B. WE ARE AGAINST SHARING OUR WATER AND SEWER WITH 148 FAMILIES. WILL MORE USERS AFFECT OUR WATER PRESSURE?
 - C. WHY IS THERE A NEED FOR A LARGE SCALE APARTMENT BUILDING IN THIS AREA OF SHOREVIEW? A MARKET STUDY WOULD SHOW WHERE AND WHAT TYPE OF HOUSING IS NEEDED. WE ARE AGAINST A 5 STORY APARTMENT BUILDING. WHY NOT CONSIDER A ONE LEVEL RETAIL SPACE WITH A RESTAURANT (NOT A BAR OR SPORTS BAR) WITH 2-LEVEL TOWNHOUSES.
- 8) ANOTHER CONCERN; HOW WILL SNOW REMOVAL BE HANDLED.
- 9) WE WANT AN ARCHITECTURAL DRAWING TO SHOW SURROUNDING RESIDENCES TO GAIN A PROPER PERSPECTIVE. IT WILL SHOW THAT THIS HIGH-DENSITY DEVELOPEMENT WILL NOT FIT IN OUR LOW DENSITY RESIDENTIAL NEIGHBORHOOD.

10) WE REQUEST AN ECONOMIC STUDY. WE WOULD LIKE TO KNOW ABOUT PREVIOUS LARGE SCALE DEVELOPMENTS IN EXISTING NEIGHBORHOODS.

IN CONCLUSION, THIS VERY UNIQUE RUSTIC PLACE –ST. MARIE NEIGHBORHOOD, WITH LARGE LOTS, IS A MIX OF RETIREES AND YOUNG FAMILIES, WHERE NEIGHBORS HELP NEIGHBORHORS. AS NEIGHBORHOOD WATCH CAPTAINS, WE EXPERIENCE THIS CLOSENESS THAT IS FOUND IN THIS NEIGHBORHOOD. OUR GOAL IS TO KEEP THIS QUALITY OF LIFE THAT WE HAVE. EVER SINCE THIS PROPOSED REDEVELOPEMENT OF THE 4.1 ACRES(COUTY RD. E-RICE STREET SHOPPING CENTER AND THREE RESIDENTIAL PROPERTIES) WAS INTRODUCED IN JULY 2015, WE TRIED TO FIND THE POSITIVES IN THESE PROPOSED CHANGES (CHANGE IS ALWAYS DIFFICULT). WE MET WITH THE MAYOR EXPRESSING OUR CONCERNS-LOOKING FOR POSITIVES OUT OF THE NEGATIVES EFFECTING OUR QUALITY OF LIFE. WE HAVE HEARD MANY TIMES FROM THE PLANNING COMMISION MEETINGS THAT THESE CHANGES WILL NOT ALTER THE ESSENTIAL CHARACTER OF THE NEIGHBORHOOD. OUR QUALITY OF LIFE INCLUDES OUR SAFETY, CONGESTION DUE TO INCREASED TRAFFIC, WATER AND SEWER PROBLEMS, PARKING, MOBILITY IN AND OUT OF OUR AREA, LACK OF PRIVACY, ETC. THIS THIRD REVISION AS IT IS, WILL EFFECT THE ESSENTIAL CHARACTER OF OUR NEIGHBORHOOD. CHANGING THE ZONING TO MIXED USE WOULD DESTROY THIS UNIQUE NEIGHBORHOOD.

THANK YOU

RICHARD AND SHARON BRAUN
3535 RUSTIC PLACE
SHOREVIEW, MN. 55126

NOTEWORTHY: WE FOUND A CHECKLIST ON THE INTERNET FOR HOMEBUYERS, PRIORITIZED AS FOLLOWS;

- 1) HOME FEATURES YOU ARE LOOKING FOR.
- 2) NEIGHBORHOOD- WHICH RATES TRAFFIC, NOISE LEVEL, SAFETY, MIX OF INHABITANTS, PARKING, ZONING REGULATIONS, POLICE, FIRE PROTECTION, ETC.
- 3) SCHOOLS
- 4) CONVENIENCE TO SCHOOL, WORK, SHOPPING, ETC.

PLEASE NOTE THAT ITEMS IN THE “NEIGHBORHOOD” SECTION, ARE WHAT ALL OF US IN THE RUSTIC PLACE – ST. MARIE NEIGHBORHOOD ARE CONCERNED ABOUT.
IF THIS PROPOSAL GOES THROUGH AS IS, IT COULD EVENTUALLY AFFECT THE EQUITY OF OUR HOMES.

Request for Comment

Elevage Development Group, LLC

157 County Road E, 185 County Road E, 3521 Rice Street and 3500 Rustic Place

File No. 2602-16-01

Thank you for taking the time to read through the comments sent in by the residents of the area that will be most affected by this proposed development. As time is a commodity coveted by all, I will attempt to not waste yours.

Along with my three children, I have attended all of the meetings for this proposed development, including the previous Planning Commission meeting, the City Council meeting and the two Neighborhood meetings held by the development group. Our family looks forward to the *responsible* redevelopment of this corner and would like to submit the following comments for the application consideration:

Height – This was an issue for the previous plan, it continues to be. Responsible development should not only fit the nature of the area but of Shoreview as a whole. This building is still too big.

Green Space – I commend the architectural firm for the lovely effort to make the surrounding grounds more beautiful than the concrete of the past proposal. We love the proposed walking paths. I do feel, however, that instead of the Commission's recommendation to introduce more green space AND address the height issue, the developers have given beautiful green space and INCREASED the height and units to "pay for it" - a bit of a bait and switch.

Cars Through the Neighborhood – As the complex now has to use Cty. E instead of Rice Street, I, as a mother, am already in a panic about the sheer volume of cars that will be using the neighborhood roads when they are unable to get out of the complex to the left to access Rice Street. Our family lives on the "straight stretch," which is already an issue. We ask that some solutions be brainstormed and put in place (speed bumps?) because once the developers build and leave, we will be left to deal with the consequences.

I realize our neighbors have submitted various other valid concerns, but the above are my family's "highlights." My last comment is this (please don't tune out quite yet):

A developer's goal is to make money. These apartments (according to Elevage) will be premium and for corporates and millennials.

Our family's goal is to be a blessing to others. Our family bought a home in this neighborhood of Shoreview so that we could have trees, good schools and room to roam.

The City of Shoreview's goal (among others) is to help these two disparate, yet equally valuable life pursuits meet in the middle. I have to believe it can be done.

Thank you for your valuable time and I look forward to the meeting on the 26th.

Anna Schaberg (and Eric, Rhea, Eli and Katherine)

3577 Rustic Place, Shoreview

Since receiving the information about the new proposal to replace the Shopping Center on Cty Rd E and Rice St we have been mulling over thoughts of how this development might add to the Rustic Place/Rice St neighborhood. After much thought and consideration of this proposal we have not been able to come up with one reason that this might enhance our neighborhood, enrich our quality of life, or be an asset to Shoreview.

There are many reasons this proposal has a negative impact. You are already aware of the Rustic place neighborhood and our concerns. Most of the families that live here have been here a long time and realize the uniqueness of this area. We are a strong knit community of 2 blocks, approximately 50+ homes, with residents that are here to watch, protect, and help each other. We feel safe and secure in our neighborhood. Many of our elder residents are in their seventies and eighties and continue to live in their homes due to the support from the neighborhood. This development will dramatically change what we have. The increase of 148 families (134 apartments and 14 townhomes) in this two block area plus businesses, that are up for grabs right now, will change the way of life we are accustomed to.

Our concerns: Safety in our homes, our yards, and on our streets. Increase of traffic on our streets, taking away our privacy and quality of life. Increase of traffic at Cty Rd E and Rice St which will add more congestion to an already congested area. Sewer and water problems could arise due to the additional apartments and townhomes. Snow being removed from the premises and being put where? There will be parking on the streets that are already narrow making it more congested for the residents. The neighbors close to this development have all of these concerns multiplied. The reduction of sunlight and the change of the air flow would negatively affect the structure of their homes and their vegetation. There is an 8 ft wall at 3520 Rustic place. Any type of grading will negatively impact this home.

Shoreview has always prided itself as being a "suburb in the woods:" a look of spaciousness with large lots. Our 2 block area is very much in sync with this feel and look. When considering developments, Shoreview is known for reducing the impact on the neighborhood and not altering the essential character of the neighborhood. Replacing the shopping center and 3 homes with a five story apartment building and 14 townhomes is exceeding the current population by a factor of 3. This is completely out of character for this neighborhood and completely out of character with the presented vision of Shoreview.

How does this development enhance our neighborhood or our quality of life?

Is this development needed in the community? Is it good for the neighborhood? Will it be an asset to both community and neighborhood? Consider the neighborhood and the impact this development would have on the Rustic Place/Rice St residents. We know that changes need to be made but this is too dramatic. This proposed development along with a potential bar/sports bar should not be considered for 4.1 acres of land that is adjacent to a residential area. We are against rezoning to Multiple Use.

The following studies are being requested: **Traffic**, **Reduction of Sunlight** on adjoining properties, **Air Flow** to ensure the air flow will not be altered from what currently exists, **Economic Study** showing impact to private residence when large scale development is incorporated into existing neighborhoods, and **Market Analysis** showing a need for additional apartments in this area. All the previous to be done before a decision is made on this project.

Don and Jan Bunde
3681 Rustic Place
Shoreview MN 55126



Kathleen Castle <kcastle@shoreviewmn.gov>

Elevage Development Group's Planned Unit Development Proposal, January, 20161 message

Marcia Figus <marciafigus@hotmail.com>

Sun, Jan 17, 2016 at 5:59 PM

To: KATHLEEN CASTLE <kcastle@shoreviewmn.gov>

Comment on the Elevage Development Group's Planned Unit Development Proposal, January, 2016

1. We request that if a Traffic Study hasn't been done to date that it be done before the 694 west ramp is closed in order to get accurate data.
2. We request that a Study of the reduction of Sun Light be done on the properties adjoining the development. The development should not cause a reduction in direct sun light to the existing neighboring homes.
3. We request that an Air Flow Study be done to insure the homes adjacent to this development will not have stagnant pockets with no air flow.
4. We request that an Economic Study be done on the impact to private residence when large scale development is incorporated into existing neighborhoods.
5. We request a Market Analysis be done to show the need for additional apartments in this area.

After the data is synthesized and analyzed, the Planning Commission can entertain what type of structure can work on the 4.1 acres without negatively impacting the "essential character of the neighborhood."

Rice St/ Rustic Pl/ Cardigan Junction Committee

(Marcia Figus)

Comment on the Elevage Development Group's Planned Unit Development Proposal, January 15, 2016

The land to be developed is 4.1 acres. It isn't flat land. The shopping mall was built on a hill such that the homes bordering it (3521 Rice St and 3520 Rustic Place) are at a lower elevation. In fact, there is an 8 ft support wall extending 200 ft along 3520 Rustic Place. If the land is filled in, 3520 will have a wall which will restrict air flow and reduce natural light. This inevitably sets up the property for future problems. Can Shoreview knowingly allow a home to incur future damage by permitting such a development? The height of the 14 town homes will exceed the 2 story homes in the area. It is completely out of character for this neighborhood. It is a solid wall overshadowing the homes to the west and the north. The townhomes will reduce the sun and the airflow inflicting future damage to the neighboring homes and their landscaping. Insects and mold will develop in this moist climate. Years of nurtured gardens will be destroyed because the natural growth environment will be altered. The rainfall will be redirected, the sun will be reduced to nonexistent, the sun cannot dry out areas, nor will the natural air current assist in the drying. This situation will continue to the other homes as you move to the north and west at a lesser amount, but it will still impact the homes. The 5 story apartment building and the 14 town homes will overshadow the neighboring area.

At Planning Commission Meetings we have heard over and over again: "this will not alter the essential character of the neighborhood." If that is the premise that decisions have been made in the past, that premise should continue. This Development will significantly alter the "essential character of the neighborhood." Our quality of life will be dramatically changed. The population density will increase 300%, traffic issues will become worse than they are now, parking could overflow onto Rustic which is a narrow street or onto Rice Street, loss of direct sunlight through the winter months or no direct sunlight at all, loss of feeling safe in our homes, loss of security for our home and our person, loss of privacy, invasion of our personal space in our own yards because of balconies/ windows on a 5 story building looking down at us. How can we enjoy being outside anymore? Feeling safe on the street? Will we need to lock our car doors in our own driveway? Lock our house door when in the backyard? We'll no longer know who belongs in the area. We'll no longer know if a car is a future danger as it moves down the street slowly. Today, we know who belongs and which cars belong in this neighborhood.

Issues of sewer, snow and water are a concern. Could the overuse by the addition of 134 apartments and 14 townhomes cause the sewer system to back up in our homes? Would Shoreview cover that damage to our homes because of this? It won't be an act of God but an act of Shoreview. Will our water pressure be affected by the additional users? Where will the snow be plowed? In a normal winter, the height of the snow is quite formidable. Will they contain it in their own parking lot or push it over onto 694 as was done this year? If it is contained on their land, they will lose parking places and where will occupants of the apartments and townhomes park?

David Graham, of ESG Architectural Firm, stated in the Star Tribune on 12.13.2015 that "the principals at ESG know that maintaining their focus on housing and hotels makes the company vulnerable to the next economic downturn, one they believe is on the horizon." <http://m.startribune.com/research-and-speed-pay-off-for-esg-the-architecture-firm-designing-so-much-twin-cities-real-estate/361616571/?section=business>. Even the architectural firm for Elevage, ESG, is aware they are overbuilding for the demand. This development for the 4.1 acres is too dense and too high to abut a residential neighborhood. It doesn't complement a wooded low density residential area bordered by the boundaries of Lake Vadnais, Grass Lake and 694.

1. We request that if a Traffic Study hasn't been done to date that it be done before the 694 west ramp is closed in order to get accurate data.
2. We request that a Study of the reduction of Sun Light be done on the properties adjoining the development. The development should not cause a reduction in direct sun light to the existing neighboring homes.
3. We request that an Air Flow Study be done to insure the homes adjacent to this development will not have stagnant pockets with no air flow.
4. We request that an Economic Study be done on the impact to private residence when large scale development is incorporated into existing neighborhoods.
5. We request a Market Analysis be done to show the need for additional apartments in this area.

Shoreview has a reputation for reducing the impact on residents when dealing with new developments. This Proposal from Elevage impacts the residents 100+%. We are against the rezoning to "mixed use." We are against having a bar/ sports bar introduced into a residential area. We are totally against the proposal at its current scale and high density.

Marcia Figus
3538 Rustic Place
Shoreview, MN 55126
651-483-3306



Kathleen Castle <kcastle@shoreviewmn.gov>

Regarding the Elevage Development Group, LLC Plan January 2016

1 message

jb3505@comcast.net <jb3505@comcast.net>

Sun, Jan 17, 2016 at 2:02 PM

To: kcastle@shoreviewmn.gov

Cc: jb3505@comcast.net

Regarding the Elevage Development Group, LLC Plan January 2016

My family had high hopes moving into our home on Rustic Place in 1966. Space for the kids, trees, nature, flowers and gardens. Living with neighbors who liked the out of doors and the nature setting. We had small ponds and drainage ditches. There was a culvert south of my property where the water would drain along Cty E to the west. The shopping center had a grocery store, hardware store, café, barber shop, flower store, liquor store, hair salon, and drug store with a post office. Everything a neighborhood could want.

Changes came on Rustic Place with storm sewer additions. It was an asset to us. But now I'm restless and agitated about what is being proposed. Because of the high density parking, there will be an over flow onto Rustic Place. No one can stop people from parking there. Anyone parking near my mailbox, will prevent my mail from being delivered. That has happened and that may happen more often. It is a narrow street. The corner of Cty E and Rustic Pl is already hard for two oncoming cars to negotiate especially if some vehicle is parked near the corner. Cars have taken out the stop sign, a school bus was hit, and many accidents avoided by quick reactions with cars running up onto the curb. I live on this corner, I see it. Vehicles visiting or making deliveries will make it almost impossible for me to back out of my driveway. How will the fire department and other emergency vehicles negotiate this with all the added congestion?

I am angry town homes will be so elevated compared to my house. It is a solid wall of 7 buildings. I will be looking out my kitchen window at walls not trees. I don't care how cute Elevage makes it look, it is still a solid wall! I won't feel safe and secure in my home. 7 buildings with windows will be staring down at me and into my home. My home for 50 years I have felt safe and secure. I don't live in St. Paul or Minneapolis where this density is accepted, I live in Shoreview. I live on a large wooded lot. I want to see the sunrise in the east in the morning. And I want to feel safe here!

I feel the density with 148 rental units (town homes and apartments) possibly 300+ people and children on a 4.1 acre plot is way too much.

Why has the watershed not been mentioned? There is a storm drain on 185 Cty E in the back yard. What about that? There is a support wall. Why isn't that being mentioned? Are there any easements dealing with these issues? In the Owasso and Montasori School project trees and watershed were addressed. Why do you choose to save trees for certain projects such as Owasso, Shoreview Library, Apple wood etc.? Why are all the trees on the residential lots being taken out? It takes a life time to grow a tree!

If I don't ask the question, I'm afraid you will ignore the potential problems that will occur. These issues directly affect me. I live here and I see all these problems. I plan to continue living here. I love my neighborhood. My husband and I built this home. My children and grandchildren and great grandchildren are always here.

I am not in favor of a bar or sports bar serving fancy beers. They are open to the late hours of the night. Even 10 pm is late for a residential neighborhood. Someone leaving the bar could drive north instead of south and drop 8 ft into 3520 Rustic Pl's back yard. Or a patron of the bar wanders onto Rustic Place looking for the car and knocking on our doors.

Shoreview is encouraging its residents to save on water usage. And this proposal has this useless pool that will be bordering 3520. We are suppose to monitor our water usage so there can be a pool for mosquitoes, leaves and trash to collect.

Cardigan Junction was here when we moved in. We don't wander into railroad property. Over the years police and fire trucks have been called back there for incidents at the rail road. They are very rare. But with increasing the population with renters who choose to wander onto the tracks there could be many future incidents. Someone was killed walking on the tracks recently. Rice St is also not a safe road to walk on. Someone was killed walking along Rice St near St Marie recently. Increasing the population will definitely mean there may be more problems with the railroad and accidents with pedestrians being hit possibly killed on Rice St.

Why on the diagram is there a stipulation of Lot 1 and Lot 2? Is Elevage making it easier for them in the future to sell off the lots? What is the designation of Block 1?

Where will the townhomes have their garbage and recycling picked up? Will there be 14 containers on Cty E every week for garbage? Will there be 28 containers every other week because of recycling? Or will there be one large dumpster for the townhomes to collect their garbage and recycling that we have to look at?

I am concerned with the application for Mixed Use. There has to be more controls over what goes into this area. Residential/ commercial zoning is what this 4.1 acres should be.

Please note on the diagram supplied by Elevage, the width of Rustic Place to Cty E is not to scale. As I stated earlier, Rustic Pl is narrow! Are homes our 2 stories. The development will dwarf our homes. This is too large for this area.

Thank you for reading this,

Joan M Benson
3505 Rustic Place
Shoreview, Mn 55126
[651.484.6539](tel:651.484.6539)



Kathleen Castle <kcastle@shoreviewmn.gov>

Elevage Proposal

Dorothy LeGault <dodylegault@aol.com>

Fri, Jan 15, 2016 at 8:37 AM

To: Kathleen Castle <kcastle@shoreviewmn.gov>

>>

>> I will not be in attendance for the January 2016 Planning Commission meeting as I am out of the country and will be until the end of the month. However, please be advised that I am vehemently opposed to the Elevage proposal that would put 14 town homes and a five story apartment building on 4.1 acres, all accessing County Road E.

>>

>> I understand and accept reluctantly that the property will be developed. However, it should not be developed to the detriment of the Rustic Place neighborhood. We are a small, unique neighborhood that can only leave via Rice Street. Elevage's proposal may even deny us that ability with the possibility of so many vehicles entering and leaving the 4.1 acres first on County Road E and then onto Rice.

>>

>> Kindly let their proposal be built elsewhere on a larger tract of land.

>>

>> Dr. Dody LeGault

>> 3546 Rustic Place

>>

>>

>> Sent from my iPad



Kathleen Castle <kcastle@shoreviewmn.gov>

Fwd: Elevage Proposal

1 message

Tom Simonson <tsimonson@shoreviewmn.gov>
To: KATHLEEN CASTLE <kcastle@shoreviewmn.gov>

Thu, Jan 14, 2016 at 9:51 PM

----- Forwarded message -----

From: **Dorothy Legault** <dodylegault@aol.com>

Date: Thursday, January 14, 2016

Subject: Elevage Proposal

To: smartin@shoreviewmn.gov, ejohnson@shoreviewmn.gov, tquigley@shoreviewmn.govCc: tschwem@shoreviewmn.gov, tsimonson@shoreviewmn.gov

This has been a difficult year for us as residents of Rustic Place, a neighborhood we have always enjoyed. The strip mall sale and loss of long time businesses, Elevage's outrageous proposals, potential loss of hundreds of trees which define our neighborhood, potential loss of sunlight in the winter, foreseeable traffic jams, loss of security . . . I could go on and on.

If it's really Shoreview's plan to have the entire piece of land developed for commercial or mixed use, then be out front with it. Make us an offer. Then hear the cries from north Rustic Place and neighbors on the west side of the street. Shoreview prides itself as a city that promotes green spaces, keeping viable neighborhoods, etc. But heck, as I've said before, Shoreview is just throwing us under the bus as they see the dollar signs from new taxes. It makes my heart heavy.

Change is inevitable. We know and accept that. This has been a great neighborhood in which to raise children. Those that were youngsters when we moved in are now grown and gone, but the cycle is starting all over. Or maybe not!

When Shoreview residents say they want restaurants, most are thinking in terms of Kozlaks, not sports bars. The new apartment on Victoria and County Road E looks quite empty. What makes city staffers think an apartment in place of our strip mall will fare any better?

The proposal for this 4.1 acre of land with 14 townhomes and a five story apartment complex is unreasonable for the neighborhood and the surrounding area. Common sense not greed for tax dollars must prevail.

We've battled CP Rail's steel containers, 180 St. Marie developer, the need for quiet zones, and now Elevage.

Our voices will be heard at the city and county level, Metropolitan Council, the media, Public Works, etc. We aren't going away.

Dody LeGault
3546 Rustic Place



Kathleen Castle <kcastle@shoreviewmn.gov>

RE: Elevage Redevelopment on Rice & Co.Rd E

1 message

Katie Cunningham <frog99752@yahoo.com>
Reply-To: Katie Cunningham <frog99752@yahoo.com>
To: kcastle@shoreviewmn.gov
Cc: smartin@shoreviewmn.gov

Thu, Jan 14, 2016 at 11:37 AM

Ms. Castle,

This is one more plea to you and the city planning commission and city council to reevaluate the scope of the project proposed for the corner of Rice St. and Co. Rd. E. We understand that foremost on the city's part is the money that will be generated for the City of Shoreview's cache. As a taxpaying citizen of Shoreview that is also a concern of mine. However, the question is how much money is enough? and where do we draw the line between money versus the needs of the community. More money is NOT always the answer much to the shagrin of our hard core capitalistic friends. I think we have already lost the battle to not have the project go forward so now the plea is to limit the size of the thing! I understand it is proposed for 6 stories! That seems excessive to me and for the surrounding neighbors that is a gross invasion of their personal privacy. I ask you to put yourself in their shoes. Please limit the scope of this project to help maintain the privacy of the existing long standing taxpayers that will be under the shadow of this monstrosity.

Sincerely,

Katie & Tim Cunningham
3651 Rustic Place
Shoreview, MN 55126

Are we looking at the total picture?

A message to the travelers on Rice Street near 694

Our neighborhood to the outsider may not appear to be that unique from other Shoreview neighborhoods but we are. What makes this grouping of 54 family homes special is that we are an island of homes, locked between a railroad yard, a freeway and Rice Street. We have no legal access to any walking or bike paths. On the website, WalkScore.com we score 20 out of 100 since we are car-dependent. We are rated so low due to the fact that for two long blocks of homes there is absolutely no place to walk without going on Rice Street with no access to sidewalks.

Right now, we have a quiet street. We all know and care about each other, and for my family it was a perfect place to raise our children. Living here, created a wonderful sense of neighborhood identity for us.

There is a proposal in place for the property development on the northwest corner of Rice St. and 694 that affects more than my little neighborhood. The loss of my privacy and sense of neighborhood as I know it is profound, and it also personal. The sudden proposed infusion of a six story building with 146 rental units, 14 rental townhouses and retail spaces to our neighborhood is staggering. We are one house away from the development. With the possible infusion of hundreds of new neighbors, their guests and retail customers we are left to our own imaginations of how this will affect us.

This development is a bigger problem than just to our neighborhood. Any person who has traveled on Rice Street near 694 knows how congested the area is already. What is not being addressed to this neighborhood is that the same developer said he owns the property across the street in Vadnais Heights, and he plans to build another large multi-story complex of rentals depending on what the market will bear.

Since this particular development stands in two cities, I question if we are losing touch with the overarching issues of the neighborhoods on all four corners. There are four cities that meet on 694 and Rice St, and I hope that Ramsey County is working on an incremental growth strategy that will not create worse traffic on a road that is already saturated with traffic during rush hours.

What our developers present for their solutions to infrastructure challenges are things he has no control over unless he pays for them himself. These solutions include the widening of Rice Street and the Rice Street Bridge, sidewalks on Rice Street and a footbridge. It seems impossible to talk about the plan on the northwest corner and ignore the intention on the northeast corner. The expansion on both sides of Rice Street into a high density multi-family residential units and retail needs to be talked about as one project since they share infrastructure and a common vision from the same developer.

I am not opposed to development; I am however against the scale of the development. I am supportive of any effort to improve the corner strip mall that the former property owner drove into distress while he sat on the property. I am sad that three of our neighbors are having their houses torn down. I am really sad to see my neighbors, who are in agreement that the scale is too big for that corner, but are not on the same page how we address the issues. I am left to wonder if this is the direction of a new Shoreview and a new intersection. We all understand a city's love of development, intensification, and urban sprawl for the benefit of an added tax base, but this can and should be achieved without changing the character of a neighborhood and city.

The architect refers to the area of Trader Joes on Excelsior near Highway 100 as a site they completed and say it would be comparable. While the building there is well done, it blends into a high retail, high density area with large stores and lots of traffic. There will be no blending in with this compound on both sides of Rice Street. The promise of walking to retail is a thin promise from the developer since there are no guarantees about the type of retail or restaurant they will have. We knew change was coming, but had no idea it would be of this magnitude.

As requested, the developers added more green space to replace a 300 car parking lot; however, the 2nd proposal increased in the number of units by adding a floor and 14 rental townhomes on Rustic Place. This was not in my opinion an adequate solution of compromise.

I understand the representatives of the developer being frustrated with us. I am truly saddened how some conversations rolled out between the council, planning committee, developers and neighbors. When a developer says, "You know I own this and the lots across Rice Street and this will happen," it diminishes a promotion of understanding and distracts any effort to work with the developers. It is easy to think that our efforts to be heard make no difference.

One of the core beliefs that I have experienced and loved about Shoreview is that people and communities matter. I hope that when this vote comes before our elected officials, they recognize how out of proportion this plan in its entirety is for our little island of homes and the infrastructure of Rice Street. Since so many of us from different cities share Rice Street, we need to talk about the impact on all citizens surrounding Shoreview including Vadnais Heights, North Oaks, Little Canada, Roseville and other northern suburbs.

Susan O'Neil
3530 Rustic Place
Shoreview, MN 55126

PROPOSED MOTION
ELEVAGE DEVELOPMENT GROUP, LLC /ELEVAGE SHOREVIEW
HOLDINGS, LLC

MOVED BY COMMISSION MEMBER: _____

SECONDED BY COMMISSION MEMBER: _____

To recommend the City Council approve the following requests submitted by Elevage Development Group, LLC/Elevage Shoreview Holdings, LLC (EDG) to redevelop the following properties: 157 County Road E, 185 County Road E, 3521 Rice Street and 3500 Rustic Place with a mixed use residential and commercial development.

Comprehensive Plan Amendment

1. The amendment changes the land use designation from C, Commercial/O, Office and RL, Low Density Residential to MU, Mixed Use.
2. Review and approval of the amendment by the Metropolitan Council.
3. The amendment will not be effective until the City grants approval of the Final Plat and PUD - Final Stage requests and the development agreements are executed.

Rezoning

1. This approval rezones the property from C2, General Commercial and R1, Detached Residential to PUD, Planned Unit Development.
2. The underlying zoning district for this PUD is: Lot 2 – R2, Attached Residential, Lot 3- R3, Multi-Dwelling Residential for the apartment units and C1, Retail Service for the commercial uses.
3. Rezoning is not effective until approvals are received for the Final Plat, PUD - Final Stage and development agreements executed.

Preliminary Plat

1. A public use dedication fee shall be submitted as required by ordinance prior to release of the final plat by the City.
2. The final plat shall include drainage and utility easements along the property lines. Drainage and utility easements along the roadways shall be 10' wide and along the side lot lines these easements shall be 5' wide. Other easements shall be dedicated as required by the Public Works Director.
3. Private agreements shall be secured between the parcels in the subdivision regarding the maintenance of shared facilities. Said agreements shall be submitted to the City Attorney for review and approval prior to the City's release of the Final Plat.
4. Comments received from the State of Minnesota and Ramsey County shall be addressed in the Final Plat submittal.
5. The Final Plat shall be submitted to the City for approval with the Final Stage PUD application.

Planned Unit Development – Development Stage

1. This approval permits the redevelopment of 157 County Road E, 185 County Road E, 3521 Rice Street and 3500 Rustic Place with a mixed use development consisting of a 5-story building that has 134 market rate apartment units and 6,800 square feet of commercial space on the first floor. Fourteen townhomes are also planned.
2. Access to the site shall be provided via the driveways off County Road E as indicated in the approved plans.
3. The items addressed in the City Engineer's memo dated January 20th shall be addressed in the Final PUD submittal.
4. Approval of the final grading, drainage, utility, and erosion control plans by the Public Works Director is required, prior to submittal to the City of applications for Final Plat and PUD – Final Stage. Final plans shall identify site construction limits and the treatment of work (i.e. driveways, parking areas, grading, etc.) at the periphery of these construction limits.
5. The applicant shall secure a permit from the Ramsey Washington Metro Watershed District prior to commencing any grading on the property.
6. The proposed mixed-use structure shall be of a 5-story design as depicted on the plans submitted with this application. Said building shall include the architectural enhancements and high-quality building materials as identified. The structure shall not exceed the 55-foot height as identified in this report and on the submitted plans.
7. The applicant shall review options to enhance landscape screening along the north property line such as berming or fencing.
8. A financial contribution to the City's Forestry fund is required since the number of required tree replacements cannot be accommodated on the development site.
9. The applicant shall provide additional information pertaining to the parking supply and demand prior to the City Council's consideration of the Development Stage application.
10. The applicant is required to enter into a Site Development Agreement and Erosion Control Agreement with the City. Said agreements shall be executed prior to the issuance of any permits for this project. The Development Agreement shall address:
 - a. Construction management and nuisances that may occur during the construction process, including parking for contractors. No parking is permitted on Victoria Street.
 - b. Best Management Practices for Water Quality improvement
 - c. Landscape maintenance
 - d. Maintenance of stormwater management facilities
11. This approval shall expire after two months if the Planned Unit Development - Final Stage application has not been submitted for City review and approval, as per Section 203.060 (C)(6).

This approval is based on the following findings:

- 1) The proposed redevelopment plan supports the policies stated in the Comprehensive Plan related to land use, housing and redevelopment.

- 2) The proposed redevelopment plan carries out the recommendations as set forth in the Housing Action Plan
- 3) The proposed redevelopment plan will not have a significant adverse impact the planned land use of the surrounding property.
- 4) The proposed deviations permit this site to be redeveloped with a use that expands life-cycle and affordable housing, including housing choice in the city.

VOTE:

AYES:

NAYS:

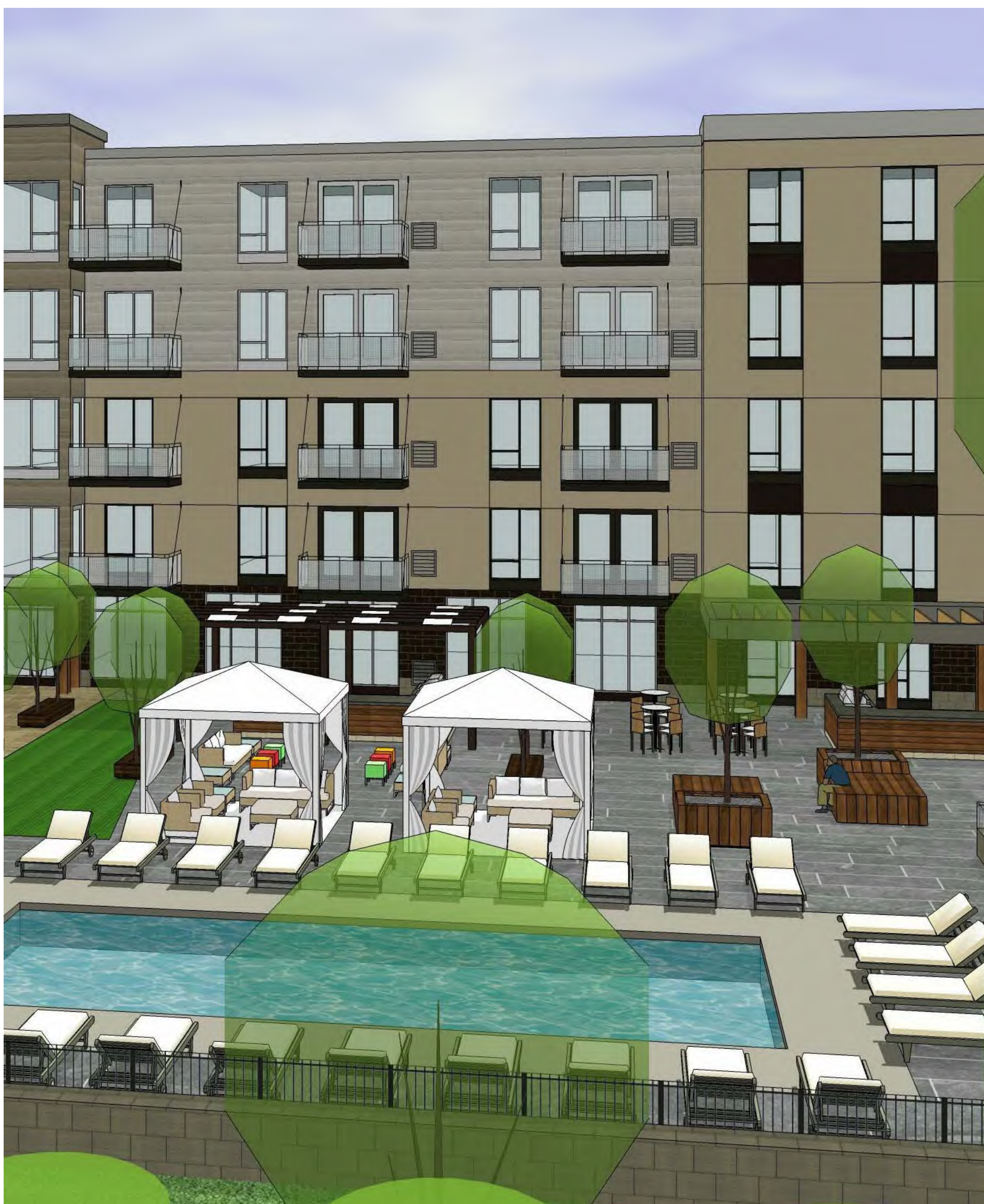
Regular Planning Commission Meeting
January 26, 2016

T:\2016 Planning Cases Files\2602-16-01 155-173 Cty Rd E-Elevage

SHOREVIEW MIXED USE

157 COUNTY ROAD E WEST, SHOREVIEW, MN 55126

SHOREVIEW
MIXED-USE
157 COUNTY ROAD E WEST,
SHOREVIEW, MN 55126



einess swenson graham architects
500 WASHINGTON AVENUE SOUTH
MINNEAPOLIS, MINNESOTA 55415
P. 612.339.5508
F. 612.339.5382
WWW.ESGARCH.COM

I hereby certify that this plan, specification, or report was prepared by me or under my direct supervision and that I am a duly licensed architect under the laws of the State of Minnesota.

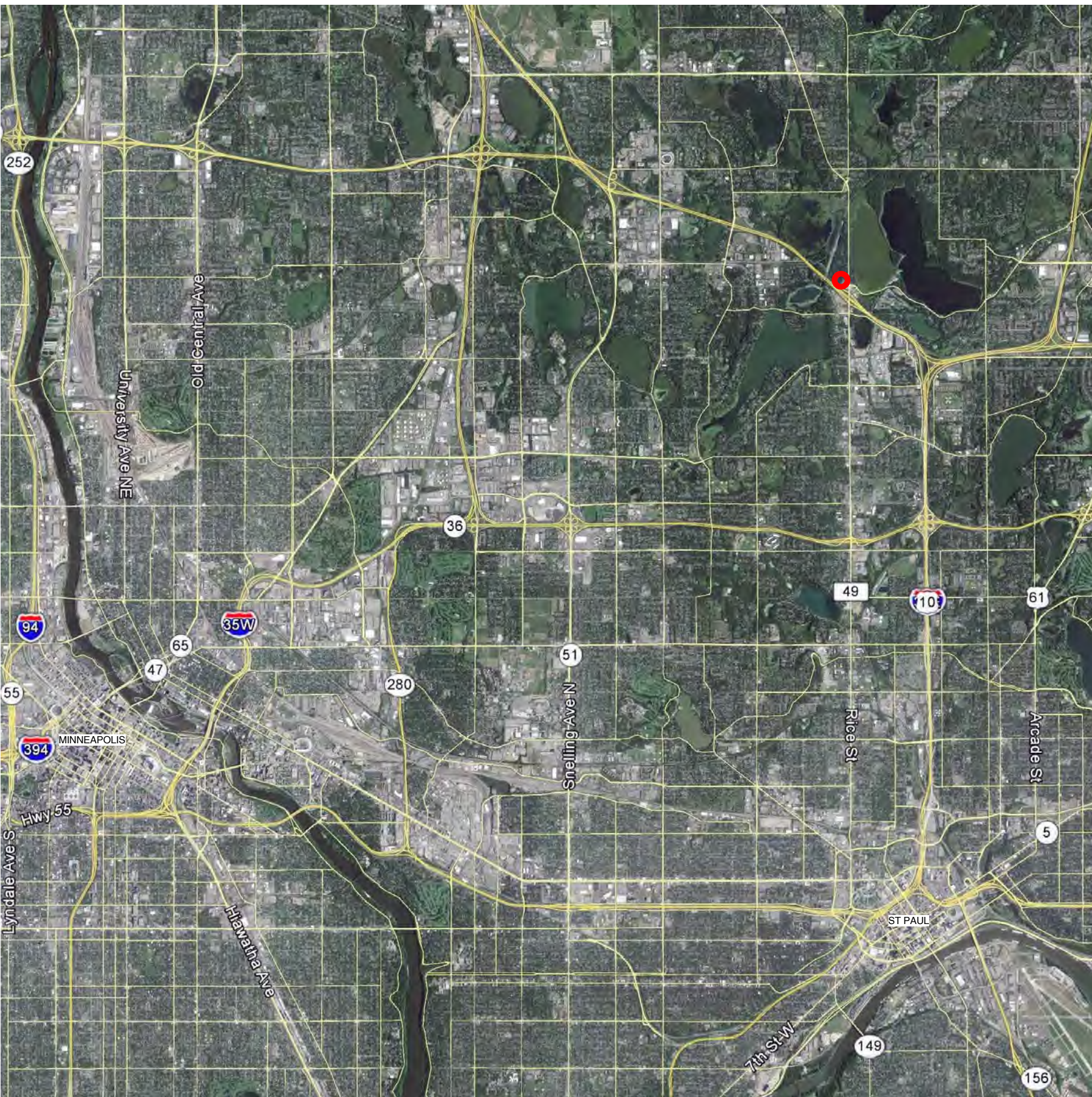
Signature

Typed or Printed Name

License # Date

NOT FOR
CONSTRUCTION

PROJECT LOCATION



Vicinity



Site Location

PROJECT METRICS

	TOTAL GSF	PARKING GSF	STALLS	RETAIL GSF	AMENITY GSF	APT GSF	APT RSF	APT UNITS	TH-Townhome		
									TH GSF	TH PARKING GSF	TH UNITS
LEVEL P1	61,164	61,164	168								
LEVEL 1	30,967		76	6,849	5,695	18,423	14,850	17		11,284	14
LEVEL 2	23,790					23,790	19,882	23	11,284		
LEVEL 3	34,252					34,252	29,574	32	11,284		
LEVEL 4	34,252					34,252	29,574	32			
LEVEL 5	33,289				1,046	32,243	27,691	30			
	217,714	61,164	244	6,849	6,741	142,960	121,571	134	22,568	11,284	14

PROJECT TEAM

Owner/Developer: Elevage Development Group, LLC.
10901 Baltimore St. NE
Blaine, MN 55449

Architect: Einess Swenson Graham Architects Inc.
500 Washington Ave. South, Suite 1080
Minneapolis, MN 55415
Ph: 612-339-5508
Fx: 612-339-5382

Contractor: T.B.D.

Civil/Landscape: Erickson Civil Site
333 N. Main Street, Suite 201
Stillwater, MN 55082
Ph: 612-309-3804

Structural Engineer: T.B.D.

**Mechanical,
Electrical,
Plumbing Engineers:** T.B.D.

SHEET INDEX

SHEET NUMBER	SHEET NAME	EDG PUD SUBMITTAL
GENERAL INFORMATION		
T1.1	TITLE SHEET	•
CIVIL		
V1	SURVEY	•
V2	FINAL PLAT	•
C1	EXISTING CONDITIONS & DEMO PLAN	•
C2	LAYOUT PLAN	•
C3	GRAVING PLAN	•
C4	UTILITY PLAN	•
C5	DETAILS	•
C6	DETAILS	•
LANDSCAPE		
L1	LANDSCAPE PLAN	•
LANDSCAPE		
E1	LIGHTING PLAN	•
LANDSCAPE		
D1	DRAINAGE MAP	•
ARCHITECTURAL		
A0.1	EXISTING SITE IMAGES	•
A0.2	DESIGN PERSPECTIVES	•
A0.3	DESIGN PERSPECTIVES	•
A0.4	DESIGN PERSPECTIVES	•
A0.5	SHADOW STUDY	•
A1.1	SITE PLAN - LEVEL 1 PLAN	•
A1.2	LEVELS P1, 2, 3-4, 5	•
A3.1	EXTERIOR ELEVATIONS	•
A3.2	EXTERIOR ELEVATIONS	•
A3.3	EXTERIOR ELEVATIONS (TOWNHOMES)	•

EDG PUD
SUBMITTAL

ORIGINAL ISSUE: 12/23/2015

REVISIONS

No. Description Date

215537

PROJECT NUMBER

ESG

ESG

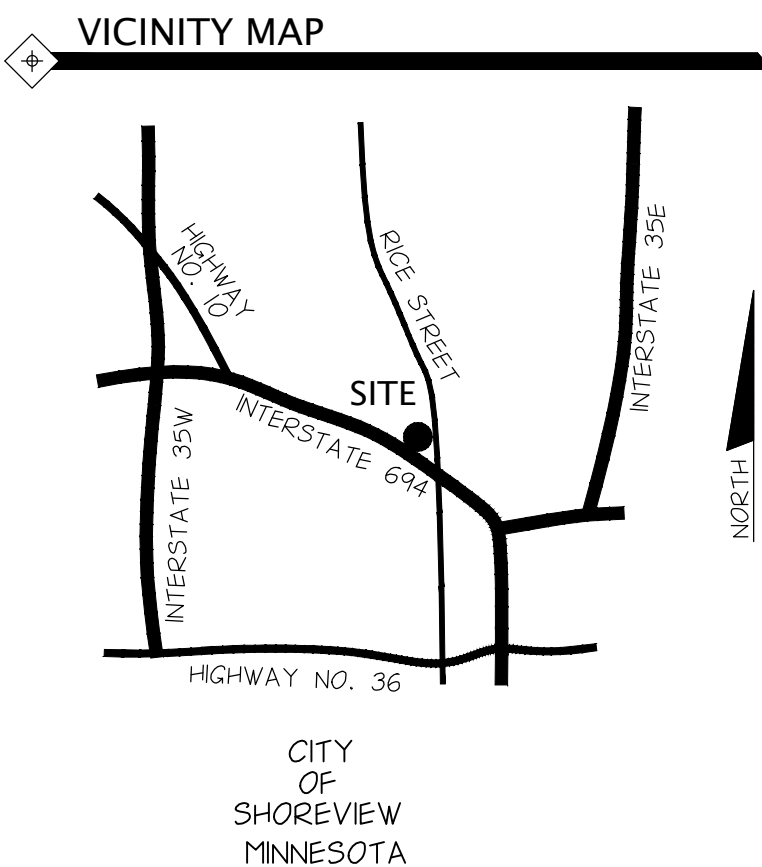
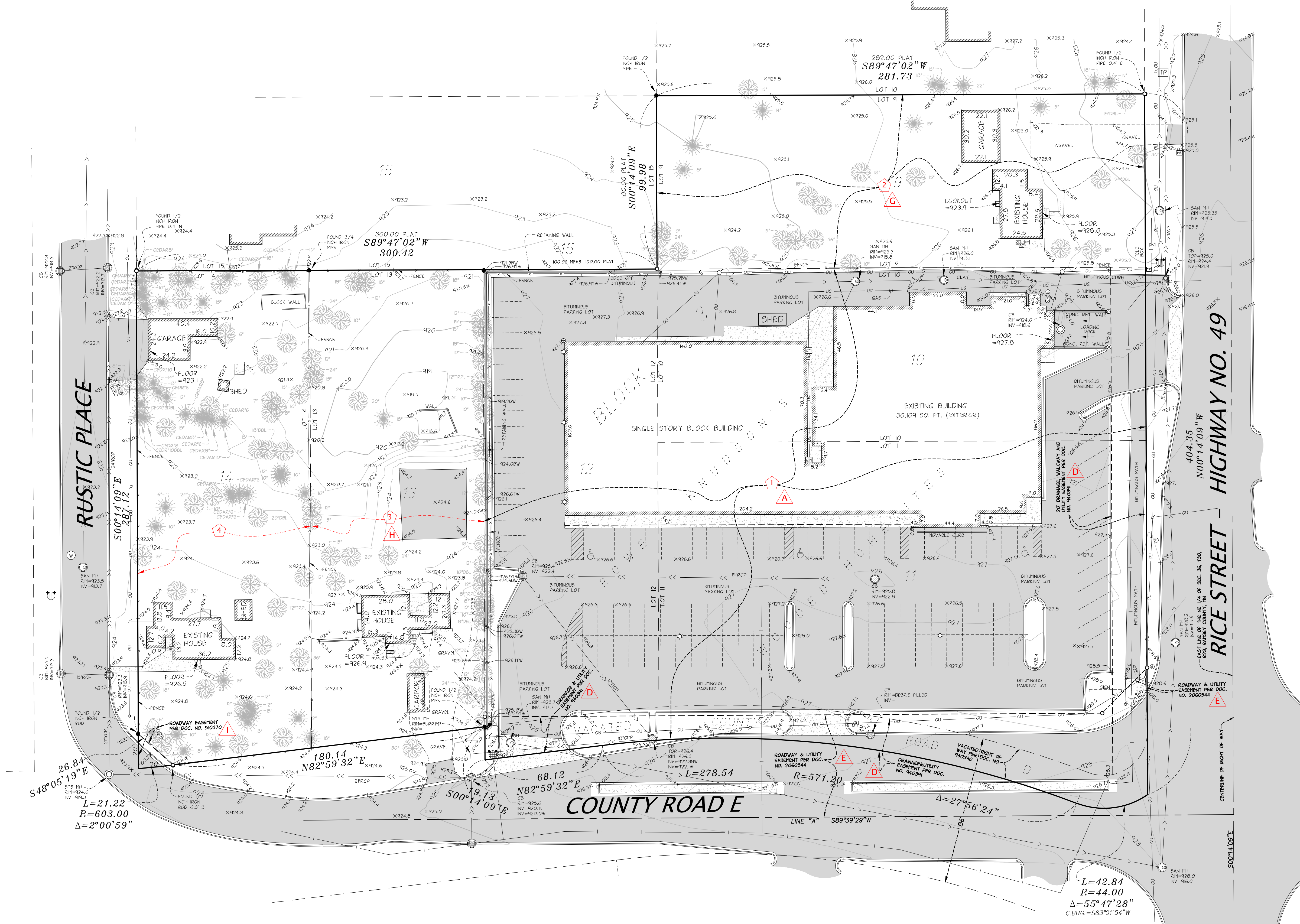
DRAWN BY CHECKED BY

KEY PLAN

SHOREVIEW MIXED-USE

TITLE SHEET

T1.1



UNDERGROUND UTILITIES NOTES:

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEE THAT THE UNDERGROUND UTILITIES SHOWN COMPROMISE ALL SUCH UTILITIES IN THE AREA, EITHER IN SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH HE DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM THE INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES. GOPHER STATE ONE CALL LOCATE TICKET NUMBER(S) 153132318, 153132322, 153132324 & 153132341. SOME MAPS WERE RECEIVED, WHILE OTHER UTILITIES DID NOT RESPOND TO THE LOCATE REQUEST. ADDITIONAL UTILITIES OF WHICH WE ARE UNAWARE MAY EXIST. OTHER UTILITIES MAY EXIST ON THIS SITE THAT WERE NOT MARKED UP.

CALL BEFORE YOU DIG!
Gopher State One Call
TWIN CITY AREA: 651-454-0002
TOLL FREE: 1-800-252-1166

BUILDING SETBACK & ZONING INFO

THIS PARCEL IS ZONED C-2 (COMMERCIAL BUSINESS). THE BUILDING HEIGHT RESTRICTIONS FOR THIS ZONING DISTRICT IS 35 FEET PER THE CITY OF SHOREVIEW PLANNING DEPARTMENT. THE FRONT BUILDING SETBACK IS 50 FEET ADJOINING STREETS, THE SIDE YARD BUILDING SETBACK IS 10 FEET AND THE REAR YARD BUILDING SETBACK IS 20 FEET. THE PARKING SETBACKS ARE 20' ADJOINING STREETS AND 5' REAR AND SIDE. THERE IS A 75% MAXIMUM IMPERVIOUS COVERAGE REQUIREMENT. ALL SETBACK INFORMATION PROVIDED BY THE CITY OF SHOREVIEW PLANNING DEPARTMENT.

SURVEY NOTES:

1. BEARINGS ARE BASED ON COORDINATES SUPPLIED BY THE RAMSEY COUNTY SURVEYORS OFFICE.
2. UNDERGROUND UTILITIES SHOWN PER GOPHER STATE ONE CALL LOCATES AND AS-BUILT PLANS PROVIDED BY THE CITY OF SHOREVIEW PUBLIC WORKS DEPARTMENT.
3. THERE MAY BE SOME UNDERGROUND UTILITIES: GAS, ELECTRIC, ETC. NOT SHOWN OR LOCATED.

FLOOD INFORMATION:

THIS PROPERTY LIES WITHIN AN NON DESIGNATED AREA AS SHOWN ON THE FLOOD RATE MAP 27123C0036G, DATED JUNE 4TH, 2010. NON DESIGNATED AREAS ARE CONSIDERED TO BE IN ZONE X.

EXISTING PARKING

THERE ARE 108 VISIBLY PARKING STALLS DESIGNATED ON THIS PARCEL INCLUDING 3 HANDICAP STALL.

BENCHMARKS

ELEVATIONS BASED ON INFORMATION AS SHOWN ON THE MNDOT GEODETIC WEBSITE. SURVEY DISK 6285 N WITH AN ELEVATION OF 927.31 WAS USED TO ESTABLISH VERTICAL CONTROL FOR THIS SURVEY (NAVD 88)

ALTA NOTES:

ITEM 16. THIS PARCEL SHOWS NO OBVIOUS SIGNS OF RECENT CONSTRUCTION

ITEM 17. THERE ARE NO PROPOSED RIGHT OF WAY CHANGES PER THE CITY OF SHOREVIEW PLANNING DEPARTMENT AS OF THE DATE OF THE CERTIFICATION.

ITEM 18. THERE WAS NO EVIDENCE OF SITE USE AS A SOLID WASTE DUMP, SUMP, OR SANITARY LANDFILL.

ITEM #19. THERE WAS NO EVIDENCE OF A WETLAND AS OF THE DATE OF THIS CERTIFICATION.

LEGAL DESCRIPTION:

The following Legal Descriptions are as shown on the various Old Republic National Title Insurance Company Title Commitments as follows:

- (Commitment Number 39729, dated September 10th, 2015.)
Lots 10, 11, and 12, Block 3, Rowe & Knudson's Wooded Homesites, and that part of the North 19 feet of County Road E, vacated, lying between the extensions Southerly across said street of the West line of said Lot 12 and the East line of said Lot 11, Ramsey County, Minnesota. Torrens Property.
- (Commercial Partners Title Commitment Number 50359, dated Sept. 27th, 2015.)
Lot 9, Block 3, Rowe & Knudson's Wooded Homesites, Ramsey County, Minnesota. Torrens Property.
- (Commercial Partners Title Commitment Number 39817, dated Feb. 25th, 2015.)
Lot 13, Block 3, Rowe & Knudson's Wooded Homesites, Ramsey County, Minnesota. Torrens Property.
- (Commercial Partners Title Commitment Number 39934, dated Mar. 26th, 2015.)
Lot 14, Block 3, Rowe & Knudson's Wooded Homesites, Ramsey County, Minnesota. Torrens Property.

AREA:

TOTAL AREA AS SHOWN = 191,600 SQ.FT.

SCHEDULE B2 NOTES:

The following survey related easements and encumbrances are shown on the various Old Republic National Title Insurance Company Title Commitments as follows:

- (Commitment Number 39729, dated September 10th, 2015.)
4. It is hereby agreed that any public utility company shall have the right to enter upon and set poles along dividing line between lots for the purpose of stringing wires thereon for the purpose of furnishing electric light and power to any of the persons purchasing lots in said addition, and such utility company or companies shall also have the right to trim trees for purpose of stringing its said wires, as shown by recital on the Certificate of Title. (COVERS ENTIRE PARCEL, NOT SHOWN)
5. Subject to an easement for public utility purposes per Document No. 623508, and subject to an easement for purposes utility purposes over that part of County Road E vacated by Document No. 940390; as shown by recital on the Certificate of Title. NOTE: The public roadway easement contained in Document No. 623508 was vacated by Document No. 940390. (AS SHOWN ON SURVEY)
6. Terms and conditions of Order dated January 18, 1956, filed February 14, 1956, as Document No. 360298. (NOT SHOWN ON SURVEY)
7. Terms and conditions of and easements per Document No. 940391. (AS SHOWN ON SURVEY)
8. Easements for roadway and utility purposes, in favor of Ramsey County, as created in Final Certificate dated December 17, 2008, filed January 9, 2009, as Document No. 2060544. (AS SHOWN ON SURVEY)
- (Commercial Partners Title Commitment Number 50359, dated Sept. 27th, 2015.)
9. Subject to covenants and agreements contained in Warranty Deed dated March 25, 1940, filed March 30, 1940, as Document No. 183155. (Shown as recital on Certificate of Title) (COVERS ENTIRE LOT, MAY HAVE EXPIRED)
- (Commercial Partners Title Commitment Number 39817, dated Feb. 25th, 2015.)
14. Terms and conditions of utility easement contained in Warranty Deed dated October 19, 1940, filed October 22, 1940, as Document No. 188182. (COVERS ENTIRE LOT, MAY HAVE EXPIRED)
- (Commercial Partners Title Commitment Number 39934, dated Mar. 26th, 2015.)
15. Terms and conditions of final Certificate, including an easement for highway purposes, and the right to erect temporary snow fences upon the land adjacent to the easement filed May 23, 1967, as Document No. 510370. (AS SHOWN ON SURVEY)

CERTIFICATION:

To: Elvage Shoreview, LLC., Commercial Partners Title, LLC, Old Republic National Title Insurance company. This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2011 Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys, jointly established and adopted by ALTA and NSPS, and includes Items 1, 2, 3, 4, 7(a), 7(b)(1), 7(c) 8, 9, 11(b), 14, 16, 17, 18, and 19, of Table A thereof. The field work was completed on November 12th, 2015

CORNERSTONE LAND SURVEYING, INC.
Dated: 11-12-15
Revised:

By: Daniel L. Thurmes
Minnesota License No. 25718

The Commercial Partners Title, LLC as agent for Old Republic National Title Insurance Company Title Commitments as listed above were relied upon for matters of record. Other easements may exist that were not shown in this commitment and are not shown on this survey.

LEGEND			
	CABLE TV PEDESTAL		STORM MANHOLE
	AIR CONDITIONER		FIRE DEPT. CONNECTION
	ELECTRIC MANHOLE		HYDRANT
	ELECTRIC METER		CURB STOP
	ELECTRIC PEDESTAL		WATER VALVE
	ELECTRIC TRANSFORMER		BOLLARD
	LIGHT POLE		FLAG POLE
	GUY WIRE		TRAFFIC SIGN
	POWER POLE		UNKNOWN MANHOLE
	GAS METER		SPOT ELEVATION
	TELEPHONE PEDESTAL		CONIFEROUS TREE
	SANITARY MANHOLE		DECIDUOUS TREE
	CATCH BASIN		
	FLARED END SECTION		
	ROOF DRAIN		
	WATER MANHOLE		
	UNDERGROUND ELECTRIC		SANITARY SEWER
	UNDERGROUND CABLE TV		STORM SEWER
	UNDERGROUND FIBER OPTIC		WATERMAIN
	UNDERGROUND TELEPHONE		FENCE
	OVERHEAD UTILITY		CURB (TYPICAL)
	UNDERGROUND GAS		CONTOURS

157
COUNTY RD. E. W.

SHOREVIEW

CONTACT:

c/o Michael J. Mergens
EntrePartner Law Firm, PLLC
Highlight Center
807 Broadway Street NE, Suite 140
Minneapolis, MN 55413
612.314.8001 (office)
612.314.8003 (direct)
612.207.5660 (mobile)
http://www.entrepartnerlaw.com

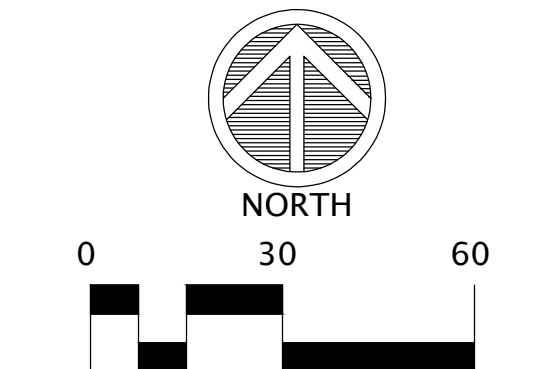
COUNTY/CITY:

RAMSEY
COUNTY

CITY OF
SHOREVIEW

REVISIONS:

DATE	REVISION
11-12-15	PRELIMINARY ISSUE



PROJECT LOCATION:

157
COUNTY RD. E. W.
PID#363023110038
PID#363023110009
PID#363023110011
PID#363023110035

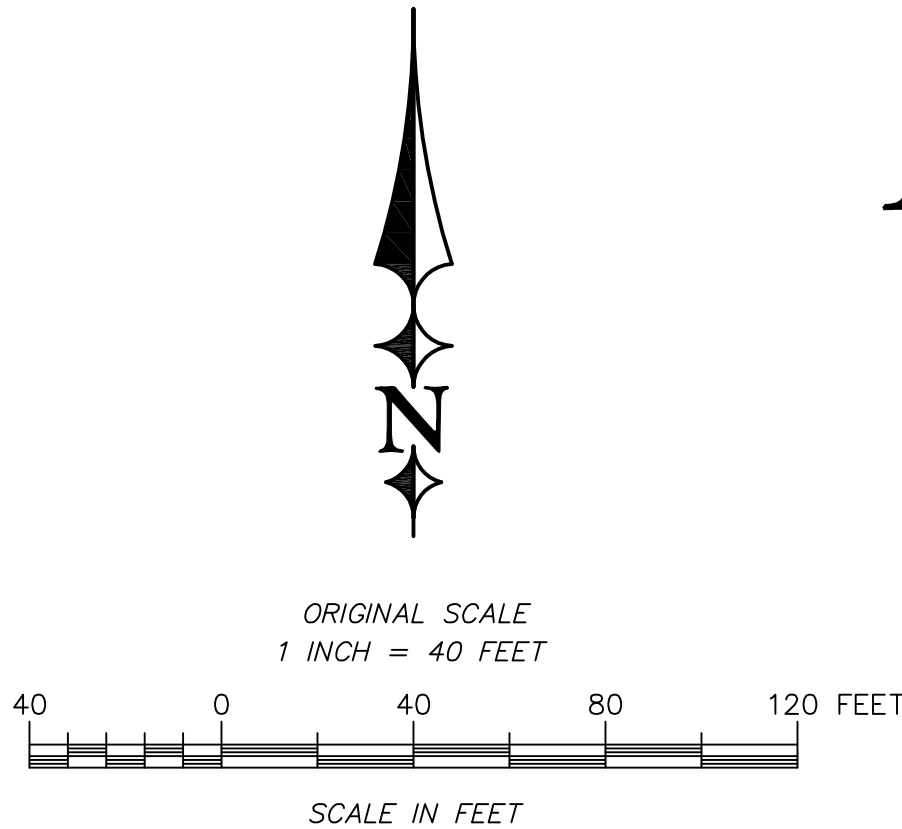
Suite #1
6750 Stillwater Blvd. N.
Stillwater, MN 55082
Phone 651.275.8969
Fax 651.275.8976
dane@cssurvey.net

CORNERSTONE
LAND SURVEYING, INC.

FILE NAME: SURVNOT03
PROJECT NO.: NOT15003

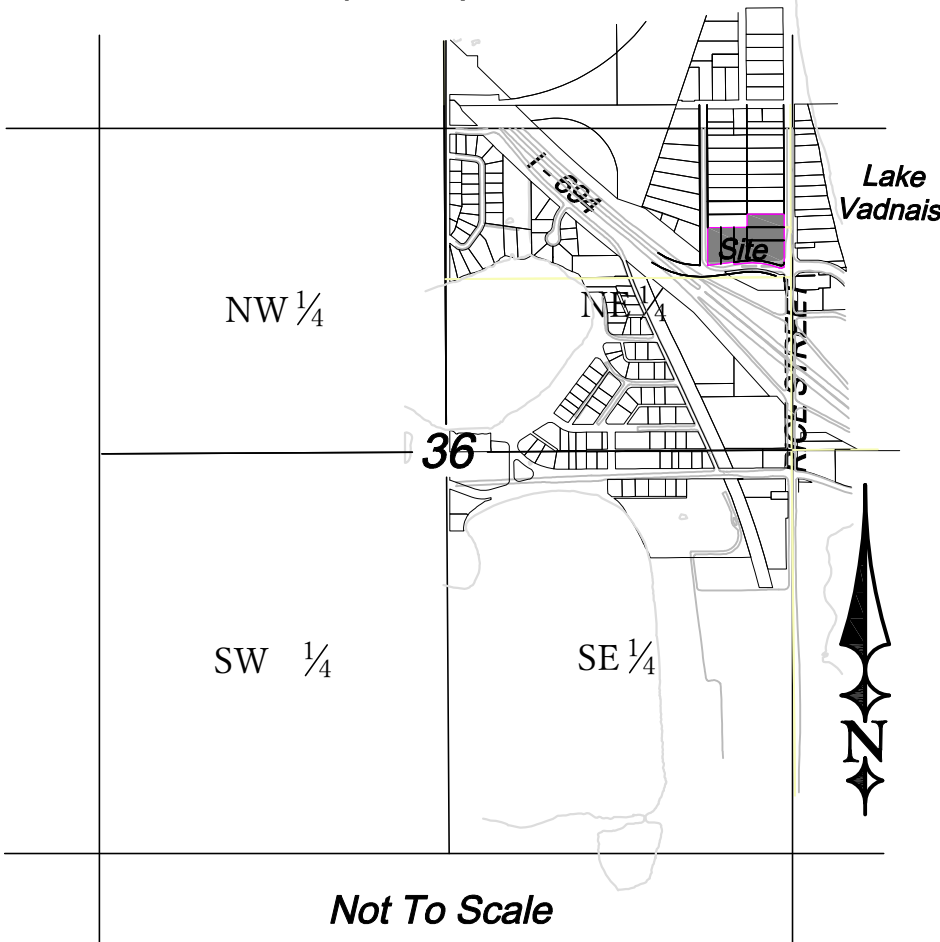
CERTIFICATE OF
SURVEY

EDG SHOREVIEW



Site Location Map

Section 36, Township 30 North, Range 23 West
Ramsey County, Minnesota



LEGEND

- DENOTES SET 1/2 INCH BY 16 INCH IRON PIPE MONUMENT MARKED WITH A PLASTIC CAP INSCRIBED "_____", UNLESS SHOWN OTHERWISE
- DENOTES FOUND MONUMENT, SIZE AND MARKINGS AS INDICATED
- ⊙ DENOTES RAMSEY COUNTY CAST IRON MONUMENT

NOTES

1) ORIENTATION OF THIS BEARING SYSTEM IS BASED ON RAMSEY COUNTY COORDINATES.

Owners Certificate

KNOW ALL MEN BY THESE PRESENTS: _____, a _____ corporation under the laws of Minnesota, owner of the following described property, situated in the City of Shoreview, County of Ramsey, State of Minnesota:

Lots 10, 11, and 12, Block 3, Rowe & Knudson's Wooded Homesites, and that part of the North 19 feet of County Road E, vacated, lying between the extensions Southerly across said street of the West line of said Lot 12 and the East line of said Lot 11, Ramsey County, Minnesota and Lot 9, 13 and 14 Block 3, Rowe & Knudson's Wooded Homesites, Ramsey County, Minnesota. Torrens Property.

Has caused the same to be surveyed and platted as EDG SHOREVIEW and does hereby dedicate or donate to the public for public use forever the public ways and the drainage and utility easements as shown on this plat.

In witness whereof said _____, a _____ corporation under the laws of the State of Minnesota, has caused these presents to be signed by its proper officer this _____ day of _____, 20____.

Signed: _____

Name and Title _____

STATE OF _____
COUNTY OF _____

The foregoing instrument was acknowledged before me this _____ day of _____, 20____, by _____, a _____ corporation under the laws of Minnesota, on behalf of the corporation.

Signature

Print Name

Notary Public, _____
My Commission Expires _____

Surveyors Certificate

I, _____, Land Surveyor, do hereby certify that I have surveyed or directly supervised the survey of the property described on this plat; prepared this plat or directly supervised the preparation of this plat; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on this plat have been correctly set; that all water boundaries and wet lands, as defined in Minnesota Statutes, Section 505.01, Subd. 3, as of the date of the Surveyor's Certification are shown and labeled on this plat; and all public ways are shown and labeled on this plat.

Dated this _____ day of _____, 20____.

Land Surveyor

Minnesota License No. _____

STATE OF MINNESOTA
COUNTY OF WASHINGTON

The foregoing Surveyor's Certificate was acknowledged before me this _____ day of _____, 20____, by Paul A. Johnson, Land Surveyor.

Todd A. Erickson
Notary Public, Minnesota
My Commission expires January 31, 2019

City of Little Canada

We do hereby certify that on the _____ day of _____, 20____, the City Council of the City of Shoreview, Minnesota, approved this plat. Also, the conditions of Minnesota Statutes, Section 505.03, Subd. 2, have been fulfilled.

Mayor

Clerk

Department of Property Records and Revenue

Pursuant to Minnesota Statutes, Section 505.021, Subd. 9, taxes payable in the year 20____ on the land hereinbefore described have been paid. Also, pursuant to Minnesota Statutes, Section 272.12, there are no delinquent taxes and transfer entered this _____ day of _____, 20____.

Director
Property Records and Revenue

Deputy

County Surveyor

I hereby certify that this plat complies with the requirements of Minnesota Statutes, Section 505.021, and is approved pursuant to Minnesota Statutes, Section 383A.42, this _____ day of _____, 20____.

Craig W. Hinzman, L.S.
Ramsey County Surveyor

County Recorder

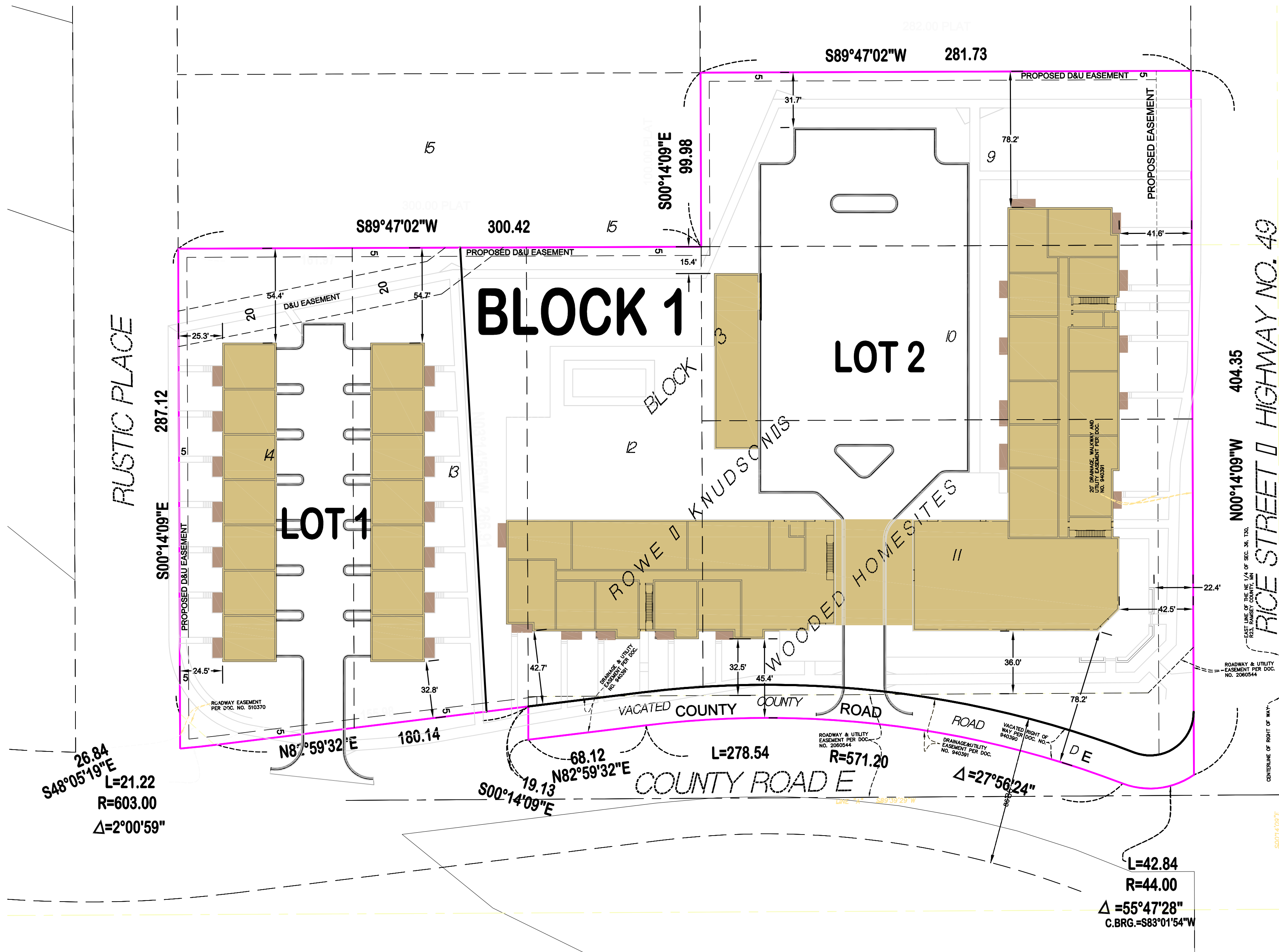
County of Ramsey, State of Minnesota
I hereby certify that this plat of EDG SHOREVIEW was filed in the office of the County Recorder for the public record on this _____ day of _____, 20____, at _____ o'clock _____ M. and was duly filed in Book _____ of Plats, Page _____, as Document Number _____.

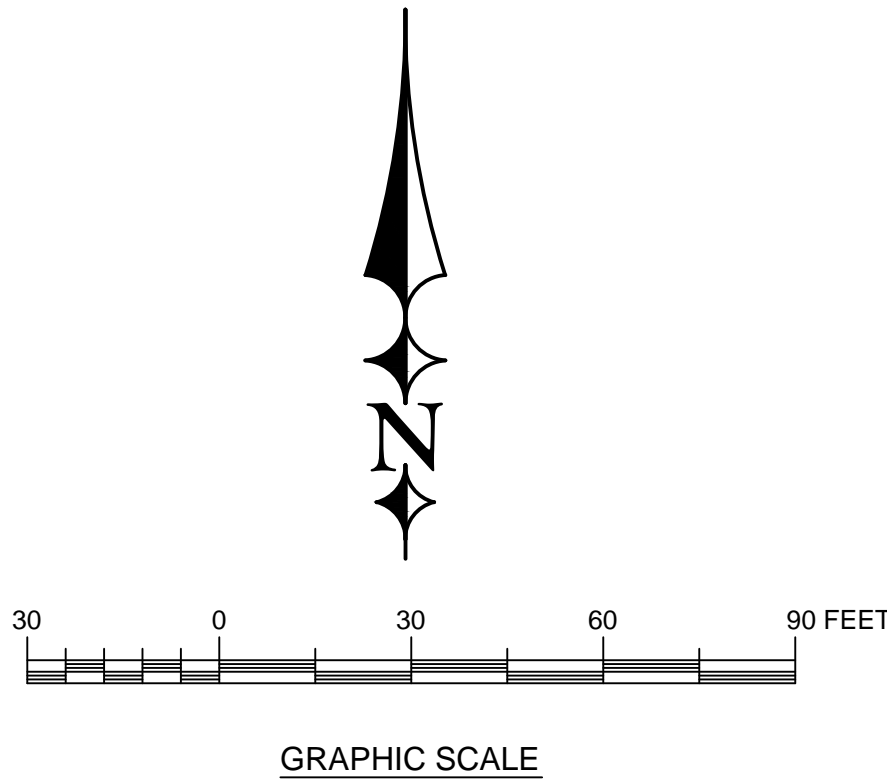
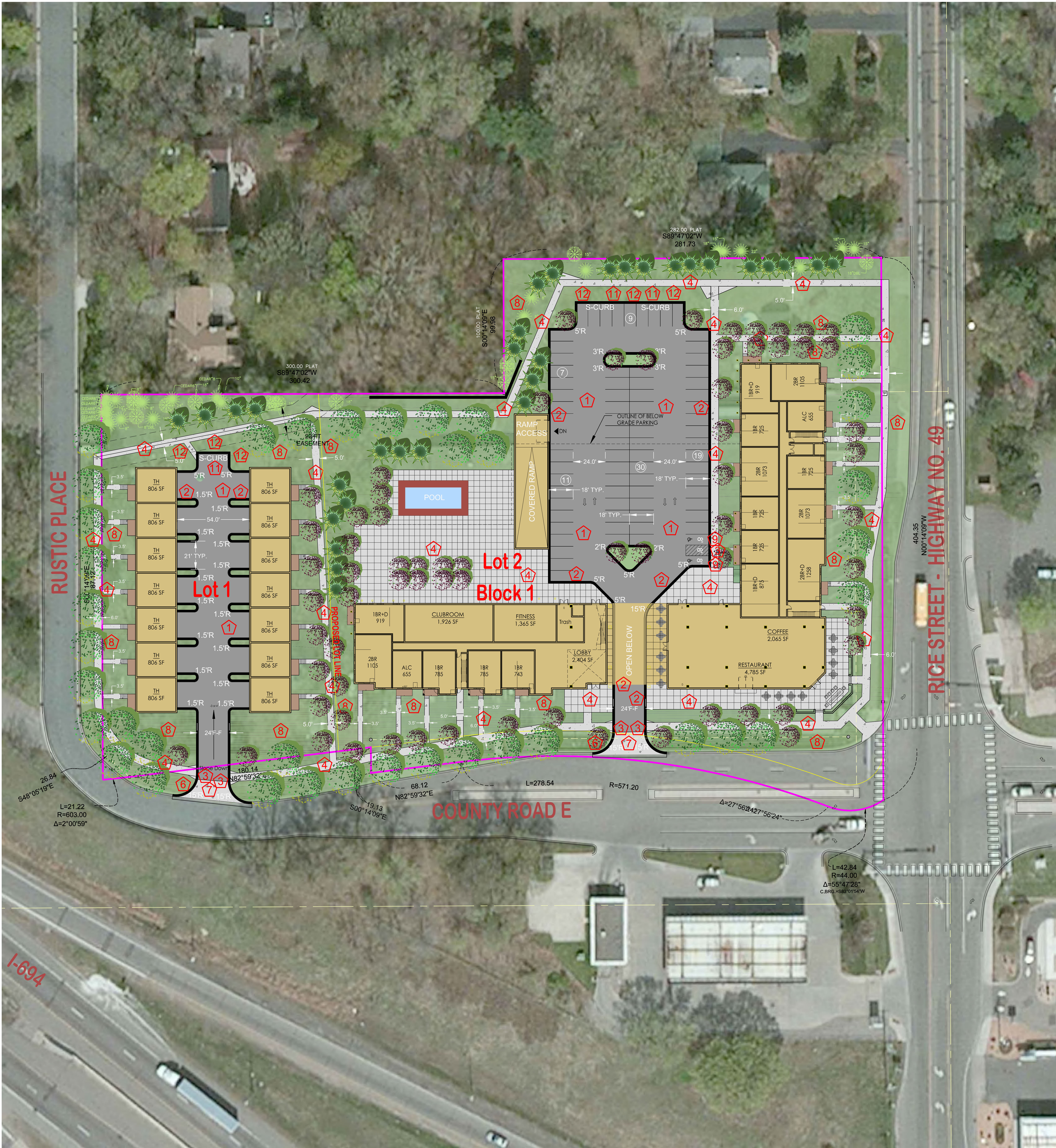
Deputy County Recorder

ERICKSON
CIVIL

333 North Main Street, Suite 201
Stillwater, Minnesota 55082
Phone (612) 309-3804

www.ericksoncivilsite.com





LEGEND:

- EXISTING BOUNDARY
- PROPOSED CONCRETE SIDEWALK/APRON
- PROPOSED BITUMINOUS PAVEMENT
- SURFACE PARKING STALL COUNT (TOTAL PROPOSED = 76)

KEYED NOTES:

- INSTALL BITUMINOUS PAVEMENT. REFER TO DETAIL 1/C5.
- INSTALL B612 CONCRETE CURB. REFER TO DETAIL 2/C5.
- CONSTRUCT CONCRETE ADA RAMP. REFER TO DETAIL 5/C5.
- INSTALL 4" CONCRETE SIDEWALK. REFER TO DETAIL 4/C5.
- MODULAR BLOCK WALL WITH RAILING (MNDOT SPEC. VERSA LOK BRONCO). ANY WALL OVER 4-FT IN HEIGHT SHALL BE DESIGNED BY AN ENGINEER AND A SIGNED PLAN SET OF DRAWINGS SHALL BE PROVIDED TO THE CITY PRIOR TO START OF CONSTRUCTION.
- INSTALL "STOP" TRAFFIC CONTROL SIGN.
- CONSTRUCT CONCRETE ENTRANCE APRON. SEE DETAIL 10/C5.
- 6" MIN. TOPSOIL AND HYDRO SEEDING WITH LOW GROW FESCUE @ 220 LBS. PER ACRE, W/20 LBS. PER ACRE ANNUAL RYE.
- INSTALL ADA SIGNAGE IN CONCRETE FOR STALL. SEE DETAIL 5/C5 FOR RAMP CONSTRUCTION. LANDING(S) SHALL BE A MINIMUM OF 4-FT WITH A 2% MAXIMUM SLOPE OR AS SHOWN ON DETAIL.
- INSTALL FENCING AT RETAINING WALL PER ARCHITECT DETAIL
- SURMOUNTABLE CURB AND GUTTER
- SNOW STORAGE AREA

LAYOUT NOTES:

- ALL RADII TO BACK OF CURB
- ALL DIMENSIONS TO THE FACE OF CURB UNLESS NOTED OTHERWISE.
- PAVEMENT STRIPING TO BE 4" WIDE WHITE EPOXY PAINTED STRIPE.

SURFACE AREA ANALYSIS

TOTAL LOT AREA	= 184,158 S.F.
LOT 1	= 46,883 S.F.
LOT 2	= 137,275 S.F.
EXISTING IMPERVIOUS SURFACE	= 101,500 S.F. (55.1%)
CONCRETE	= 4,470 S.F.
BIT.	= 59,350 S.F.
SHED/GARAGE	= 710 S.F.
HOUSE	= 3,860 S.F.
BUILDING	= 30,110 S.F.
GRAVEL	= 3,000 S.F.
TOTAL PROPOSED IMPERVIOUS SURFACE	= 106,770 S.F. (57.0%)
SIDEWALK	= 20,880 S.F.
BIT.	= 35,220 S.F.
APARTMENT	= 34,675 S.F.
GARAGE	= 2,595 S.F.
TOWN HOMES	= 13,400 S.F.
LOT 1 PROPOSED IMPERVIOUS SURFACE	= 29,785 S.F. (63.5%)
SIDEWALK	= 5,010 S.F.
BIT.	= 11,375 S.F.
TOWNHOMES	= 13,400 S.F.
LOT 2 PROPOSED IMPERVIOUS SURFACE	= 76,985 S.F. (56.1%)
SIDEWALK	= 15,870 S.F.
BIT.	= 23,845 S.F.
GARAGE	= 2,595 S.F.
APARTMENT	= 34,675 S.F.

ERICKSON
CIVIL

333 North Main Street, Suite 201
Stillwater, Minnesota 55082
Phone (612) 309-3804
www.ericksoncivilsite.com

DRAWING PHASE:

OWNER REVIEW

AGENCY REVIEW

BID DOCUMENT

PERMIT SET

AS-BUILT DOCUMENT

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

TODD A. ERICKSON, PE
40418
LICENSE NO.
EDG PUD
DATE:

OWNER/DEVELOPER

ELEVAGE DEVELOPMENT GROUP, LLC
10901 Baltimore St. NE
Blaine, Minnesota 55449

PROJECT TITLE

SHOREVIEW MIXED USE
157 COUNTY ROAD E, W.
SHOREVIEW, MN 55126

NO.

REVISION

DESCRIPTION

DATE

JOB NO.

15-172

SHEET TITLE

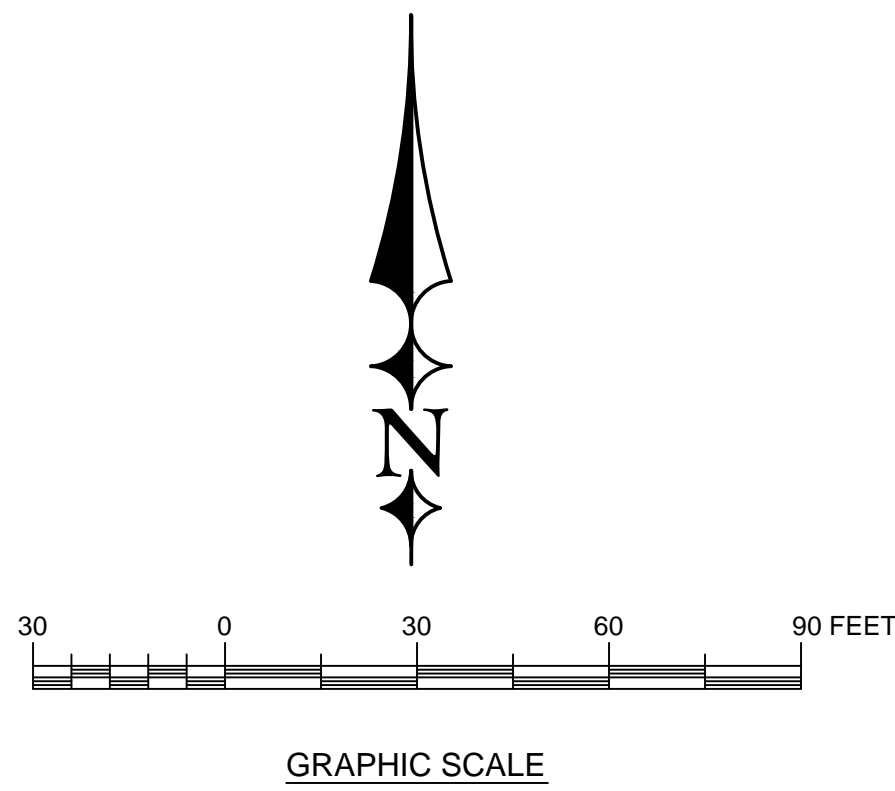
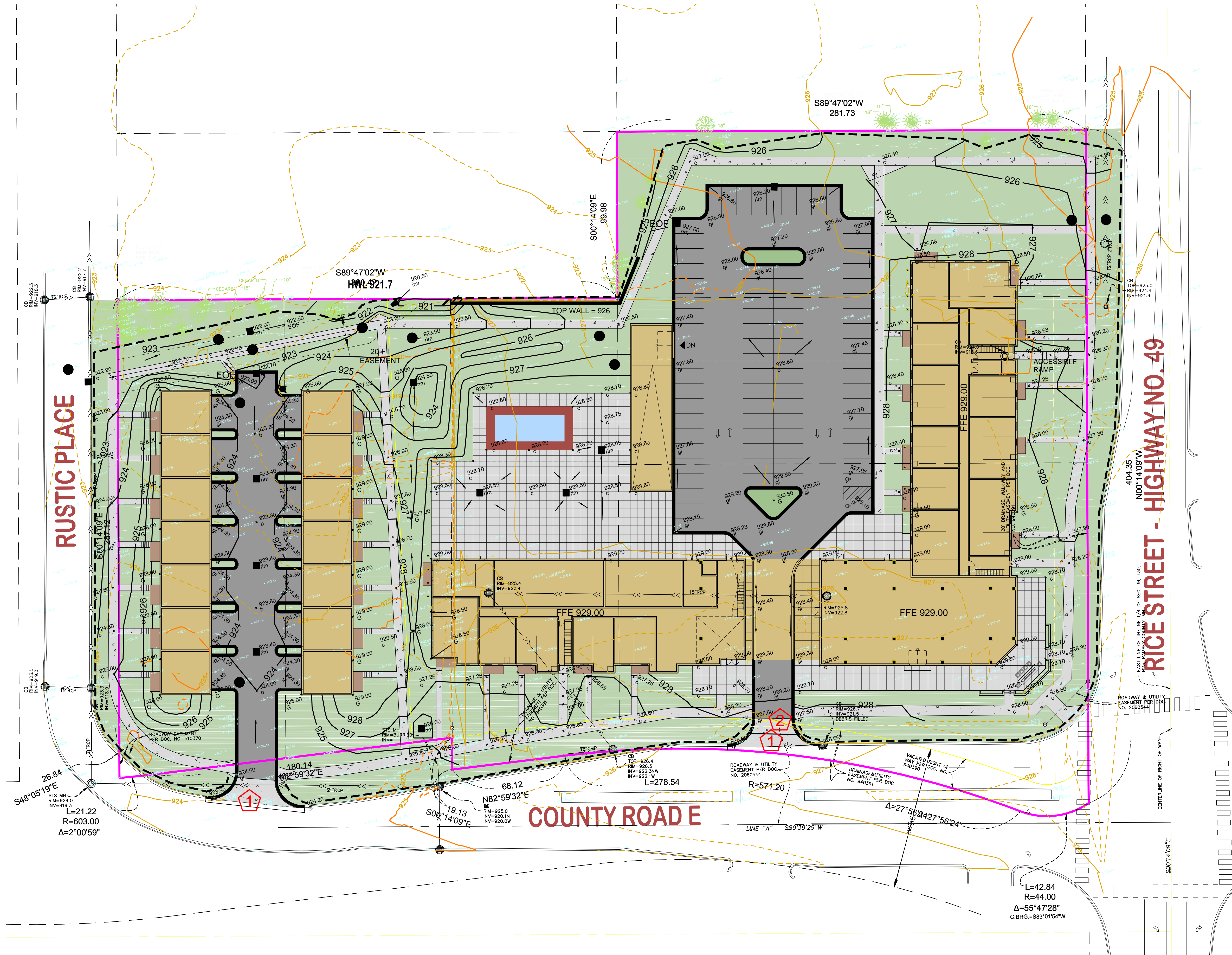
LAYOUT PLAN

SHEET NO.

C2

SHEET 2 OF 6

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LEGEND:

- EXISTING PROPERTY BOUNDARY
- EXISTING 1-FT CONTOUR
- PROPOSED 1-FT CONTOUR
- PROPOSED SPOT ELEVATION
- G = PROPOSED GROUND SURFACE
- gl = GUTTER LINE
- c = CONCRETE SURFACE
- b = BITUMINOUS SURFACE
- tw = TOP OF WALL
- bw = BOTTOM OF WALL
- NOTE: * REPRESENTS EXISTING GRADE.
- PROPOSED HEAVY DUTY SILT FENCE

KEYED NOTES:

- 1 MATCH EXISTING BITUMINOUS PAVEMENT ELEVATIONS.
- 2 PROPOSED ROCK CONSTRUCTION ENTERANCE. REFER TO DETAIL 6/C5.

Contractor SHALL notify District staff at least 48 hours prior to the construction of the infiltration basin in order to provide for District construction observation. Failure to notify the District prior to the installation may result in the removal of work completed. Contact Nicole Soderholm at 651-792-7976.

Contact Nicole Soderholm, District Inspector, at 651-792-7976 prior to beginning construction.

NOTES:

- ALL CONSTRUCTION AS CALLED FOR ON THESE CONTRACT DOCUMENTS SHALL BE PERFORMED IN ACCORDANCE WITH ALL OSHA REQUIREMENTS.
- THE GRADING CONTRACTOR SHALL SCHEDULE THE SOILS ENGINEER SO THAT CERTIFICATION OF ALL CONTROLLED FILLS WILL BE FURNISHED TO THE OWNER DURING AND UPON COMPLETION OF THE PROJECT.
- SPOT ELEVATIONS/CONTOURS SHOWN AS FINISHED GRADE ELEVATIONS.
- PRIOR TO ON SITE EXCAVATION OR DEMOLITION WORK, INSTALL EROSION CONTROL MEASURES IN LOCATIONS SHOWN OR AS DIRECTED BY THE ENGINEER OR CITY STAFF.
- EROSION CONTROL MEASURES SHOWN ON THE EROSION CONTROL PLAN ARE THE ABSOLUTE MINIMUM. THE CONTRACTOR SHALL INSTALL SEDIMENT TRAPS OR BASINS AND BIO LOG AS DEEMED NECESSARY TO CONTROL EROSION.
- GRADING OPERATIONS SHALL BE CONDUCTED IN A MANNER TO MINIMIZE THE POTENTIAL FOR SITE EROSION. SEDIMENT CONTROL PRACTICES MUST BE ESTABLISHED PRIOR TO THE START OF ANY UP GRADIENT LAND DISTURBING ACTIVITIES
- PROVIDE 6" OF NATIVE TOPSOIL IN GREEN AREAS.
- ALL EXPOSED SOIL AREAS MUST BE STABILIZED AS SOON AS POSSIBLE TO LIMIT SOIL EROSION BUT IN NO CASE LATER THAN 14 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.
- IF SEDIMENT EWALKS THE CONSTRUCTION SITE, OFF-SITE ACCUMULATIONS OF SEDIMENT MUST BE REMOVED IN A MANNER AND AT A FREQUENCY SUFFICIENT TO MINIMIZE OFF-SITE IMPACTS.
- TEMPORARY SOIL STOCKPILES MUST HAVE SILT FENCE OR OTHER EFFECTIVE SEDIMENT CONTROLS, AND CANNOT BE PLACED IN SURFACE WATERS, INCLUDING STORMWATER CONVEYANCES SUCH AS SWALES AND DITCHES UNLESS THERE IS A BYPASS IN PLACE FOR THE STORMWATER.
- SLOPES 3:1 AND GREATER SHALL BE STABILIZED WITH EROSION CONTROL BLANKET.
- MAINTAIN AND REPAIR EROSION CONTROL MEASURES (INCLUDING REMOVAL OF ACCUMULATED SILT) UNTIL VEGETATION IS ESTABLISHED. CONTRACTOR TO INSPECT AND DOCUMENT EROSION CONTROL DAILY AND AFTER ANY RAIN EVENT. ALL SEDIMENT CONTROL FEATURES MUST BE REPAIRED WHEN THE SEDIMENT REACHES 1/3 THE HEIGHT OF THE STRUCTURE, OR REPLACED WITHIN 24 HOURS OF DISCOVERY. EROSION CONTROL STRUCTURES FOUND DAMAGED MUST BE REPAIRED OR REPLACED WITHIN 24 HOURS UPON DISCOVERY. REMOVAL OF EROSION CONTROL STRUCTURES REQUIRED AFTER SITE IS STABILIZED (AT DIRECTION OF ENGINEER).
- ALL EXISTING CITY STREETS SHALL BE SWEEPED AS NEEDED AND AS REQUESTED BY ENGINEER OR CITY STAFF.
- REFER TO GEOTECHNICAL REPORT FOR ADDITIONAL REQUIREMENTS.
- AFTER GRADING OPERATIONS ARE COMPLETED, LANDWALK CONTRACTOR SHALL UNCOMPACT ALL GREEN AREAS PRIOR TO SODDING AND LANDSCAPING.
- PERMANENT RESTORATION IN LAWN AREAS SHALL CONSIST OF PLACING SOD PER MNDOT 3878.2.A SPECIFICATIONS. SOD STRIPS SHALL NOT HAVE DEAD OR DRY EDGES AND SHALL NOT BE CUT MORE THAN 24 HOURS IN ADVANCE OF DELIVERY.
- TEMPORARY STABILIZATION OF SLOPES AND GRADING AREAS DURING CONSTRUCTION SHALL BE A MNDOT 150 MIXTURE. TEMPORARY SEED MIXTURE SHALL BE PLACED WITH A DRILL AT A RATE OF 60 LBS/ACRE.
- SOD AND INSTALLATION OF EROSION CONTROL BLANKET SHALL BE COMPLETED WITHIN 48 HOURS OF FINAL GRADING.
- POSITIVE DRAINAGE OF MINIMUM 2% SLOPE SHALL BE ACHIEVED AWAY FROM PROPOSED BUILDING.

SWPPP SEQUENCING:

- OBTAIN MPCA PERMIT MINIMUM 7 DAYS PRIOR TO STARTING TREE CLEARING ACTIVITIES.
- INSTALL SWPPP MAILBOX WITH A COPY OF THE PLANS, SWPPP AND EROSION CONTROL LOGS.
- INSTALL ROCK CONSTRUCTION ENTRANCE AS SHOWN IN DETAILS.
- INSTALL PERMITER CATCH BASIN SEDIMENT CONTROL MEASURES (INLET PROTECTION).
- INSTALL SILT FENCING/BIO LOGS AS SHOWN ON PLAN PRIOR TO ANY SITE DISTURBANCE OR TREE REMOVAL. SITE PERIMETER FENCING SHALL BE INSTALLED PRIOR TO ANY SITE DISTURBANCE.
- INTERIOR SILT FENCE AND BIO LOG SEDIMENT CONTAINMENT SHALL BE PLACED TO CONTROL STOCKPILES OR CONCENTRATED AREAS OF FLOW DURING CONSTRUCTION AND AFTER SITE ROUGH GRADING IS COMPLETE.
- INSTALL ORANGE CONSTRUCTION FENCING AROUND INFILTRATION CHAMBER AREA. KEEP CONSTRUCTION TRAFFIC OFF OF THIS AREA UNTIL EXCAVATION OF SYSTEM IS TO START.
- INSTALL ORANGE CONSTRUCTION FENCING AROUND DRIP LINE OF TREES TO REMAIN.
- NOTIFY RAMSEY WASHINGTON METRO WATERSHED DISTRICT FOR APPROVAL OF PERIMETER EROSION CONTROL MEASURES PRIOR TO STARTING CONSTRUCTION (3 BUSINESS DAY NOTIFICATION REQUIRED).
- SEED ANY TOPSOIL STOCKPILE AND OR HYDROMULCH WITH MINIMUM 20/LBS PER ACRE OATS.
- INSTALL MPCA APPROVED CONCRETE CLEANOUT STATION PRIOR TO CONCRETE USE ON SITE.
- CONDUCT GRADING OPERATIONS.
- MAINTAIN LOGS OF RAIN EVENTS AND CORRECTIVE ACTIONS FOR EACH EVENT IN SWPPP MAILBOX PER MPCA NPDES PERMIT REQUIREMENTS.
- UTILIZE TEMPORARY HYDROMULCH AND SEEDING TO MAINTAIN SEDIMENT ON SITE.
- MAINTAIN AND CLEANOUT SILT FENCING AND CATCH BASIN SEDIMENT CONTROL AS INDICATED ON PLANS OR IN THE NPDES PERMITTING.
- AFTER INSTALLATION OF PROPOSED STORM SEWER, IMMEDIATELY INSTALL SEDIMENT CONTROL MEASURES.
- HYDROSEED ENTIRE SITE FOR FINAL STABILIZATION WITH PROPOSED SEED MIXTURE AND HYDROMULCH WITH TACKIFIER.

VERSA LOK BRONCO UNIT



Width (face) 67.5"
Height 30"
Depth 45"
Face Area 14 ft
Weight 4650 lbs
Max Unreinforced Height 10 ft
Max Geogrid Reinforced Height 150+ ft

*The maximum stable, unreinforced VERSA-LOK wall height in best conditions, may be lower depending on soil, site and loading conditions (including terraces). Taller walls need geogrid soil reinforcement, designed by a qualified engineer. Check your local building code requirements. Please contact your supplier or VERSA-LOK representative for assistance.

ERICKSON CIVIL
333 North Main Street, Suite 201
Stillwater, Minnesota 55082
Phone (612) 309-3804
www.ericksoncivilsite.com

DRAWING PHASE:	
OWNER REVIEW	
AGENCY REVIEW	✓
BID DOCUMENT	
PERMIT SET	
AS-BUILT DOCUMENT	

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TODD A. ERICKSON, PE
40418
LICENSE NO.
EDG PUD
DATE:

OWNER/DEVELOPER
ELEVAGE DEVELOPMENT GROUP, LLC
10901 Baltimore St. NE
Blaine, Minnesota 55449

PROJECT TITLE
SHOREVIEW MIXED USE
157 COUNTY ROAD E, W.
SHOREVIEW, MN 55126

NO.	REVISION DESCRIPTION	DATE
1	POND CONTOURS - MPCA	09/14/15

JOB NO. 15-172

SHEET TITLE

GRADING PLAN

SHEET NO.

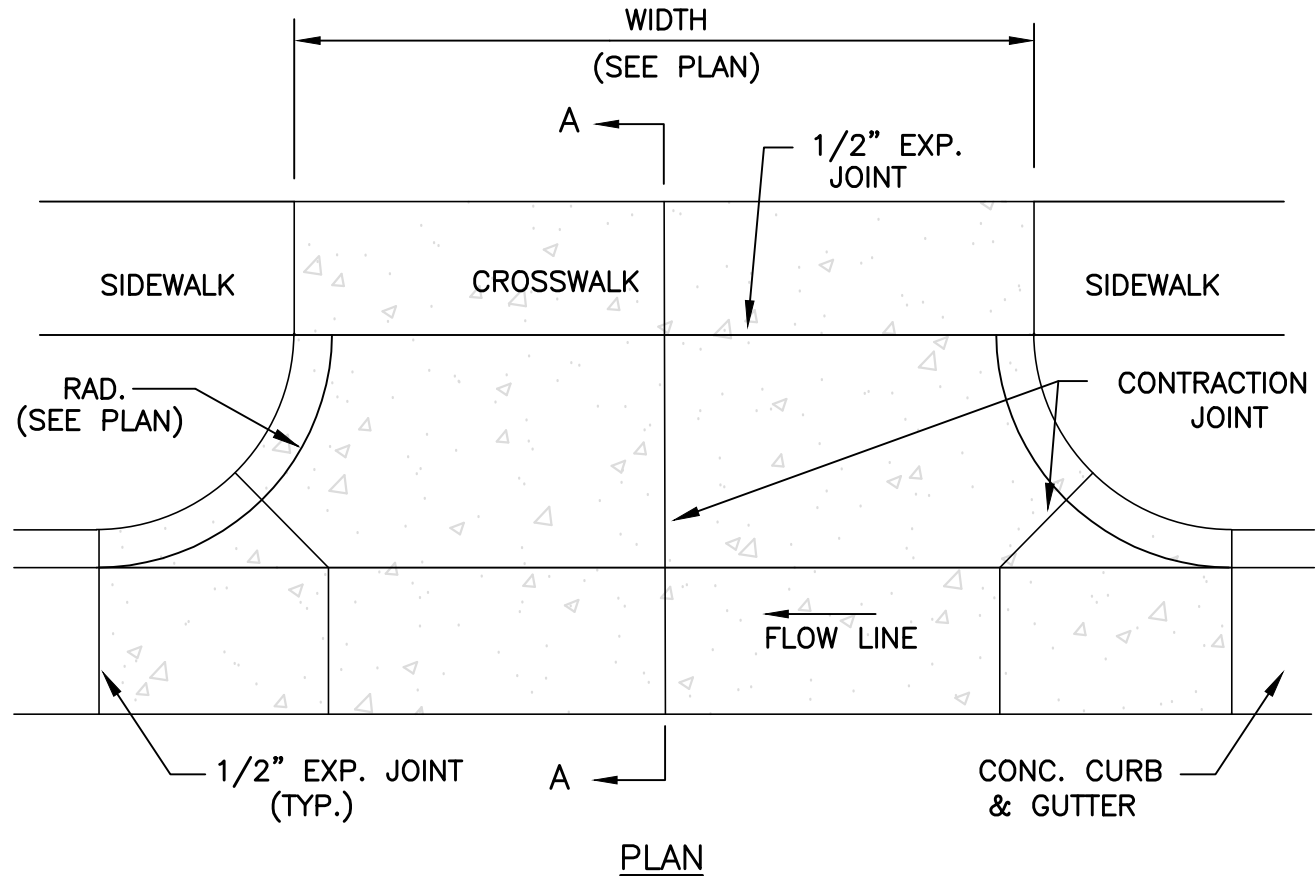
C3



- ### KEYED NOTES:

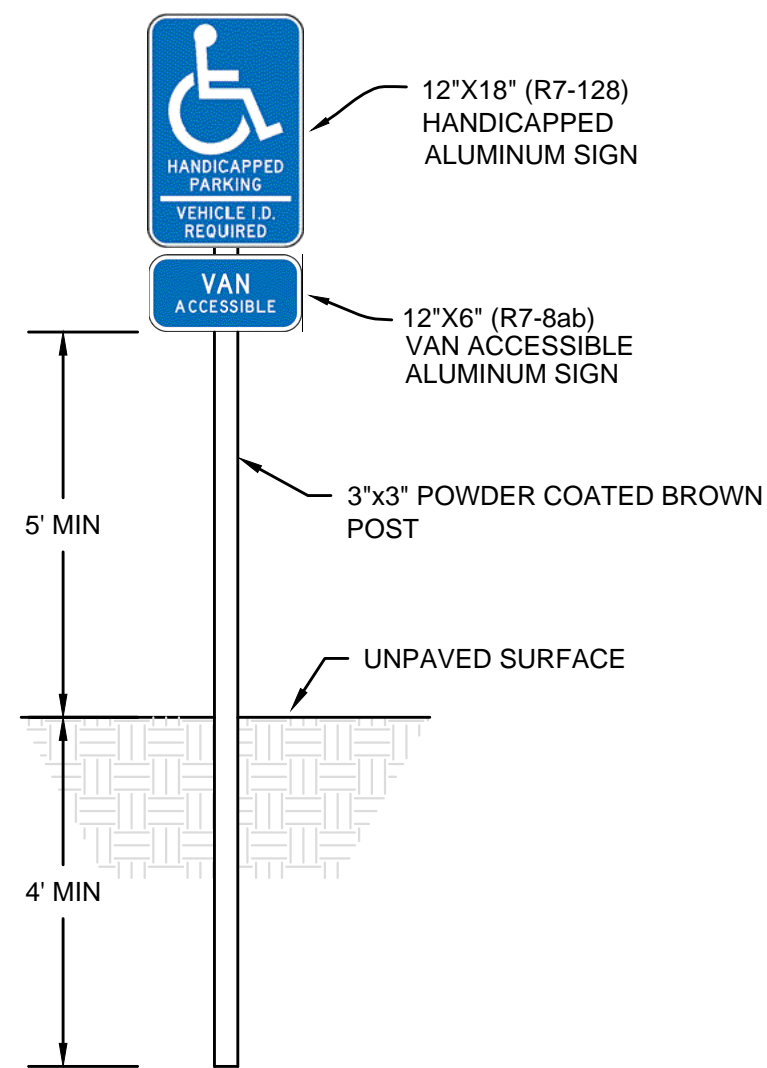
- NOTES:**

- 1) WATERMAIN: 4" & 8" DIP CL. 52 ALL MECHANICAL JOINTS. 8' MINIMUM BURY.
- 2) FIRE CONNECTION PER THE SHOREVIEW REQUIREMENTS.
- 3) SANITARY SEWER: 8" PVC SCHEDULE 40
- 4) STORM SEWER: RCP (CLASS AS SHOWN ON PLAN)
- 5) PRESSURE TEST SANITARY SEWER PER CITY OF SHOREVIEW REQUIREMENTS
- 6) HYDROTEST WATERMAIN PER CITY OF SHOREVIEW REQUIREMENTS.
- 7) BACTERIA TEST WATER SERVICE PER CITY OF SHOREVIEW REQUIREMENTS.

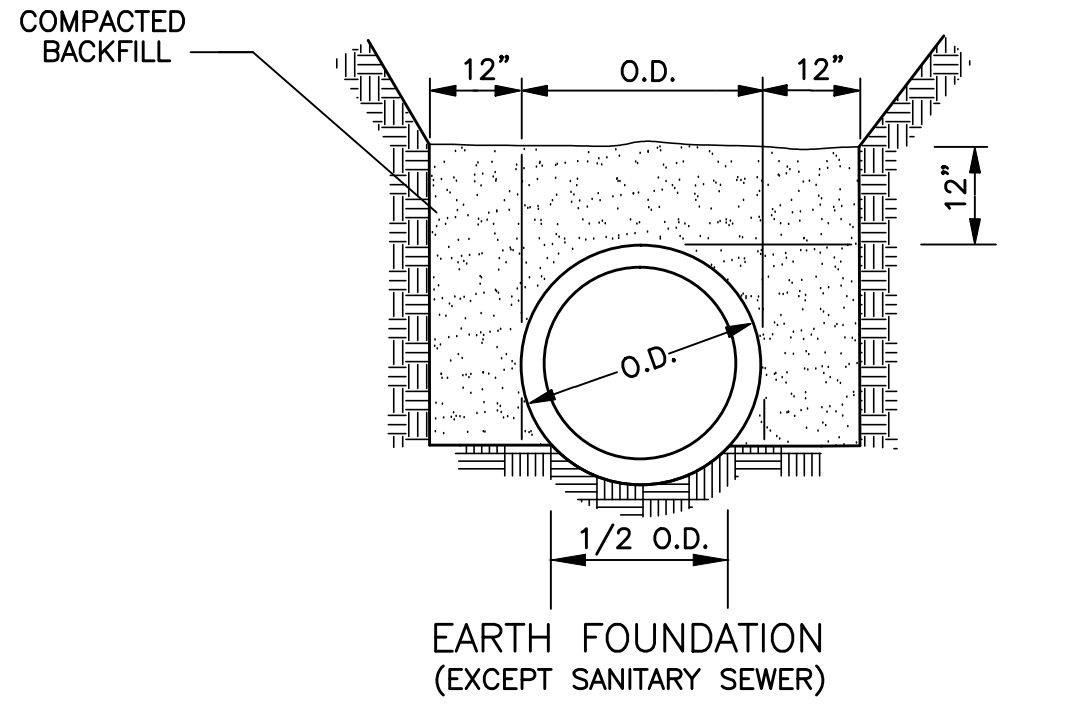


- NOTES :
- PANEL WIDTH SHALL NOT EXCEED 10' WITHOUT @ CONTR. JOINT.
 - WHERE SIDEWALK EXISTS OR IS PROPOSED, CROSSWALK SHALL BE SAME THICKNESS AS DRIVEWAY.

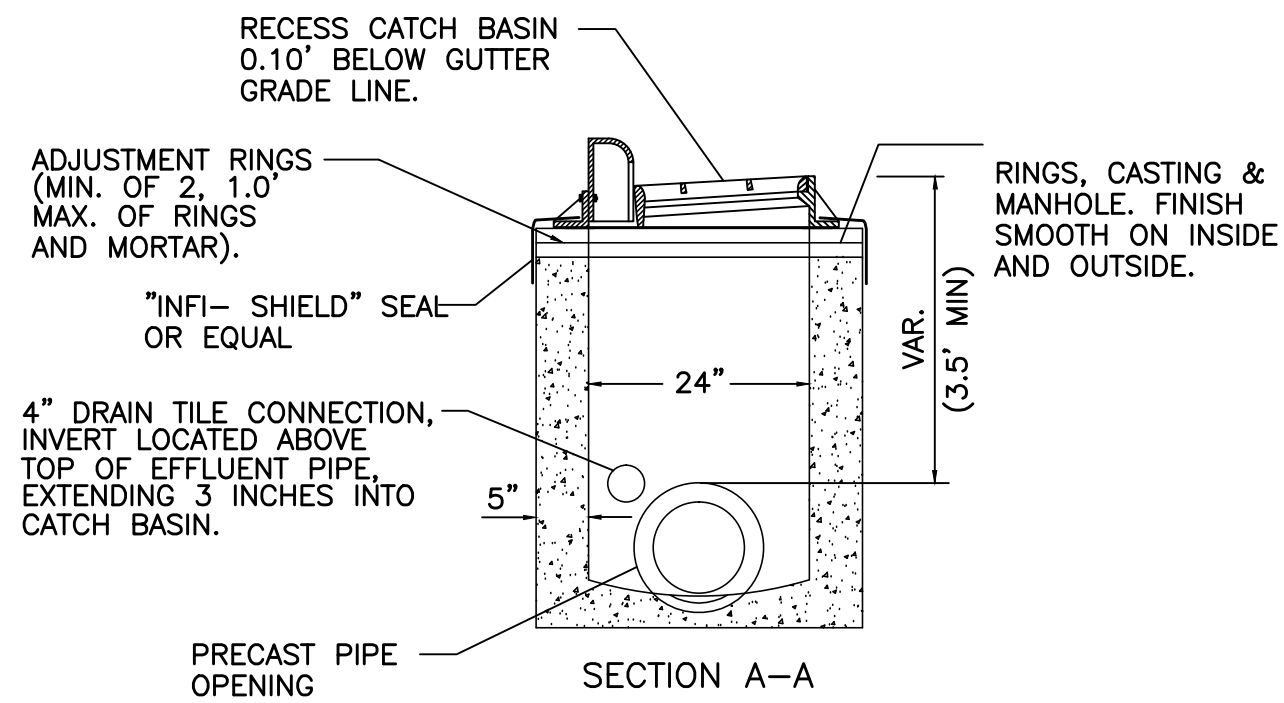
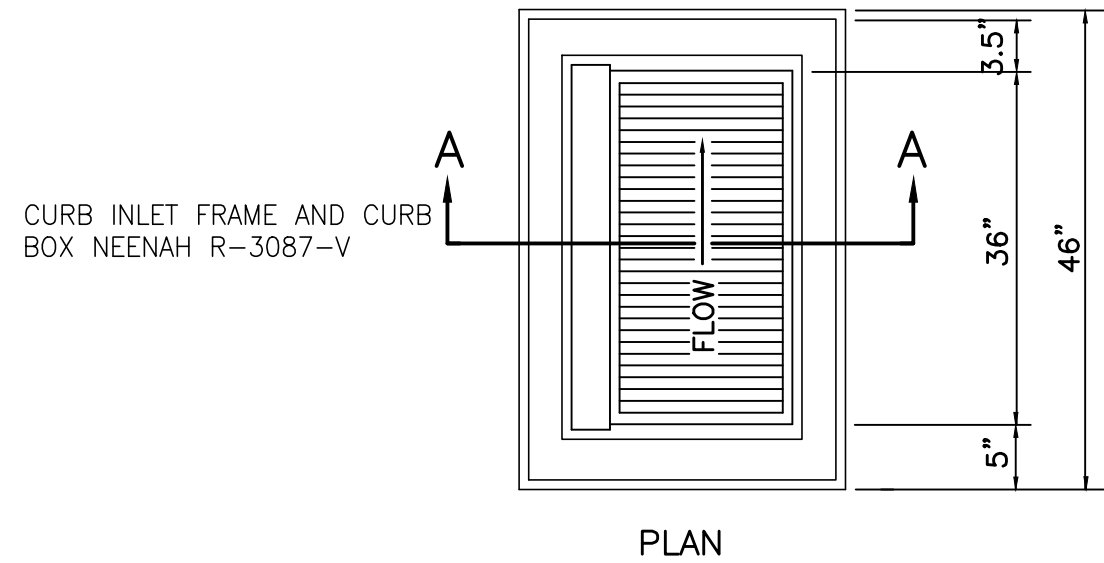
10 CONCRETE DRIVEWAY
C5 NO SCALE



11 ADA SIGN INSTALLATION
C2 NO SCALE

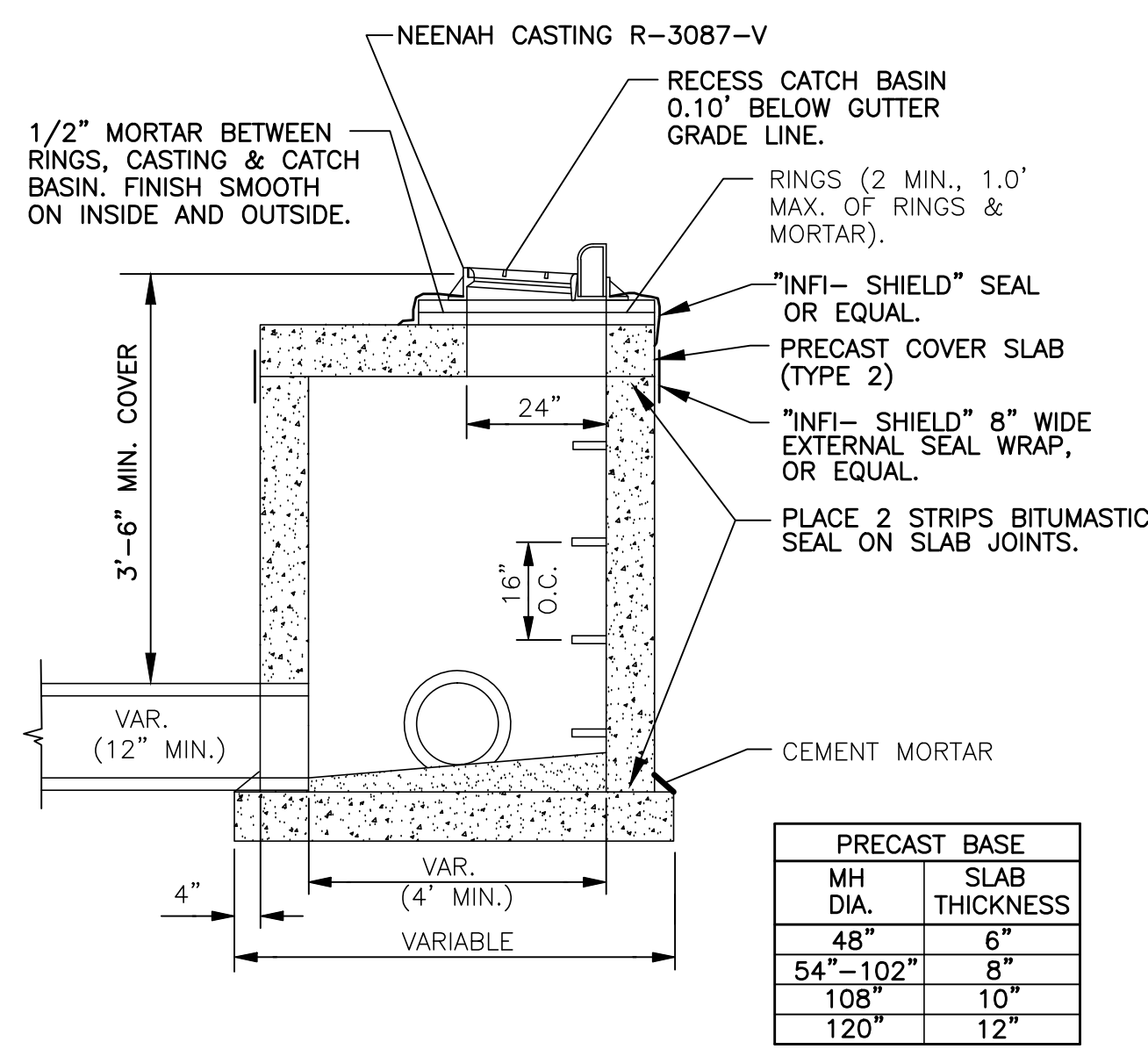


7 PIPE FOUNDATION DETAILS
C5 NO SCALE



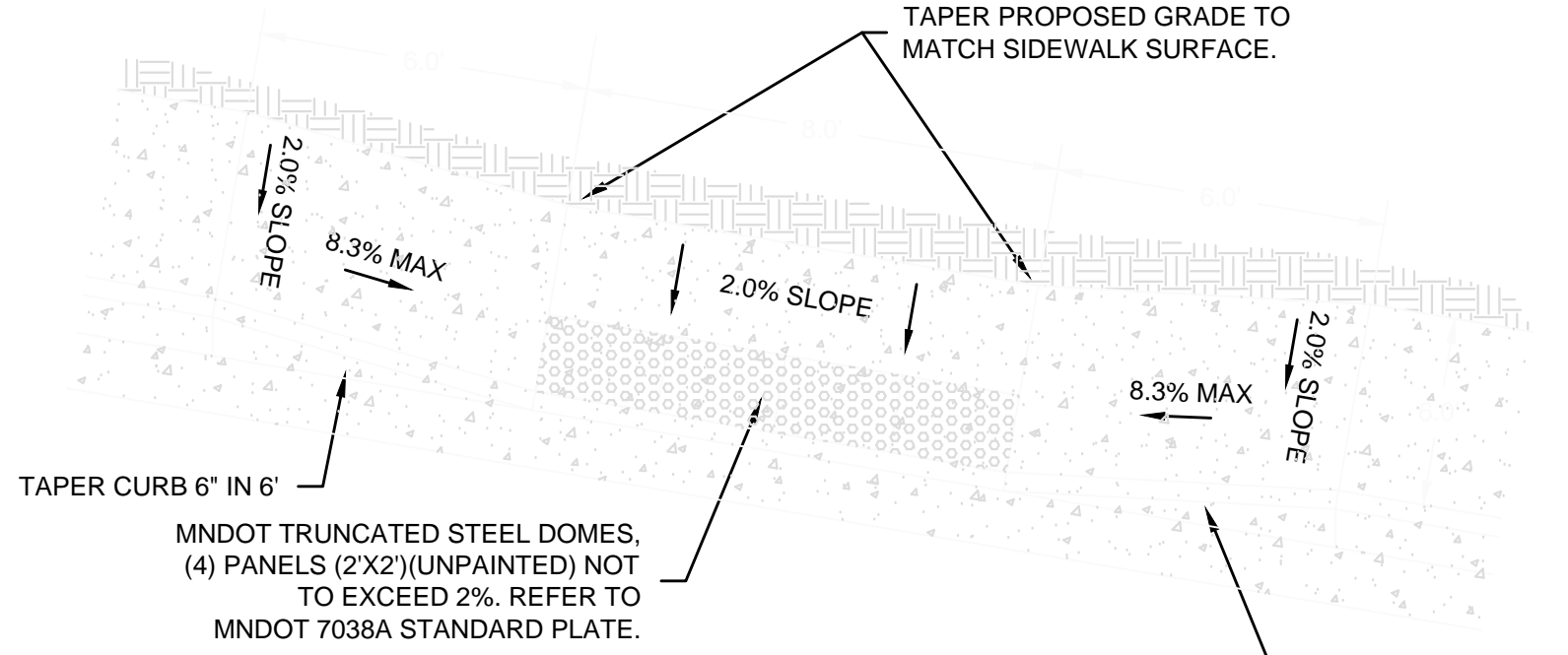
PRECAST REINFORCED CONCRETE CATCH BASIN & BASE SLAB
PER ASTM C478

8 CATCH BASIN
C5 NO SCALE

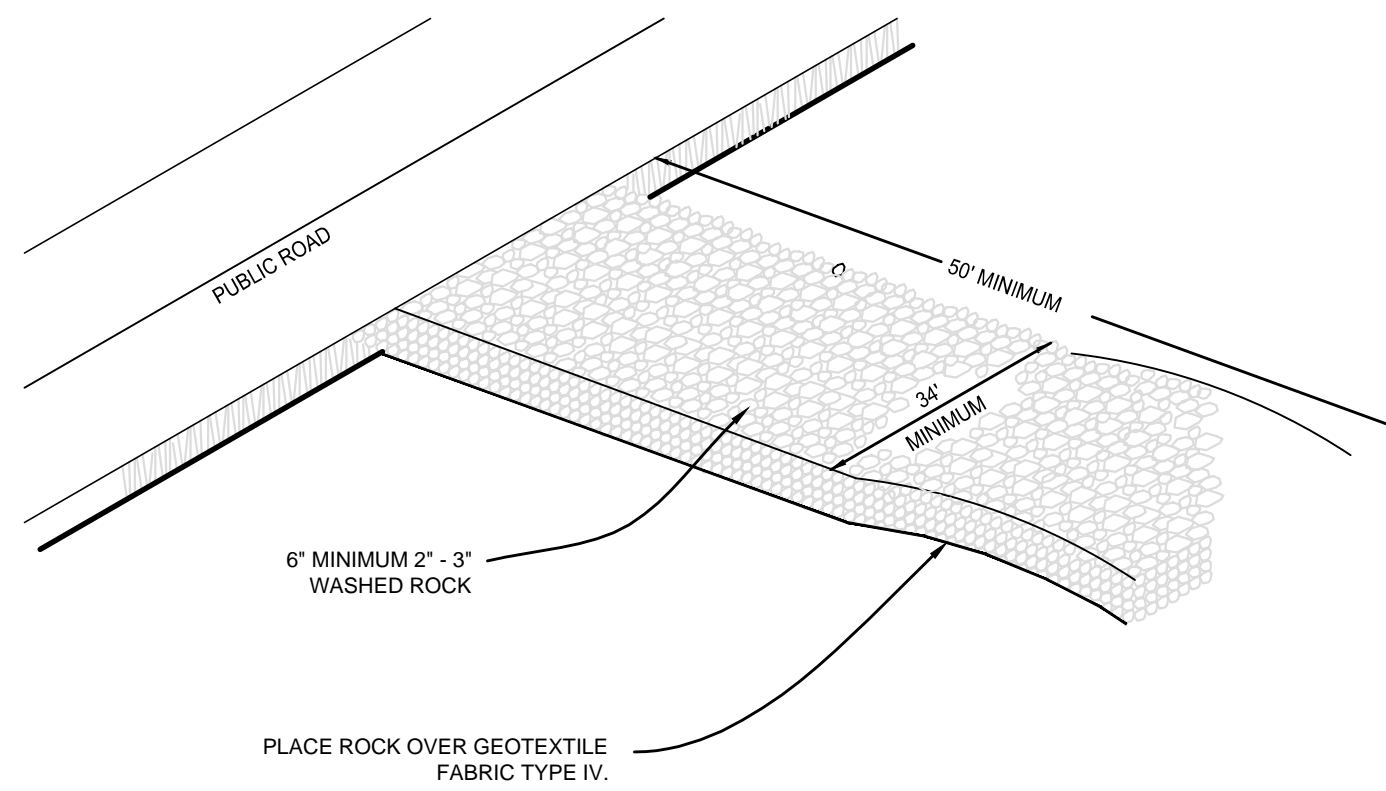


NO WOOD SHALL BE USED FOR ADJUSTING CASTING; CEMENT MORTAR ONLY.
CAST IRON FRAME & GRATE CASTINGS PER SPECIFICATIONS.
MANHOLE STEPS SHALL BE CAST IRON, ALUMINUM OR STEEL REINFORCED PLASTIC PER ASTM C478. LOCATION SHALL BE AS NOTED IN THE SPECIFICATIONS.
PRECAST REINFORCED CONCRETE BASE SLAB & COVER SLAB PER ASTM C478,
PRECAST REINFORCED CONCRETE MANHOLE SECTIONS PER ASTM C478,
OR 8\"/>

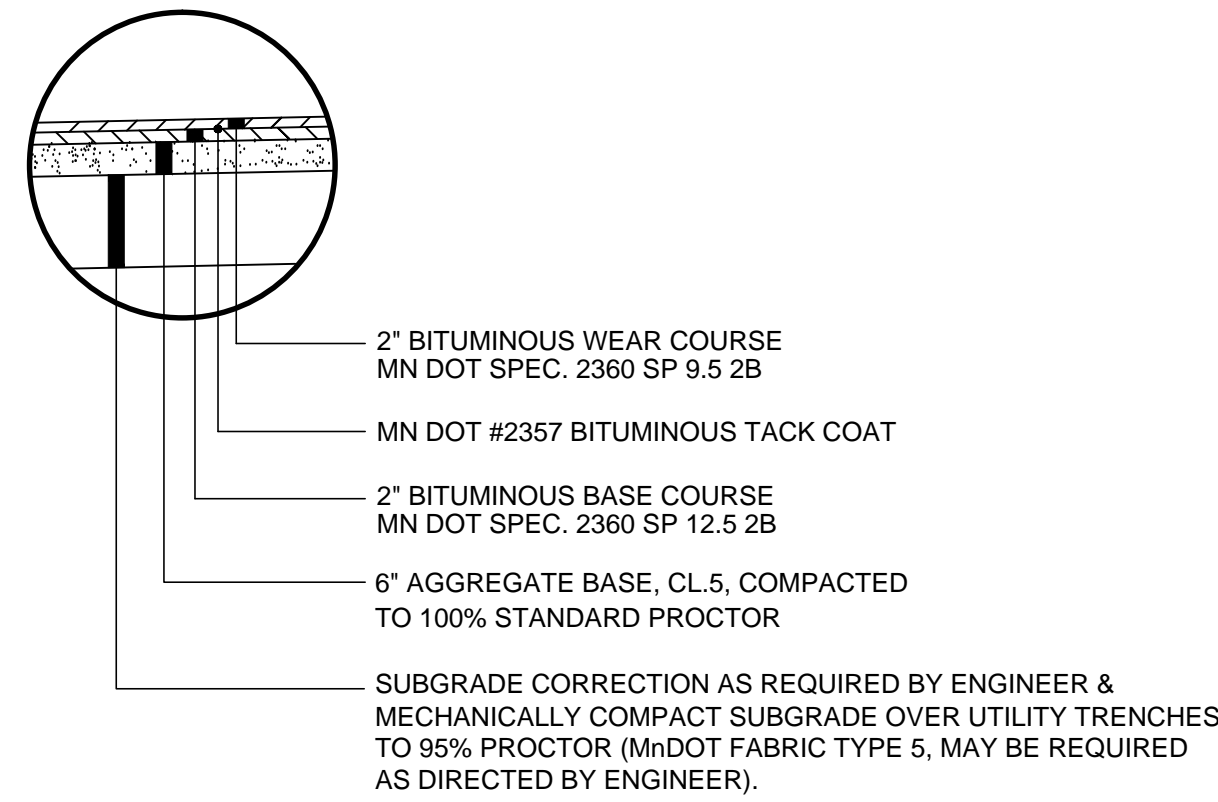
9 CATCH BASIN/MANHOLE
C5 NO SCALE



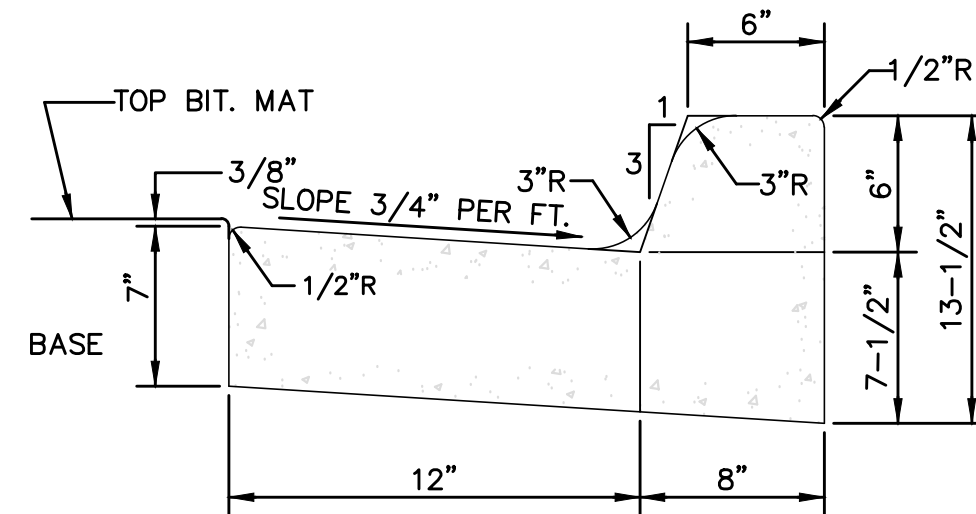
5 PARALLEL CURB RAMP
C5 NO SCALE



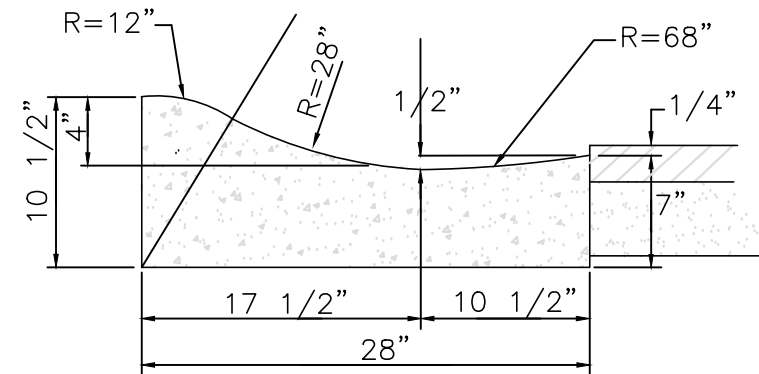
6 ROCK CONSTRUCTION ENTRANCE
C5 NO SCALE



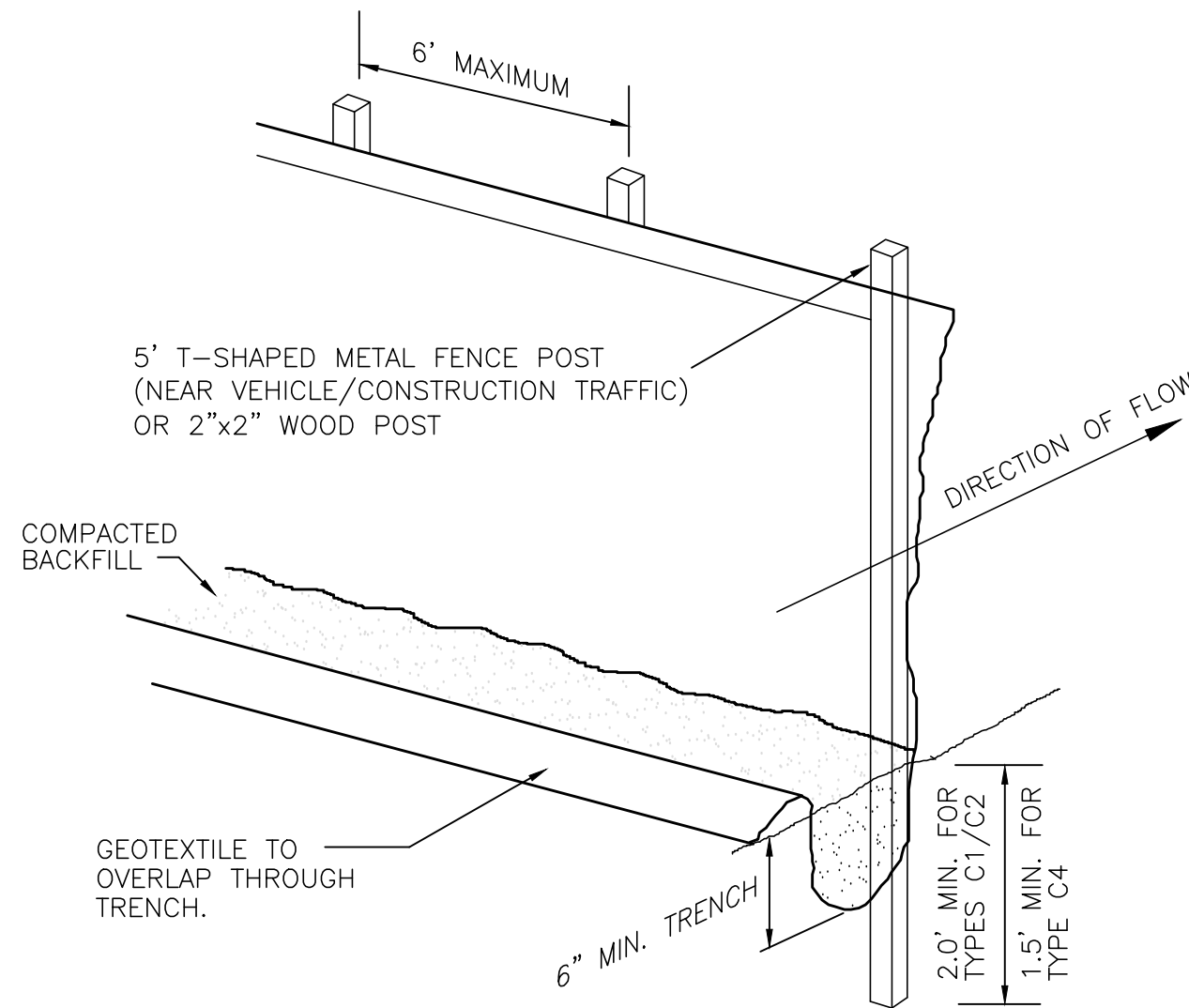
1 BITUMINOUS PAVING SECTION
C5 NO SCALE



2 B612 CURB & GUTTER
C5 NO SCALE

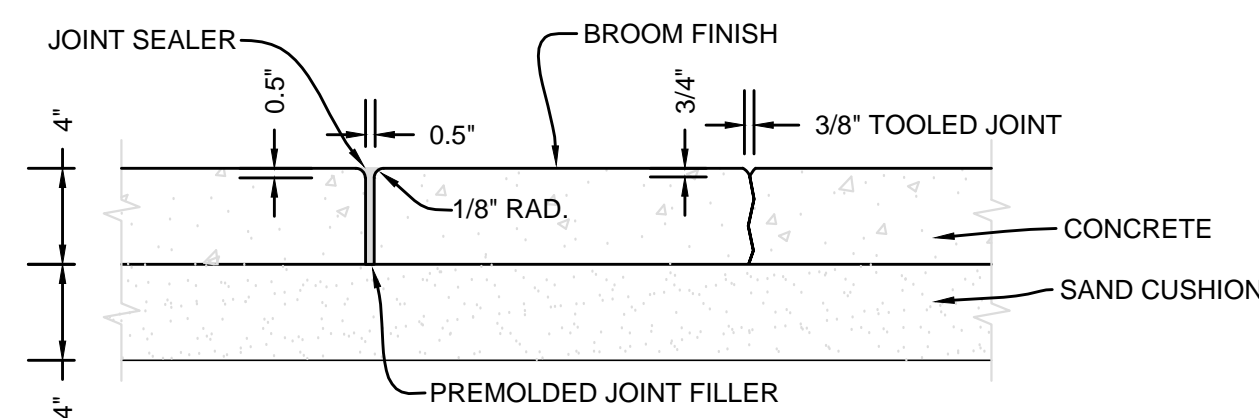


3 SURMOUNTABLE CURB & GUTTER
C5 NO SCALE



NOTE : -SILT FENCE INSTALLATION SHALL CONFORM TO MNDOT2573.3, TYPE C1/C2 NEAR VEHICLE/CONSTRUCTION TRAFFIC, TYPE C4 AT ALL OTHER LOCATIONS.
-MATERIALS SHALL CONFORM TO MNDOT 3886.

6 SILT FENCE
C5 NO SCALE



NOTES:

TOOLED JOINTS:

- JOINT SPACING SHALL MATCH WALK WIDTH UNLESS SHOWN OTHERWISE ON PLANS.

EXPANSION JOINTS:

- WHERE WALK BUTTS ANY FIXED OBJECT SUCH AS WALLS, CURBS, MANHOLES, ETC.
- 45\"/>
- JOINT SEALER SHALL MEET ASTM D-412, GRAY, SELF LEVELING, EPOXY, AS WITH \"QUICKJOINT 300\" OR EQUAL.

4 CONCRETE SIDEWALK DETAIL
C5 NO SCALE

DRAWING PHASE:

OWNER REVIEW
AGENCY REVIEW
BID DOCUMENT
PERMIT SET
AS-BUILT DOCUMENT

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10901 Baltimore St. NE
Blaine, Minnesota 55449

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157 COUNTY ROAD E, W.
SHOREVIEW, MN 55126

NO.	REVISION	DESCRIPTION	DATE

JOB NO. 15-172

SHEET TITLE

DETAILS

SHEET NO.

C5

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CIVIL

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www.ericksoncivilsite.com

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✓ AGENCY REVIEW

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SHEET TITLE

DETAILS

SHEET NO.

C6

SHEET 6 OF 6

NEENAH R-1772 CASTING (SOLID LID)

12" CONCRETE REINFORCEMENT COLLAR

TURF

GEOTEXTILE FABRIC

42" PERFORATED PIPE WITH END CAPS

CONTECH ALUMINIZED STEEL

GEOTEXTILE FABRIC

UNDISTURBED EARTH

6" TYP.

BASE ELEVATION = 914.50

NOTES:

- ALL REFERENCES TO CLASS I OR II MATERIAL ARE PER ASTM D2321 "STANDARD PRACTICE FOR UNDERGROUND INSTALLATION OF THERMOPLASTIC PIPE FOR SEWERS AND OTHER GRAVITY FLOW APPLICATIONS", LATEST EDITION.
- ALL RETENTION AND DETENTION SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH ASTM D2321, LATEST EDITION AND THE MANUFACTURER'S PUBLISHED INSTALLATION GUIDELINES.
- MEASURES SHOULD BE TAKEN TO PREVENT THE MIGRATION OF NATIVE FINES INTO THE BACKFILL MATERIAL, WHEN REQUIRED. SEE ASTM D2321.
- FILTER FABRIC:** A GEOTEXTILE FABRIC MAY BE USED AS SPECIFIED BY THE ENGINEER TO PREVENT THE MIGRATION OF FINES FROM THE NATIVE SOIL INTO THE SELECTED BACKFILL MATERIAL.
- FOUNDATIONS:** WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH SUITABLE MATERIAL AS SPECIFIED BY THE ENGINEER. AS AN ALTERNATIVE AND AT THE DISCRETION OF THE DESIGN ENGINEER, THE TRENCH BOTTOM MAY BE STABILIZED USING A GEOTEXTILE MATERIAL.
- BEDDING:** SUITABLE MATERIAL SHALL BE CLASS I AND II. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER.
- INITIAL BACKFILL:** SUITABLE MATERIAL SHALL BE CLASS I OR II IN THE PIPE ZONE EXTENDING NOT LESS THAN 6" ABOVE CROWN OF PIPE. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. MATERIAL SHALL BE INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION.

6 UNDERGROUND STORAGE PIPE
C6 NO SCALE

8.0' Minimum cover required over top of water main.

PLAN

Stainless Steel Tapping Sleeve

Provide copper conductivity strap to bypass tapping sleeve. Minimum 1/16" x 3/4" wide flat copper strip.

SECTION A-A

Resilient Wedge Valve Conforming to AWWA C-509-80 standards

Compacted Granular Backfill

Undisturbed Earth

8" Conc. block

Thrust Block

4 WATER MAIN WET TAP
C6 NO SCALE

NEENAH R-4040

Bell End

One piece 8" PVC (white)

100' MAX.

8" PVC

12" x 8" RED.

Long Sweep 90° Bend

Encase PVC bend in concrete Min. 1/3 Cu. Yds.

8" PVC

12" x 8" RED.

Long Sweep 45° Bend

Encase PVC wye in concrete Min. 1/3 Cu. Yds.

END OF LINE

IN LINE

7 COURTYARD DRAINAGE SYSTEM
C6 NO SCALE

WATERMAIN

HYDRANT

GATE VALVE & BOX

BACK OF CURB

VARIES 2' VARIES 3'

PROP. LINE

PROP. LINE

SEE PLAN

24"

13"

HYDRANT FLAG, FLEX STAKE - FH 800 SERIES OR APPROVED EQUAL.

WATEROUS PACER HYDRANT AS APPROVED BY CITY OF SHOREVIEW

ATTACH TRACER WIRE TO BREAKAWAY FLANGE (WHEN APPLICABLE)

18" LONG 3/4" PVC CONDUIT (TRACER WIRE ONLY)

TRACER WIRE (WHEN APPLICABLE)

POLYETHYLENE ENCASUREMENT AROUND FULL LENGTH OF HYDRANT

COVER WITH GEOTEXTILE FABRIC MNDOT 3733 TYPE III

CONCRETE BLOCK REACTION BACKING TO VIRGIN SOIL

1 1/2" WASHED ROCK ALL AROUND MIN. 1' C.Y.

MANHOLE BLOCK

SEE STD. DWG. 203

COVER AS SPECIFIED

MNDOT 3733 TYPE III GEOTEXTILE FABRIC

USE "MEGA LUG" BRAND MECHANICAL JOINT THRUST RETAINER GUARDS AT ALL JOINTS.

NOTES:

- USE MEGA LUGS ON ALL JOINTS. PROVIDE CONCRETE BLOCKING BEHIND HYDRANT AND TEE.
- USE COR-BLUE T BOLTS ON ALL FITTINGS.
- PLUG WEEP HOLES IF GROUNDWATER IS PRESENT.
- IF WEEP HOLES ARE PLUGGED, PUMPER NOZZLE CAP MUST BE PAINTED GREEN AND A TAG SHALL BE ATTACHED TO THE HYDRANT STATING "DRAIN HOLES PLUGGED".
- SUPPLY 1 EXTRA FLEX STAKE FOR EVERY 10 HYDRANTS INSTALLED. MINIMUM ONE PER PROJECT.
- SUPPLY 1 HYDRANT STORZ WRENCH OR HYDRANT REPAIR KIT PER PROJECT.
- SUPPLY 1 6" HYDRANT EXTENSION AS DIRECTED BY THE ENGINEER.
- ALL HYDRANTS NOT IN SERVICE SHALL BE COVERED WITH BLACK POLY PLASTIC.

*EXTRA STAKES AND REPAIR KITS ARE TO BE DELIVERED TO THE CITY GARAGE.

5 TYPICAL HYDRANT LAYOUT (VALVE IN BOULEVARD)
C6 NO SCALE

O.D. + 2'

O.D.

12"

1/2" MORTAR BETWEEN

COMPACTED BACKFILL

* ASTM D2321 EMBEDMENT MATERIAL (MnDOT SPEC. 3149.2F)

PIPE INSTALLATION IN ACCORDANCE WITH ASTM D 2321, TABLE 2.

* EMBEDMENT MATERIAL SHALL CONFORM TO ASTM D2321, TABLE 1.

3 CONCRETE REACTION BACKING (FOR BENDS)
C6 NO SCALE

1-1/2" O.D. POWDER COAT BLACK RAILING SCH. 40 RAILING (ALL)

CORE DRILL AND EPOXY RAILING IN PLACE

SEE PLANS FOR # OF STEPS THIS LENGTH INCREASES WITH GREATER NUMBER OF STEPS

15.58'

20.22

3.58

12.00'

10.50

63.435'

35.0

43.50

11.74'

12.00

30.0

6.00

12.00

2 TYPICAL EXTERIOR RAILING DETAIL
C6 NO SCALE

UNDISTURBED SOIL

40 MIN.

ELEVATION

UNDISTURBED SOIL

30 MIN.

PLAN

SECTION

2500 PSI CONCRETE 28 DAY STRENGTH

1.5'

D

6" MIN.

3 CONCRETE REACTION BACKING (FOR BENDS)
C6 NO SCALE

DRAWING PHASE:
OWNER REVIEW
✓ AGENCY REVIEW
BID DOCUMENT
PERMIT SET
AS-BUILT DOCUMENT

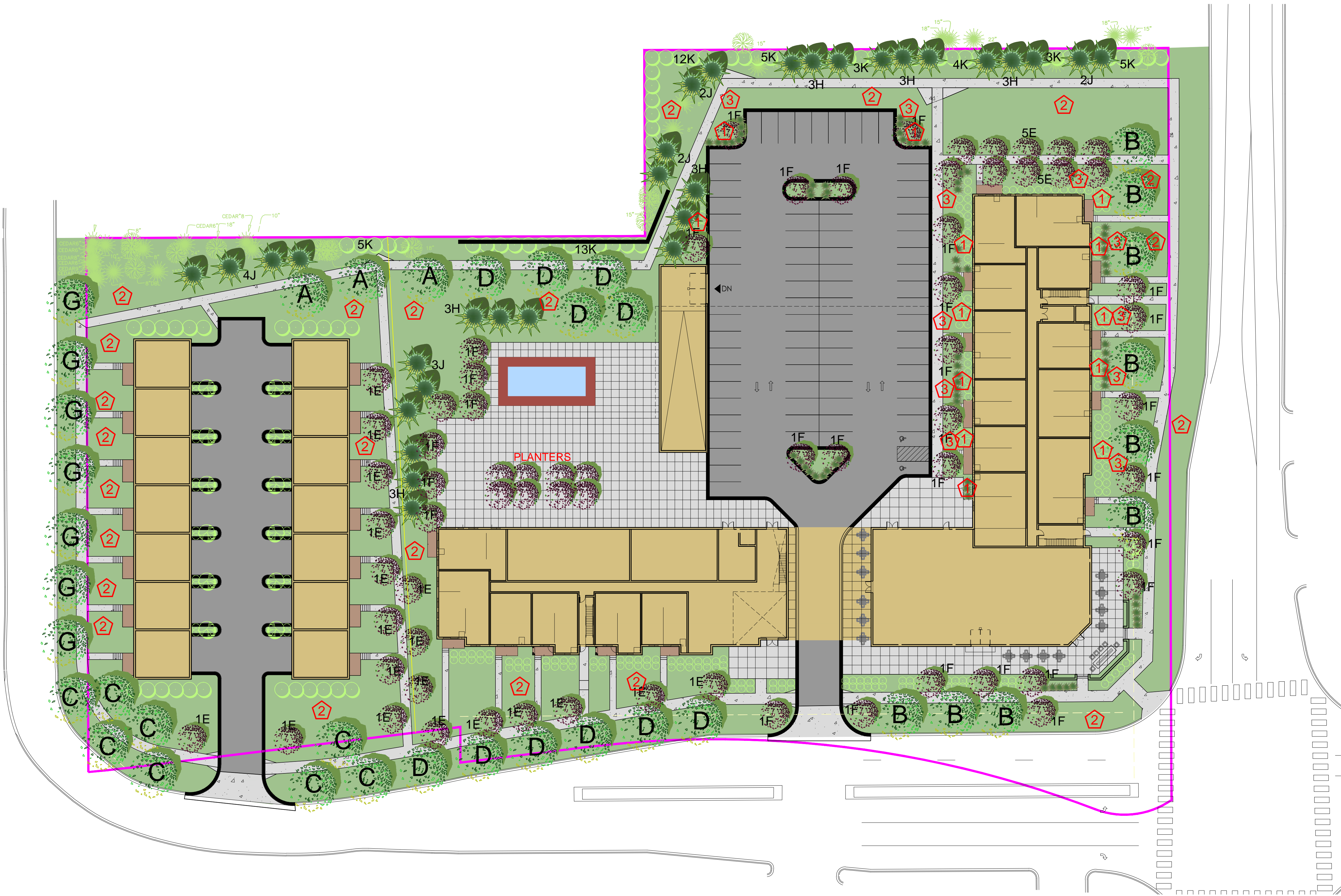
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SHOREVIEW, MN 55126

NO.	REVISION	DESCRIPTION	DATE



LEGEND:

- PROPERTY BOUNDARY
- DENOTES PROPOSED DECIDUOUS TREE
- DENOTES PROPOSED ORNAMENTAL TREE
- DENOTES PROPOSED CONIFEROUS TREE
- DENOTES PROPOSED SHRUB/PERENNIAL BED (2 TO 4-FT MATURITY SIZE)
- DENOTES PROPOSED #5 CONIFEROUS SHRUB (2 TO 3-FT MATURITY SIZE)

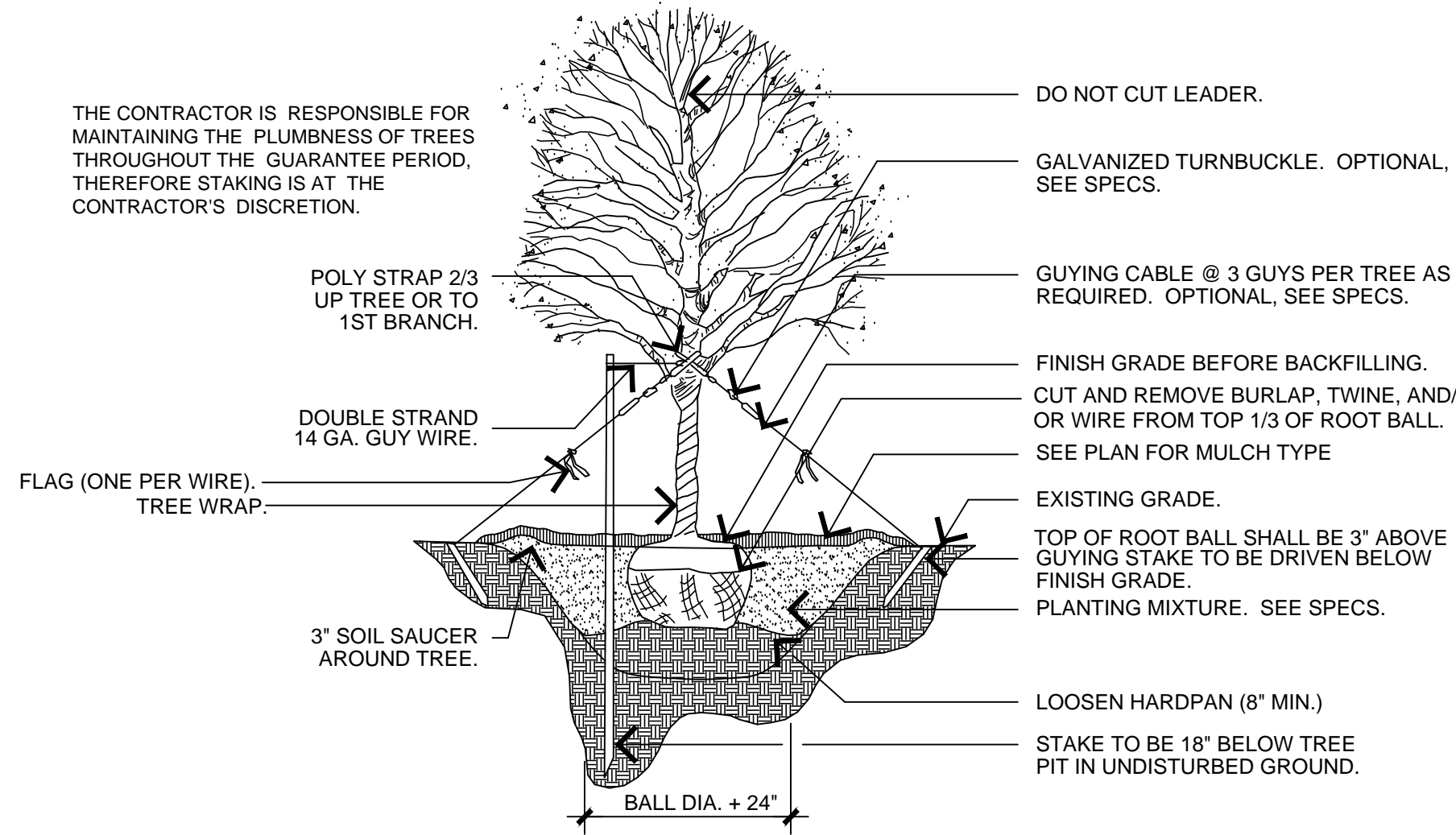
KEYED NOTES:

- 1 6" COARSE SHREDED HARDWOOD MULCH (NO FABRIC)
- 2 6" OF TOPSOIL AND HYDROSEED 220 LBS./ACRE LOW GROW FESCUE WITH 20 LBS./ACRE ANNUAL RYE, WITH TACKIFIER AND FERTILIZER.
- 3 HEAVY DUTY BROWN METAL EDGING.
- 4 4" OF 2'-4" LIMESTONE ROCK OVER 6 MIL BLACK POLY

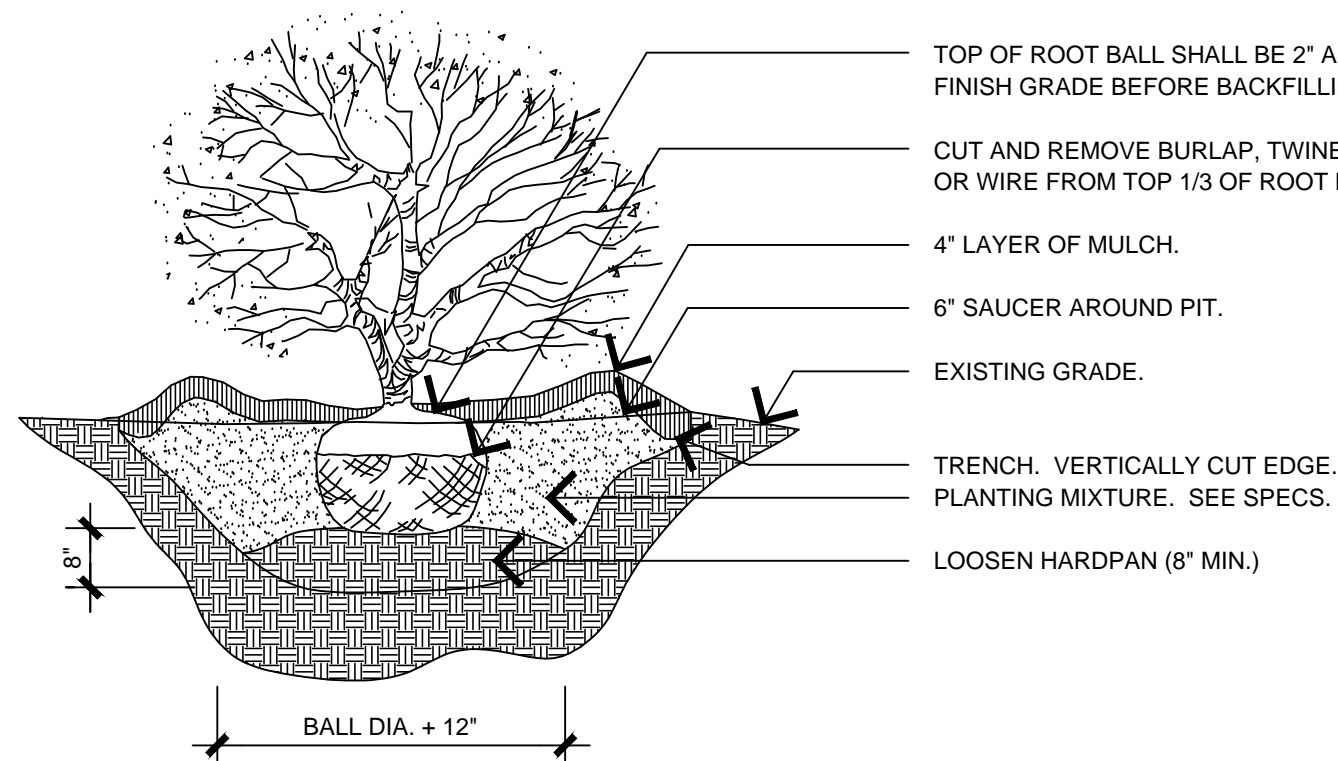
NOTES:

- 1) ALL PLANTING BEDS SHALL RECEIVE MIN. 6" TOPSOIL. HEAVY LANDSCAPE FABRIC AND BROWN METAL EDGING WHERE MULCH MEETS LAWN.
- 2) ALL LAWN AREAS SHALL RECEIVE A MINIMUM OF 6" OF TOPSOIL.
- 3) ENTIRE SITE SHALL BE IRRIGATED. DESIGN/BUILD BY CONTRACTOR.
- 4) HYDROMULCH WITH TACKIFIER ALL OPEN AREAS WITH LOW GROW FESCUE MIX FROM TWIN CITY SEED COMPANY AND BE SEEDED AT A RATE OF 220 LBS. PER ACRE, W/ 20LBS/ACRE ANNUAL RYE.
- 5) BUILDING CONTRACTOR SHALL INCLUDE COST OF ELECTRICAL AND PLUMBING FOR INSTALLATION OF IRRIGATION SYSTEM. IRRIGATION CONTROL BOX SHALL BE BY LANDSCAPE CONTRACTOR.
- 6) MAINTENANCE STRIP AND OTHER AREAS IDENTIFIED FOR MULCH SHALL BE 4" MINUS WASHED LIMESTONE PLACED AT A MIN. DEPTH OF 4" OVER 6 MIL POLY. PERENNIAL AND ANNUAL PLANTING AREAS SHALL RECEIVE 6" OF COARSE SHREDED HARDWOOD MULCH WITH NO FABRIC.

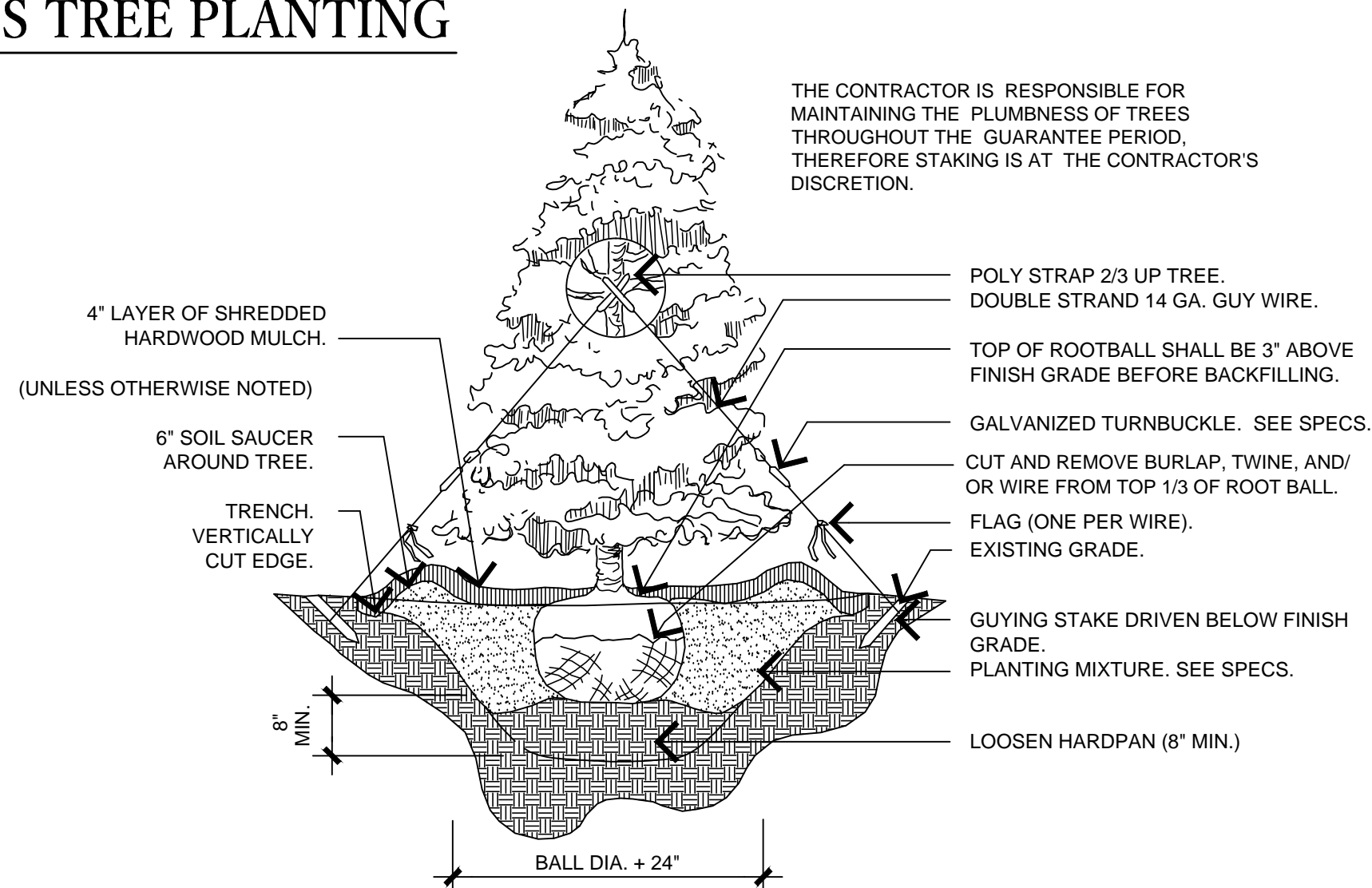
	Plant Name	Latin Name	Quantity	Size	Type
A	Autumn Blaze Maple	Acer x freemanii "Jeffersred"	3	2-1/2" DIA.	B&B
B	Skyline Honey Locust	Gleditsia triacanthos 'Skycole'	9	2-1/2" DIA.	B&B
C	Regal Prince Oak	Quercus 'Regal Prince'	8	2-1/2" DIA.	B&B
D	Sunburst Honey Locust	Gleditsia triacanthos 'Suncole'	10	2-1/2" DIA.	B&B
E	Adirondack Flowering Crab	Malus 'Adirondack'	29	2-1/2" DIA.	B&B
F	Snowdance Japenese Lilac	Syringa reticulata 'Snowdance'	23	2-1/2" DIA.	B&B
G	Royal Red Norway Maple	Acer platanoides "Royal Red"	6	2-1/2" DIA.	B&B
H	Fat Albert Blue Spruce	Picea pungens 'Fat Albert'	18	8-FT	B&B
J	Black Hills Spruce	Picea glauca 'Densata'	10	10-FT	B&B
K	Miss Kim Lilac	Syringa patula 'Miss Kim'	50	#5	Container
L	Diablo Nine Bark	Physocarpus opulifolius 'Monlo.' "Diablo"		#5	Container
M	Spiraea Tor Birchleaf	Spiraea betulifolia 'Tor'		#2	Container
N	Purple Emperor Sedum	Sedum 'Purple Emperor'		#1	Container
O	Strawberry Candy Daylily	Hemerocallis 'Strawberry Candy'		#1	Container
P	Karl Foerster Reed Grass	Calamagrostis x acutiflora 'Karl Foerster'		#1	Container



DECIDUOUS TREE PLANTING



SHRUB PLANTING



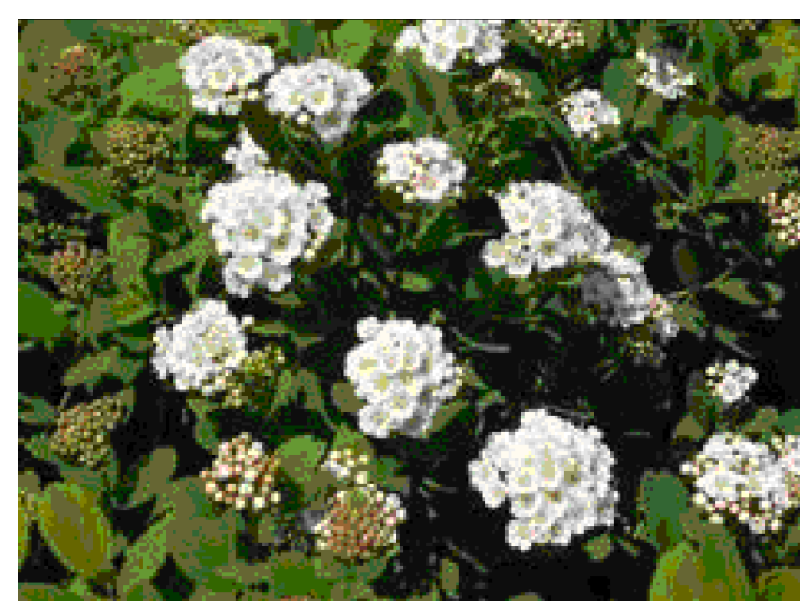
CONIFEROUS TREE PLANTING



Strawberry Candy Daylily



Karl Foerster Reed Grass



Spiraea Tor Birchleaf



Purple Emperor Stonecrop

DRAWING PHASE:

OWNER REVIEW

✓ AGENCY REVIEW

	BID DOCUMENT
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	PERMIT SET
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TODD A. ERICKSON, PE
40418

LICENSE NO.

EDG PUD

DATE: _____

WINNER/DEVELOPER

LEVELAGE DEVELOPMENT GROUP, LLC

ST TITLE

SHOREVIEW MIXED USE
157 COUNTY ROAD E, W.
SHOREVIEW, MN 55126

[illegible]

JOB NO. 15-172

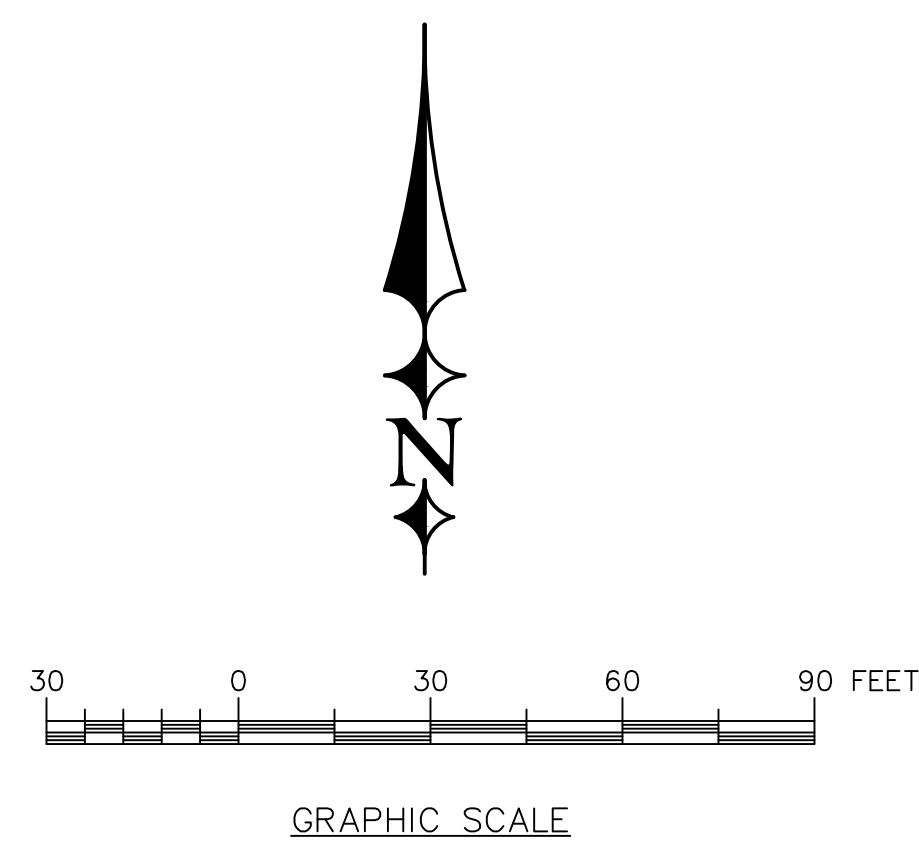
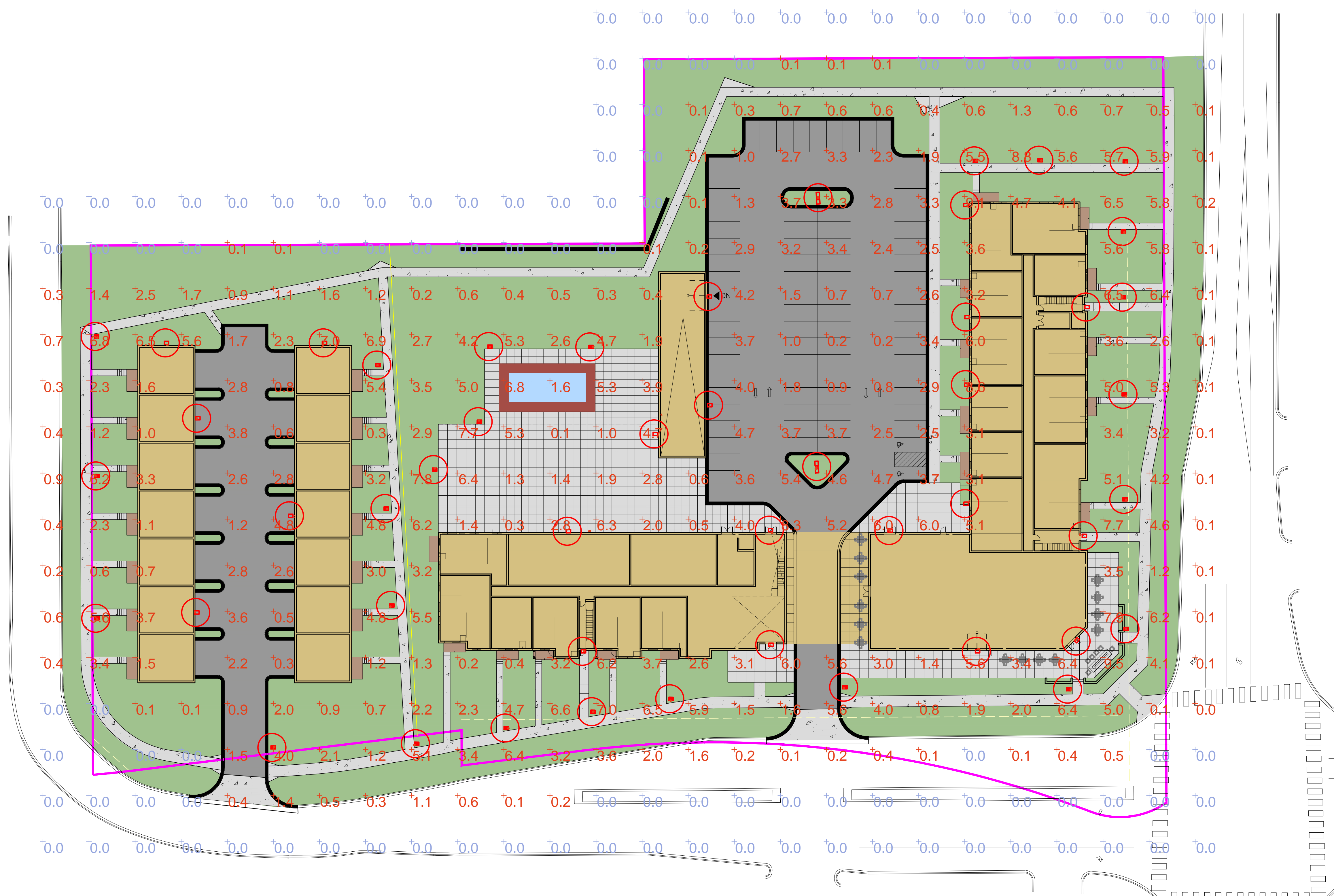
SHEET TITLE

LIGHTING PLAN

SHEET NO.

E1

SHEET 1 OF 1



LEGEND:

PROPOSED BOUNDARY

SINGLE - 20-FT POLE MOUNTED - DSX1 - 60C - 1000 - 40K - T4M - SPA - DDBXD

SINGLE - 20-FT POLE MOUNTED - DSX1 - 60C - 1000 - 40K - T5W - SPA - DDBXD

SINGLE - 10-FT POLE MOUNDTED - MRP - 42C - 1000 - 40K - SR5 - DBLBXD

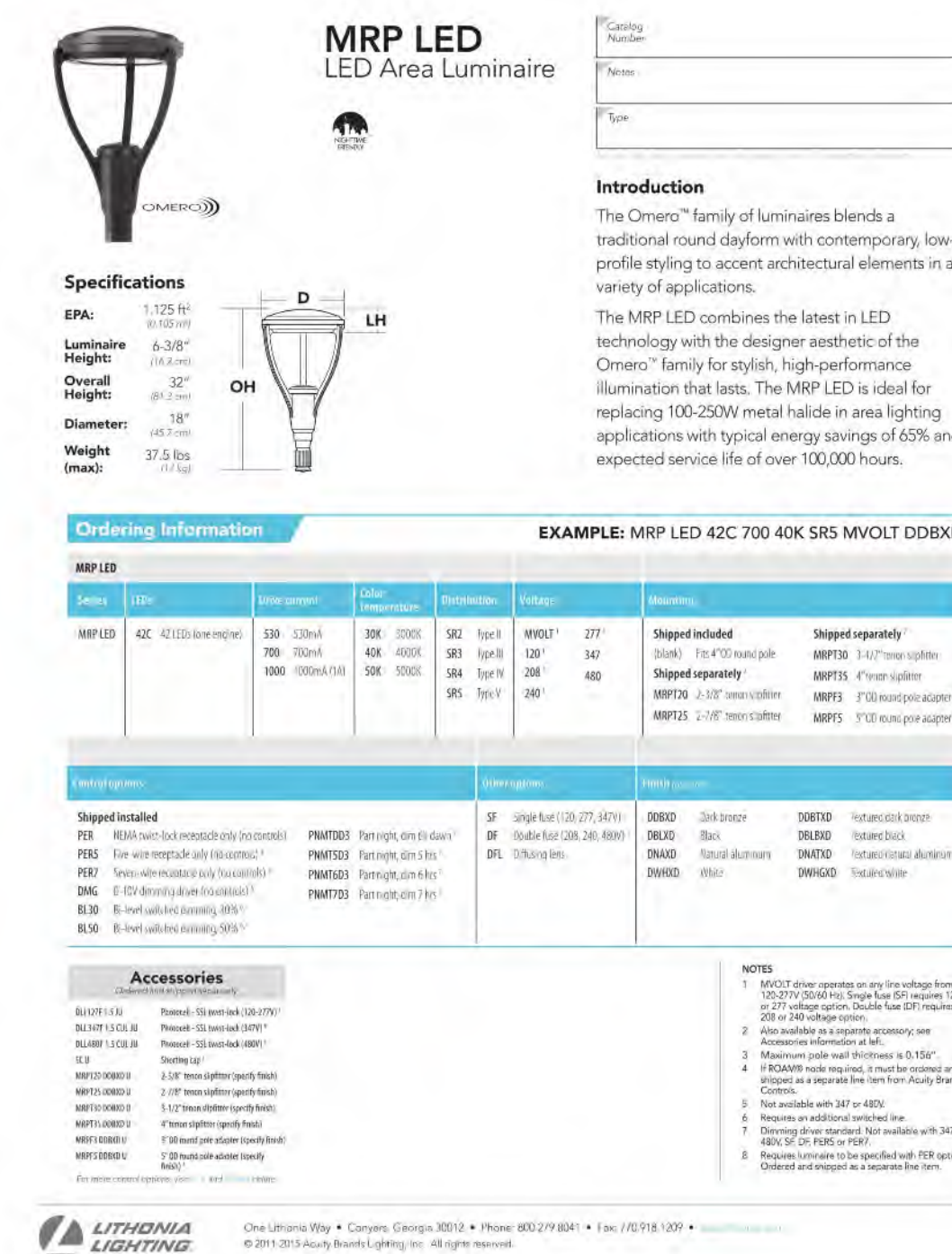
SINGLE - 14-FT BUILDING MOUNTED - CSXW - 30C - 1000 - 40K - T4M - DDBXD
VERIFY TYPE, LOCATION AND HEIGHT WITH ARCHITECT

PROPOSED LIGHTING INTENSITY (FOOT CANDLE)

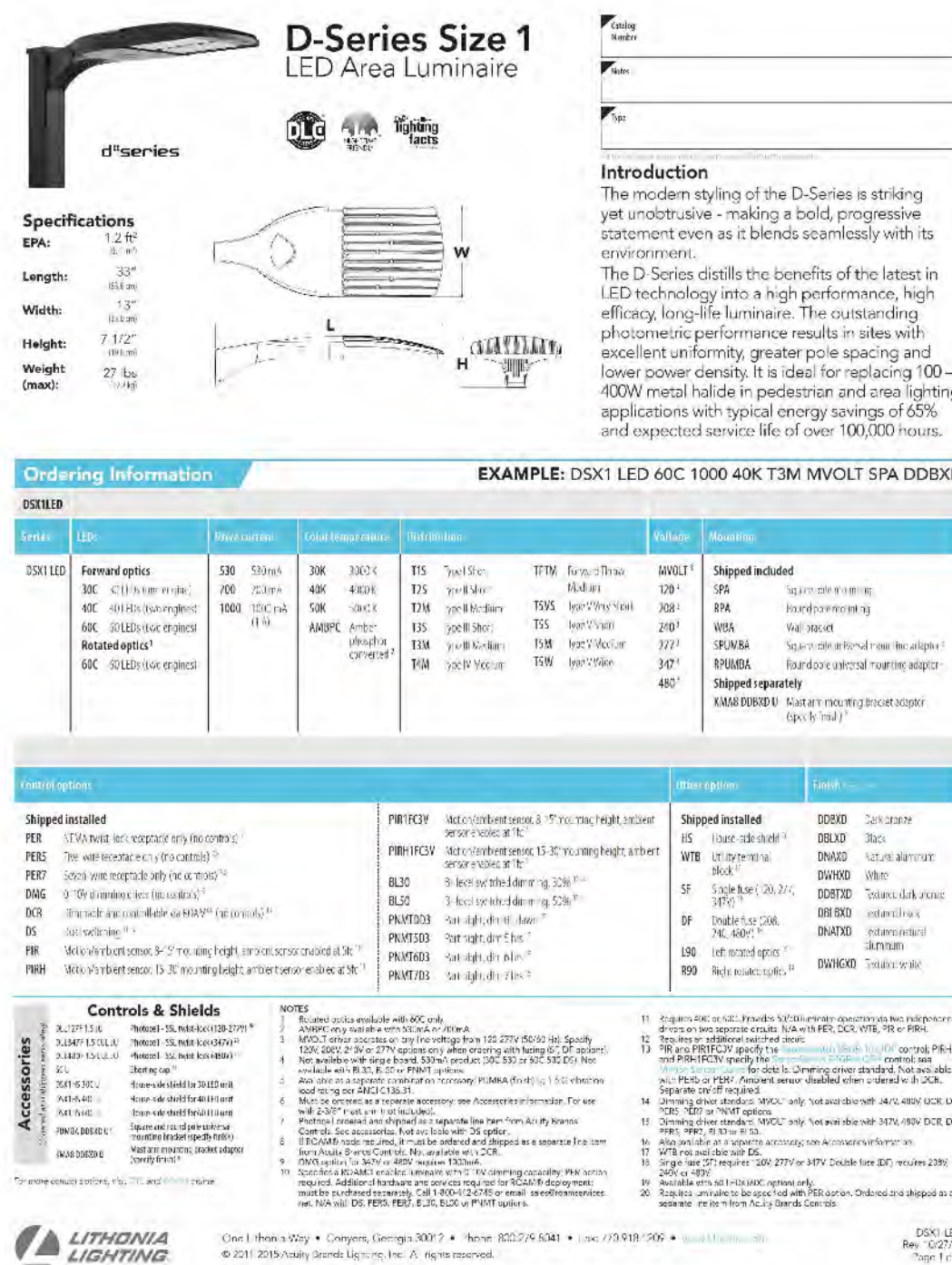
NOTES:

- 1) FINAL DESIGN OF ELECTRICAL SYSTEM BY LICENSED ELECTRICAL CONTRACTOR
- 2) WIRING LOCATION AND SIZING TO BE DETERMINED BY INSTALLING ELECTRICIAN
- 3) INSTALLATION SHALL INCLUDE CONNECTION TO EXISTING POWER PANEL ON INTERIOR OF BUILDING
- 4) INSTALLATION SHALL INCLUDE THE INSTALLATION OF A TIMER AND LIGHT SENSOR

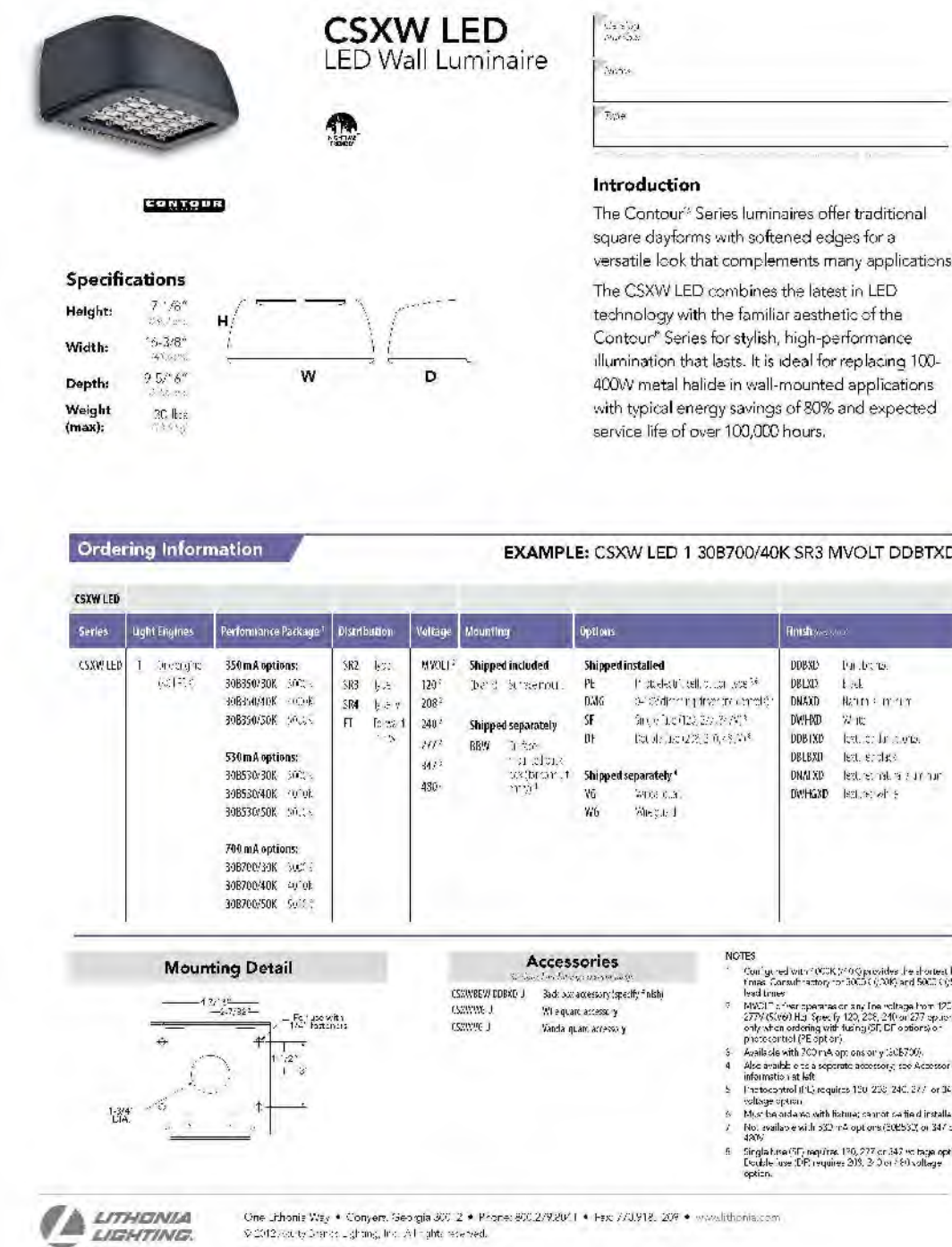
DECORATIVE LIGHTING



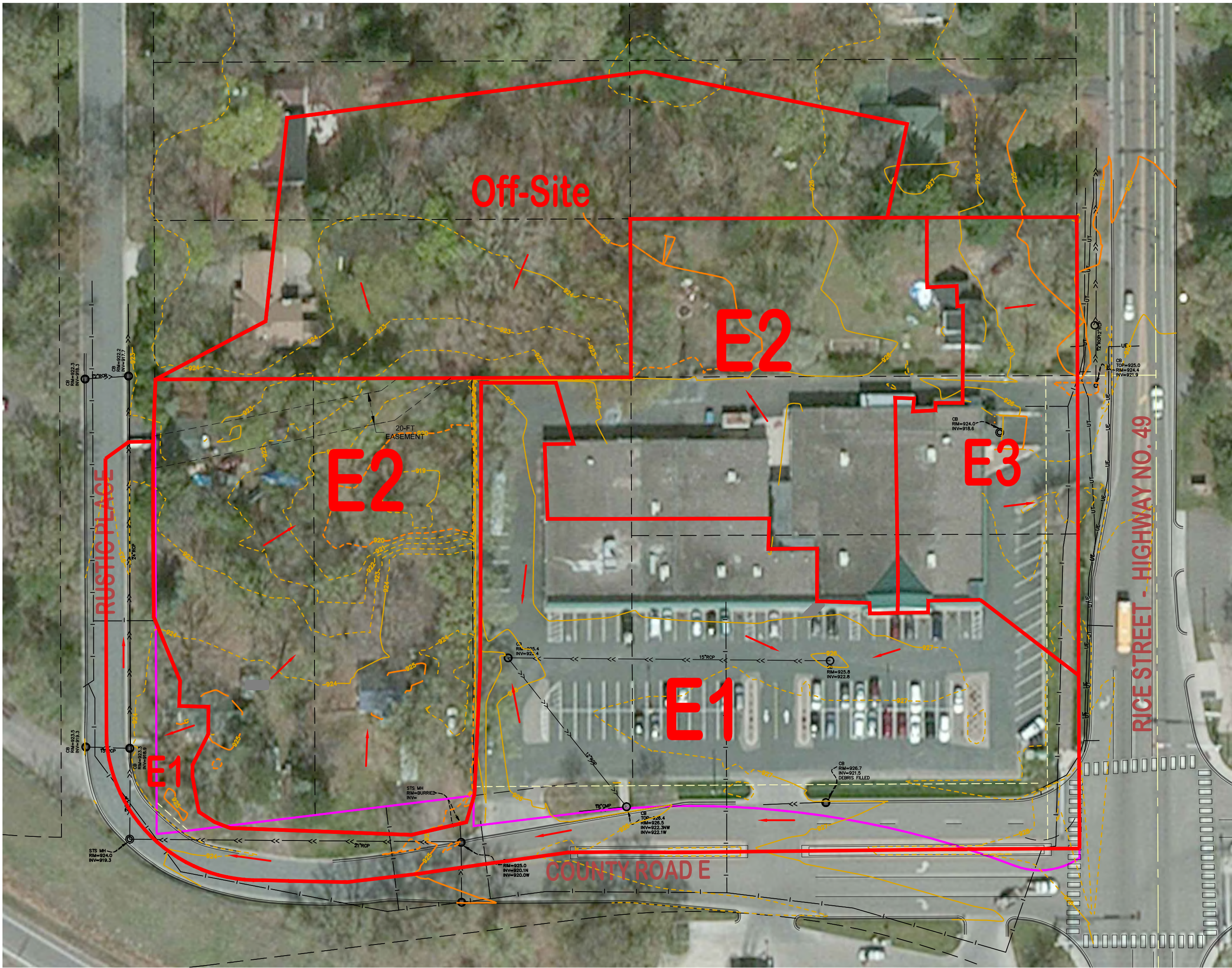
PARKING LOT LIGHTING



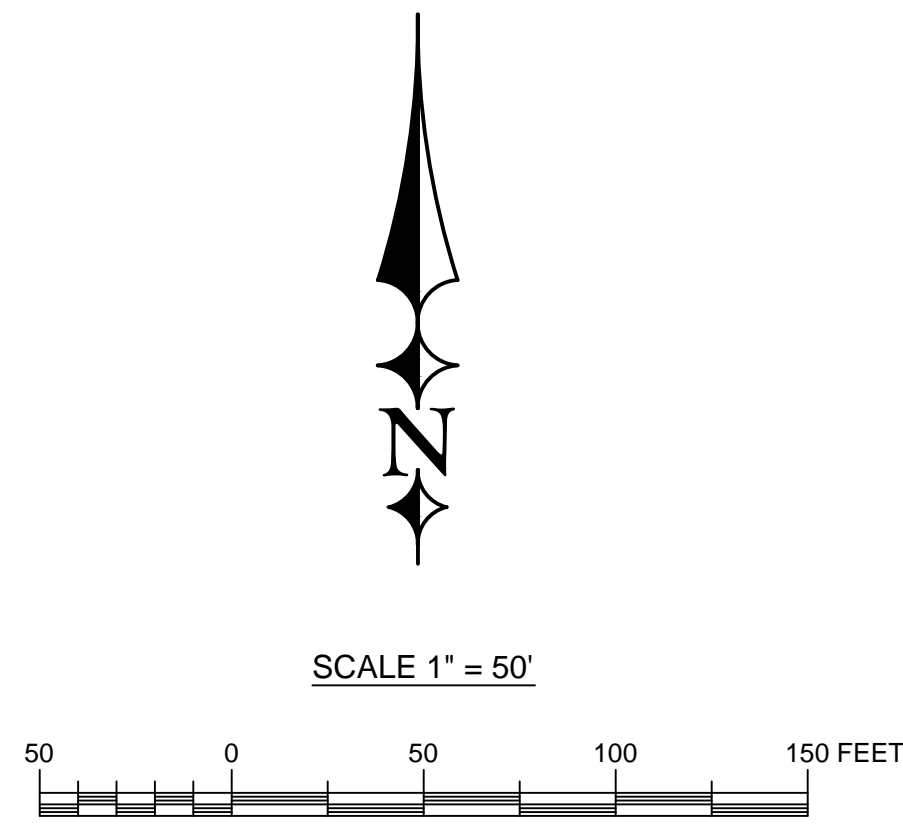
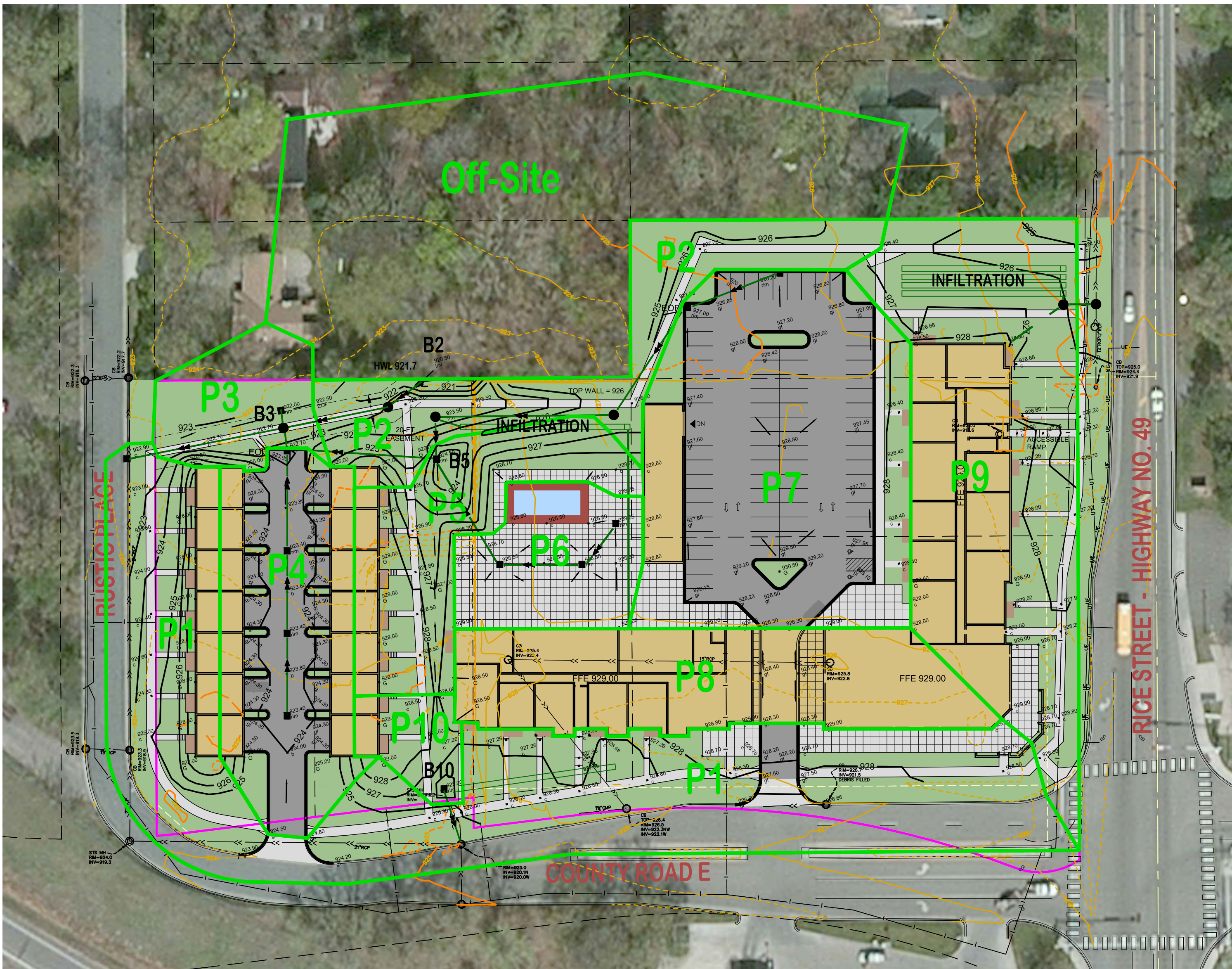
BUILDING MOUNTED LIGHTING



EXISTING DRAINAGE MAP



PROPOSED DRAINAGE MAP



LEGEND:

- 1002 EXISTING 1-FT CONTOUR INTERVAL
- EXISTING DRAINAGE ARROW
- EXISTING DRAINAGE AREA
- PROPOSED DRAINAGE ARROW
- PROPOSED DRAINAGE AREA

EXISTING DRAINAGE AREAS

DRAINAGE AREA ID	DRAINAGE AREA (SQ. FT.)	IMPERVIOUS AREA (SQ. FT.)	DESCRIPTION
E1	88,195	74,557	EXISTING UNCONTROLLED RUNOFF TO CITY STORM SEWER
E2	101,299	36,125	FLOWS TO EXISTING DEPRESSION AND THEN OUTLETS TO STORM
E3	25,296	15,694	EXISTING UNCONTROLLED RUNOFF TO CITY STORM SEWER
Off-Site	51,998	4,487	EXISTING OFF-SITE FLOW

PROPOSED DRAINAGE AREAS

DRAINAGE AREA ID	DRAINAGE AREA (SQ. FT.)	IMPERVIOUS AREA (SQ. FT.)	DESCRIPTION
P1	54,281	19,708	OVERLAND FLOW FROM YARD AND STREET AREAS TO STORM SEWER
P2	15,725	2,439	OVERLAND FLOW TO NEW FES/PONDING AREA
P3	7,343	1,138	FLOW TO SMALL PONDING AREA B3
P4	18,986	16,919	TOWNHOME DRIVEWAY FLOW TO STORM SEWER
P5	13,967	3,447	FLOW TO SMALL PONDING AREA B5
P6	9,555	9,555	PATIO/POOL AREA FLOW TO STORM SEWER
P7	35,916	27,525	PARKING LOT FLOW TO STORM SEWER
P8	19,972	19,972	UNDERGROUND STORAGE SYSTEM FOR RATE AND VOLUME CONTROL
P9	35,166	19,018	UNCONTROLLED FLOW TO CITY STORM SEWER
P10	3,643	1,187	FLOW TO SMALL PONDING AREA B10

RUNOFF SUMMARY COMPARISON

STORM EVENT	RATE DISCHARGE COMPARISON (CFS)	
	EXISTING TOTAL	PROPOSED TOTAL
2-YEAR	7.69	6.16
10-YEAR	13.64	12.79
100-YEAR	25.34	17.71

MPCA - 1.1 Inch Event - Volume Control

NEW IMPERVIOUSNESS = 5,270 Sq. Ft.
DISTURBED EXISTING IMPERVIOUSNESS = 101,500 Sq. Ft.
VOLUME TO INFILTRATE = 9,787 Cu. Ft., INFILTRATION VOLUME = 9,890 CU. FT. > 9,787 CU. FT.
INFILTRATION RATE = 0.45 INCH/HOUR

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TODD A. ERICKSON, PE
40418
LICENSE NO.
EDG PUD
DATE:

OWNER/DEVELOPER
ELEVAGE DEVELOPMENT GROUP, LLC
10901 Baltimore St. NE
Blaine, Minnesota 55449

PROJECT TITLE
SHOREVIEW MIXED USE
157 COUNTY ROAD E, W.
SHOREVIEW, MN 55126

NO.	REVISION	DESCRIPTION	DATE

JOB NO. 15-172

SHEET TITLE

DRAINAGE MAP

SHEET NO.

D1

SHEET 1 OF 1



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VIEW LOOKING NORTH



VIEW LOOKING NORTHWEST



VIEW LOOKING SOUTHWEST



AERIAL VIEW OF SITE

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KEY PLAN

SHOREVIEW MIXED-USE

EXISTING SITE IMAGES

A0.1



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DESIGN PERSPECTIVES
A0.2



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DESIGN PERSPECTIVES

A0.3



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KEY PLAN

SHOREVIEW MIXED-USE

DESIGN PERSPECTIVES

A0.4



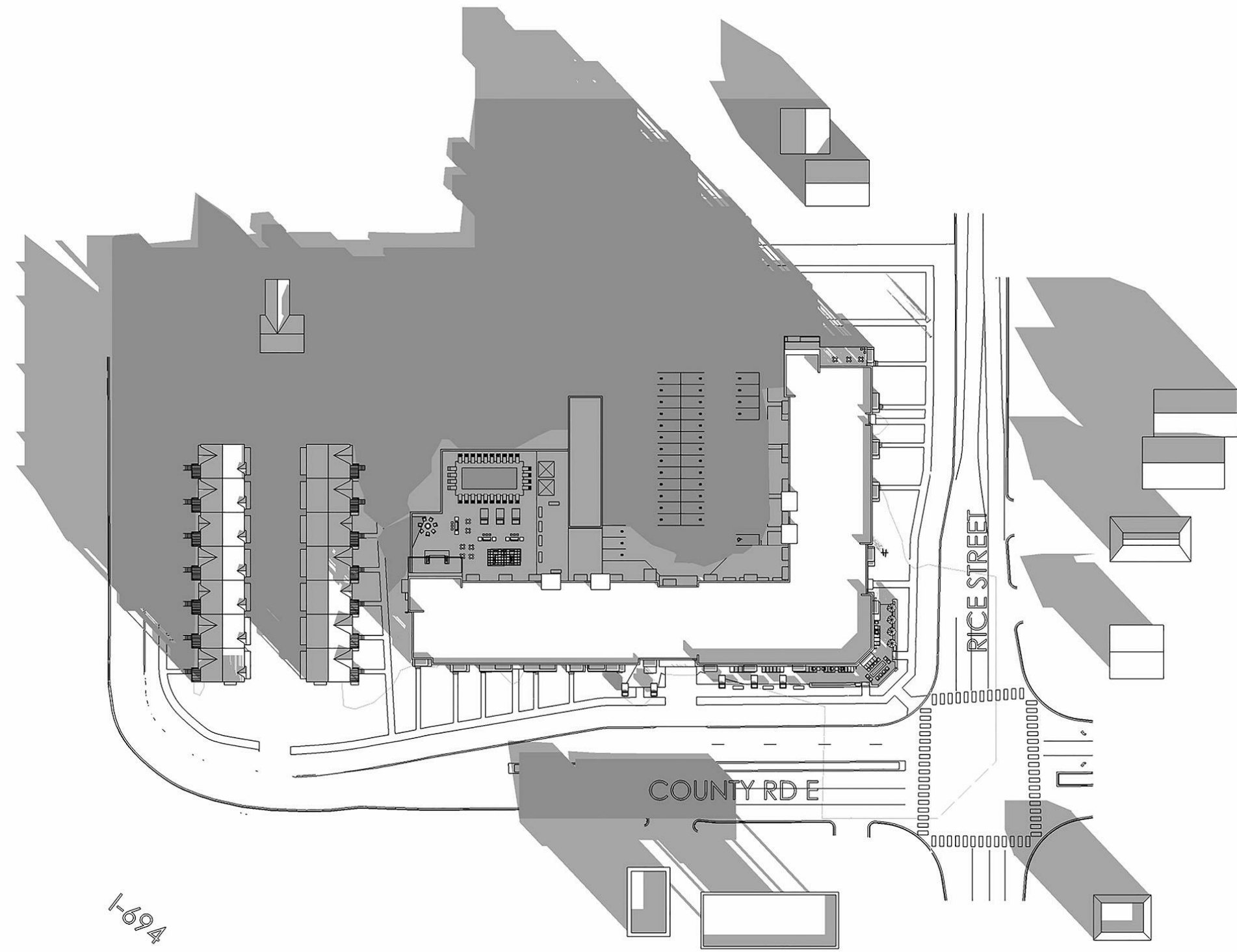
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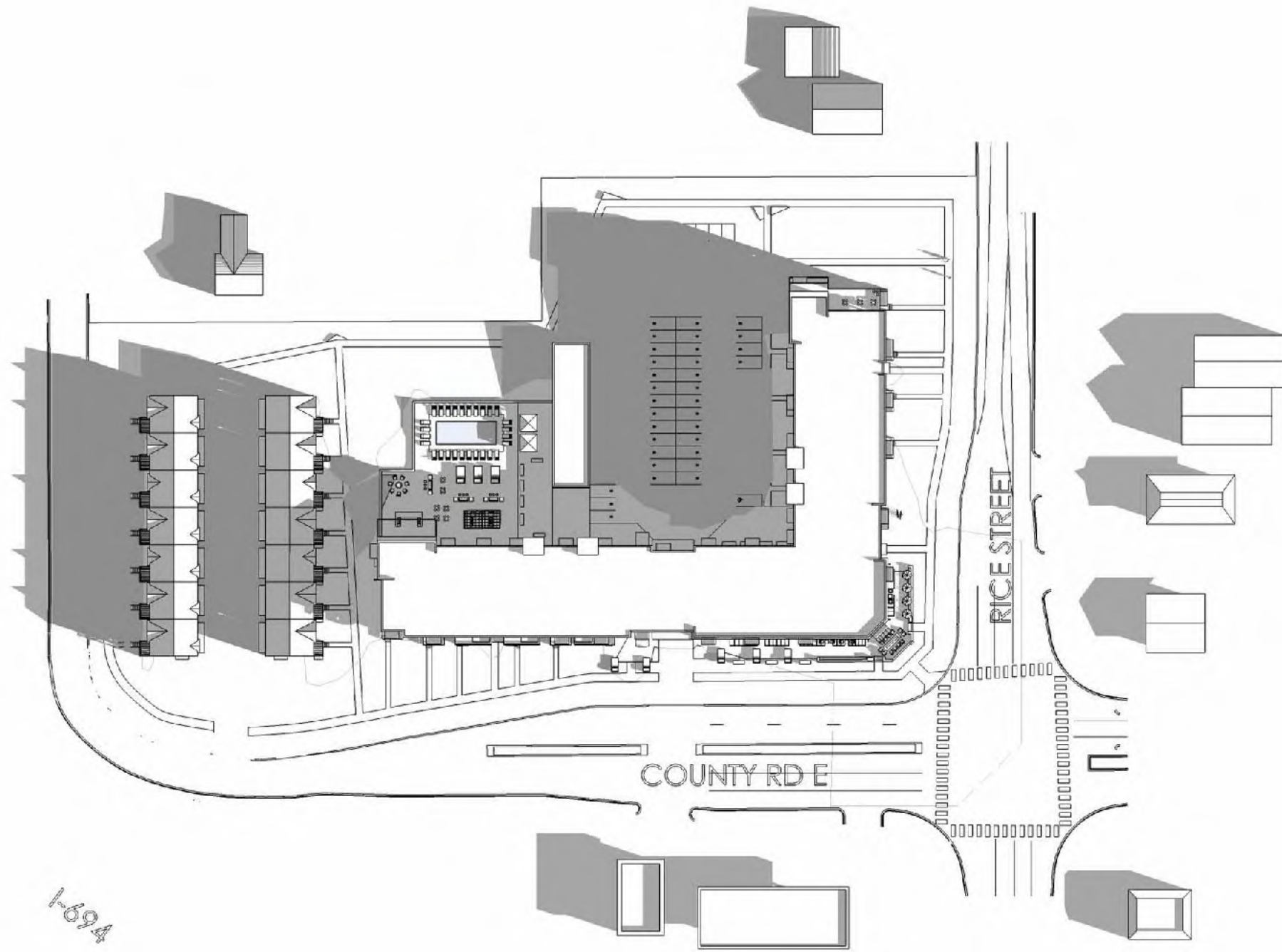
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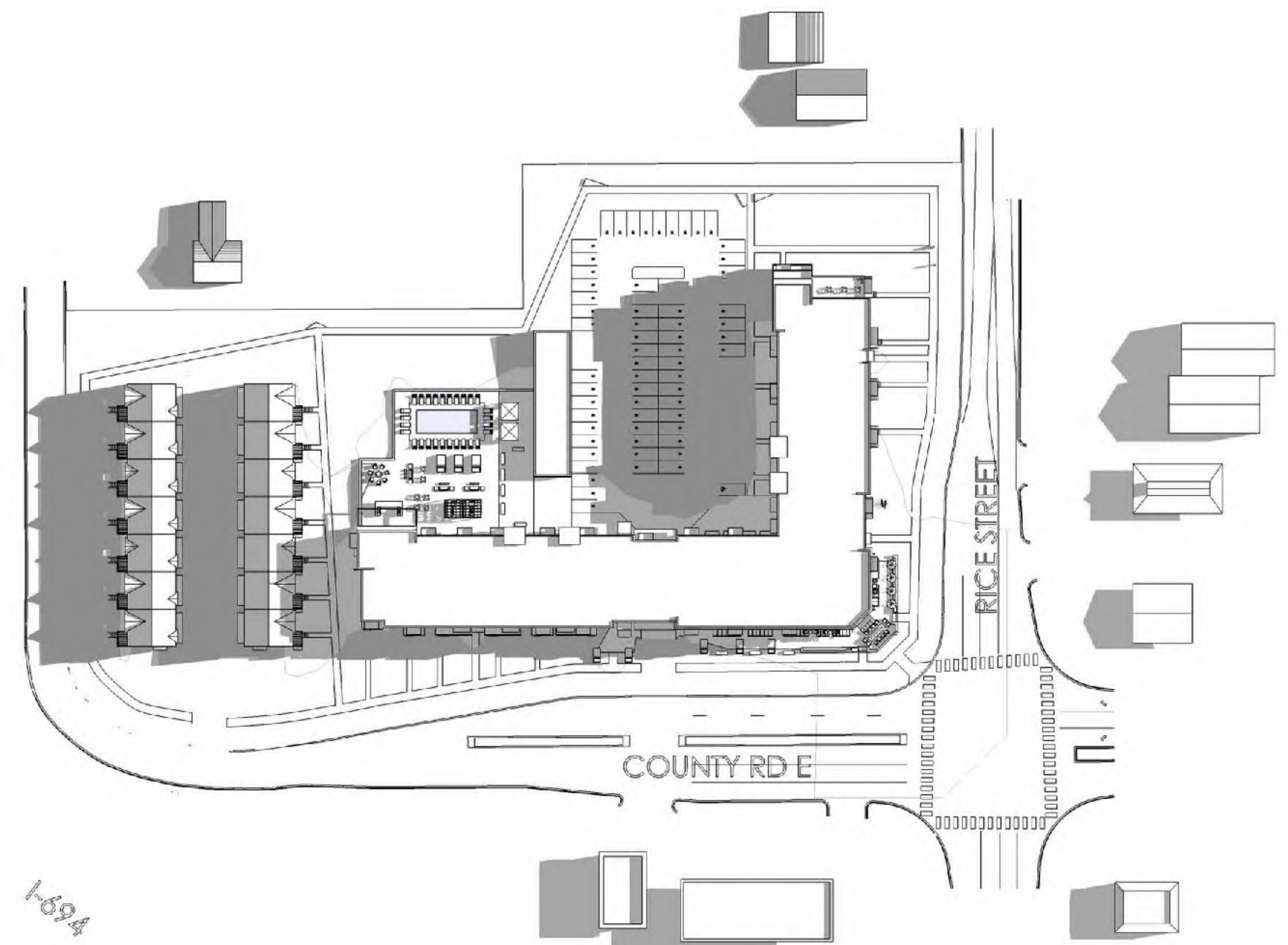
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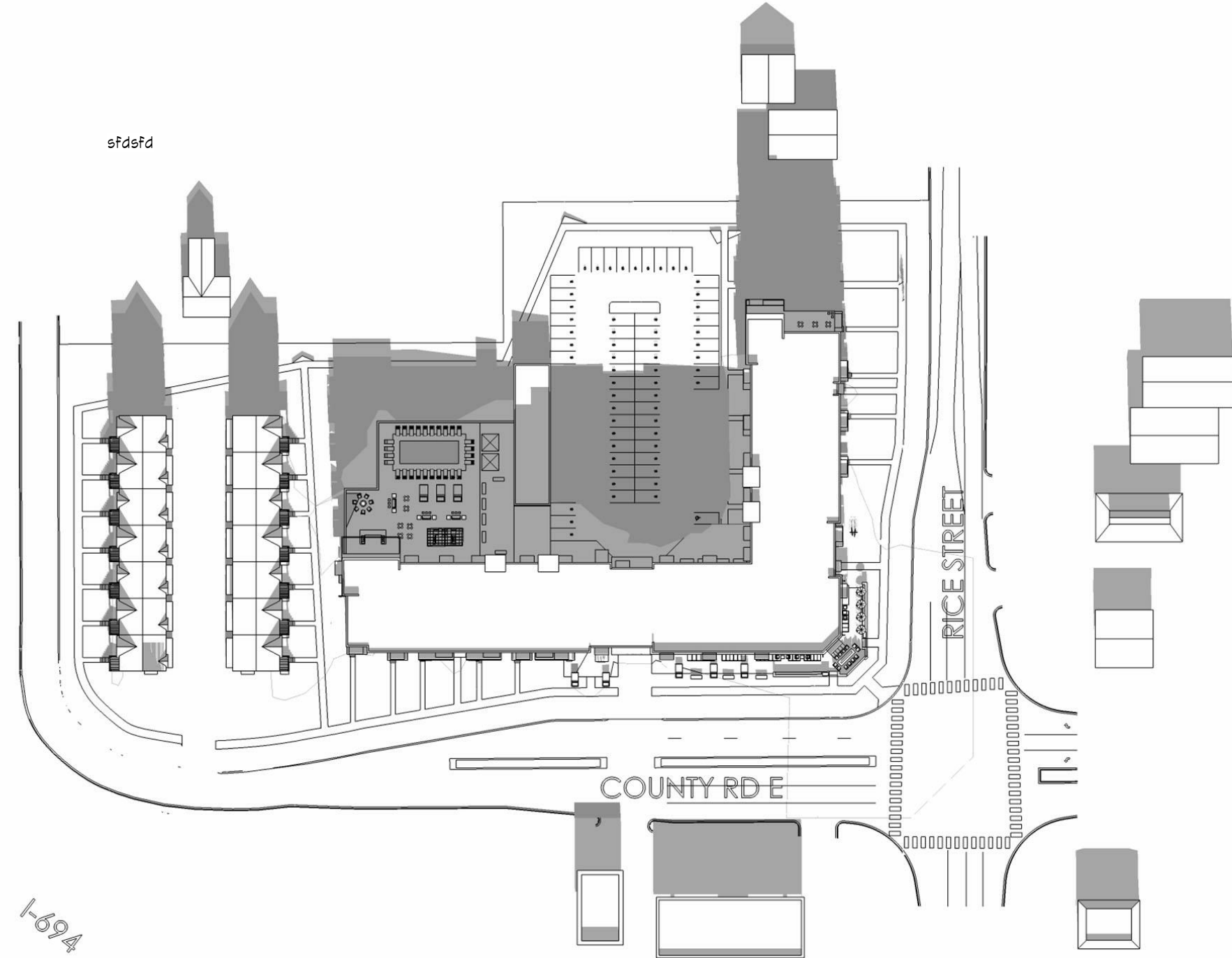
DECEMBER MORNING 9AM



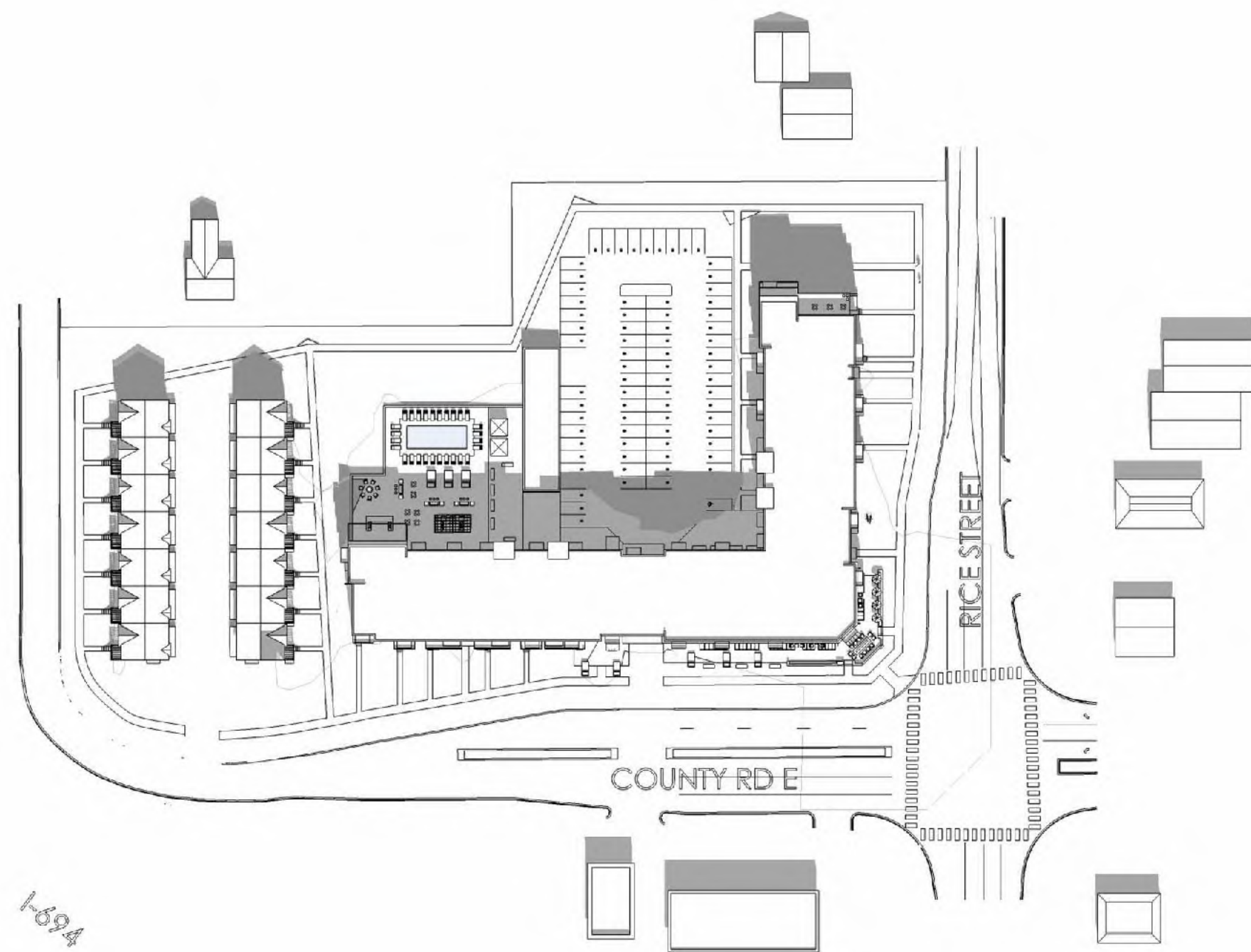
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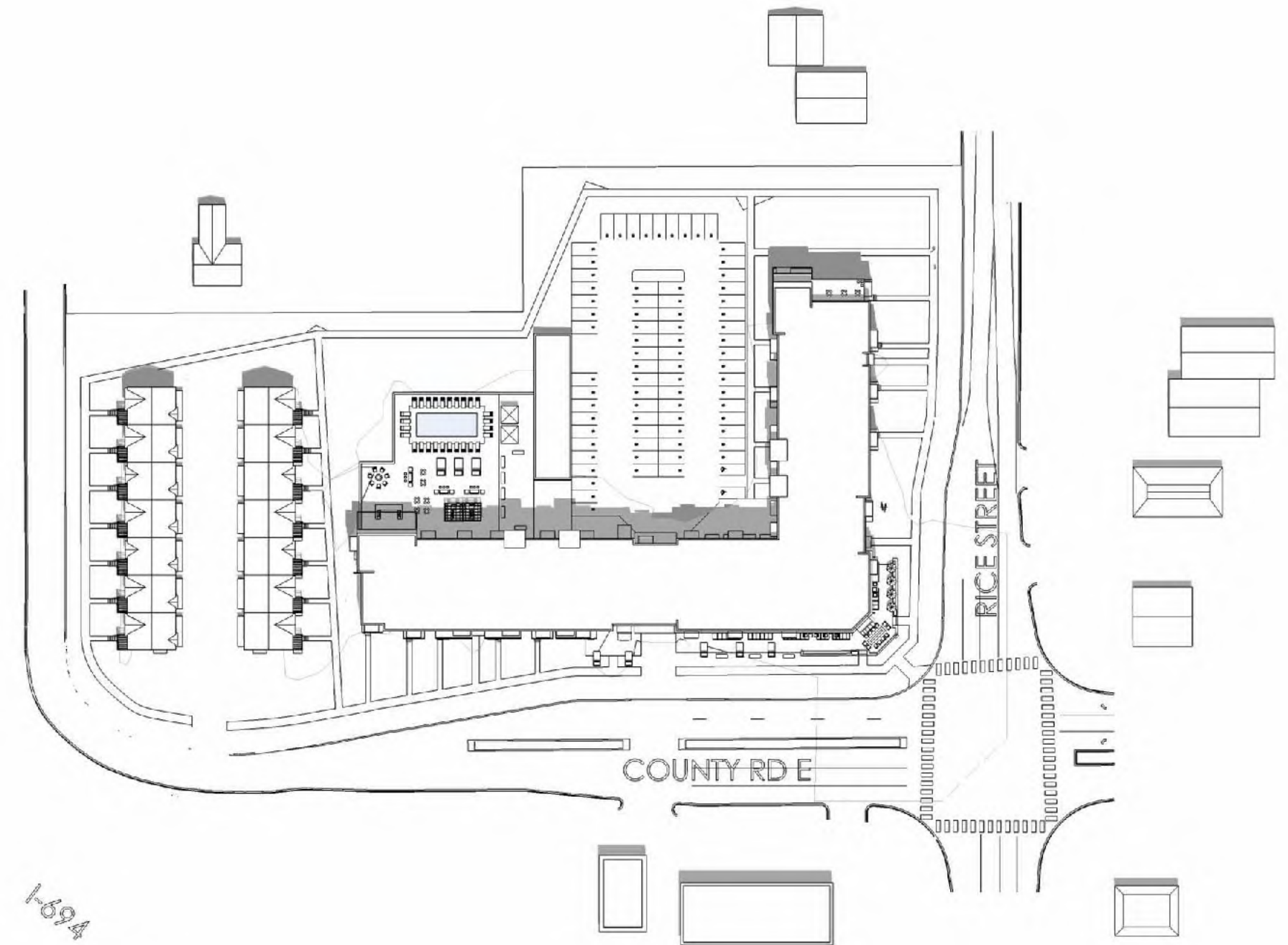
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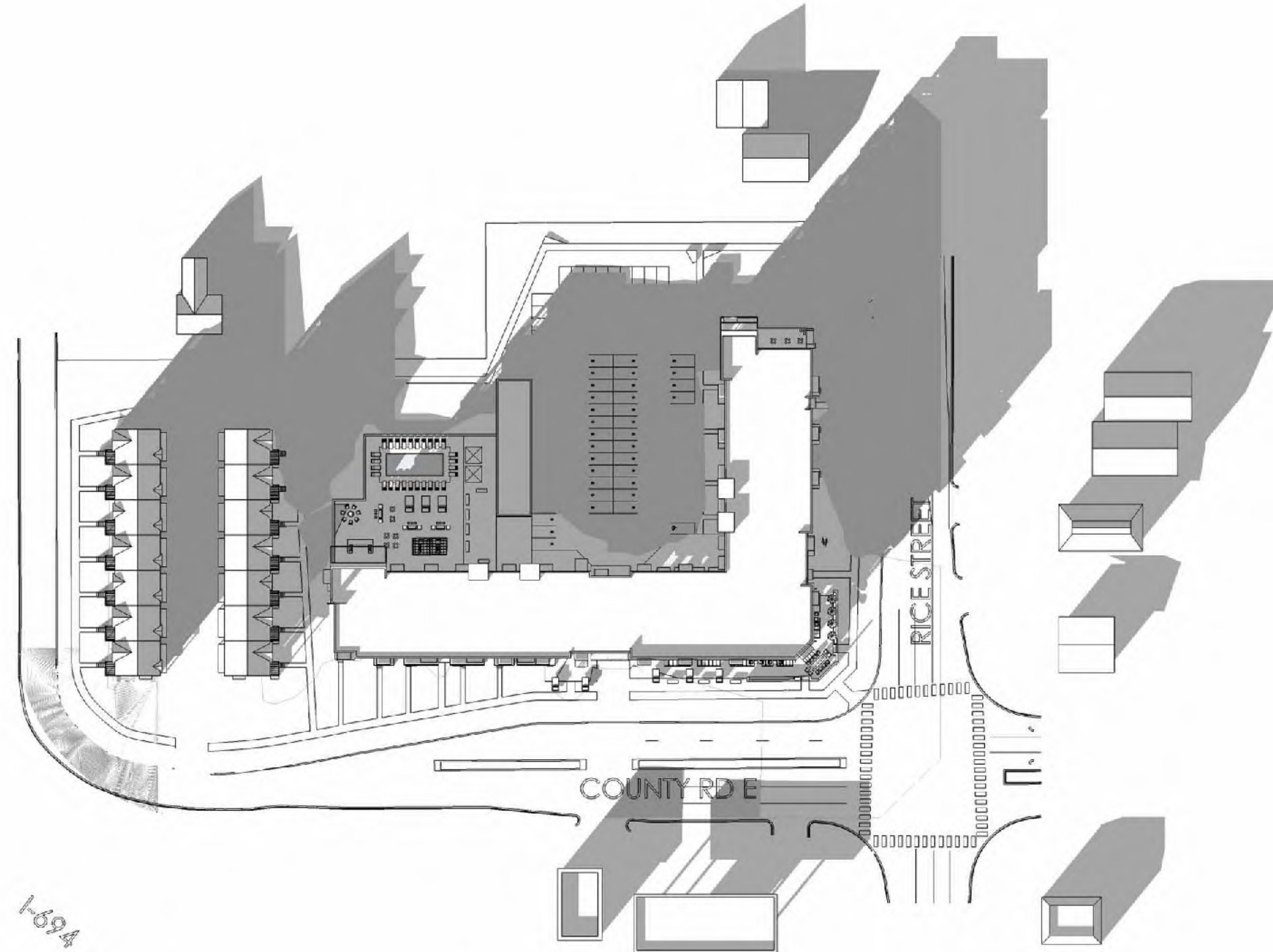
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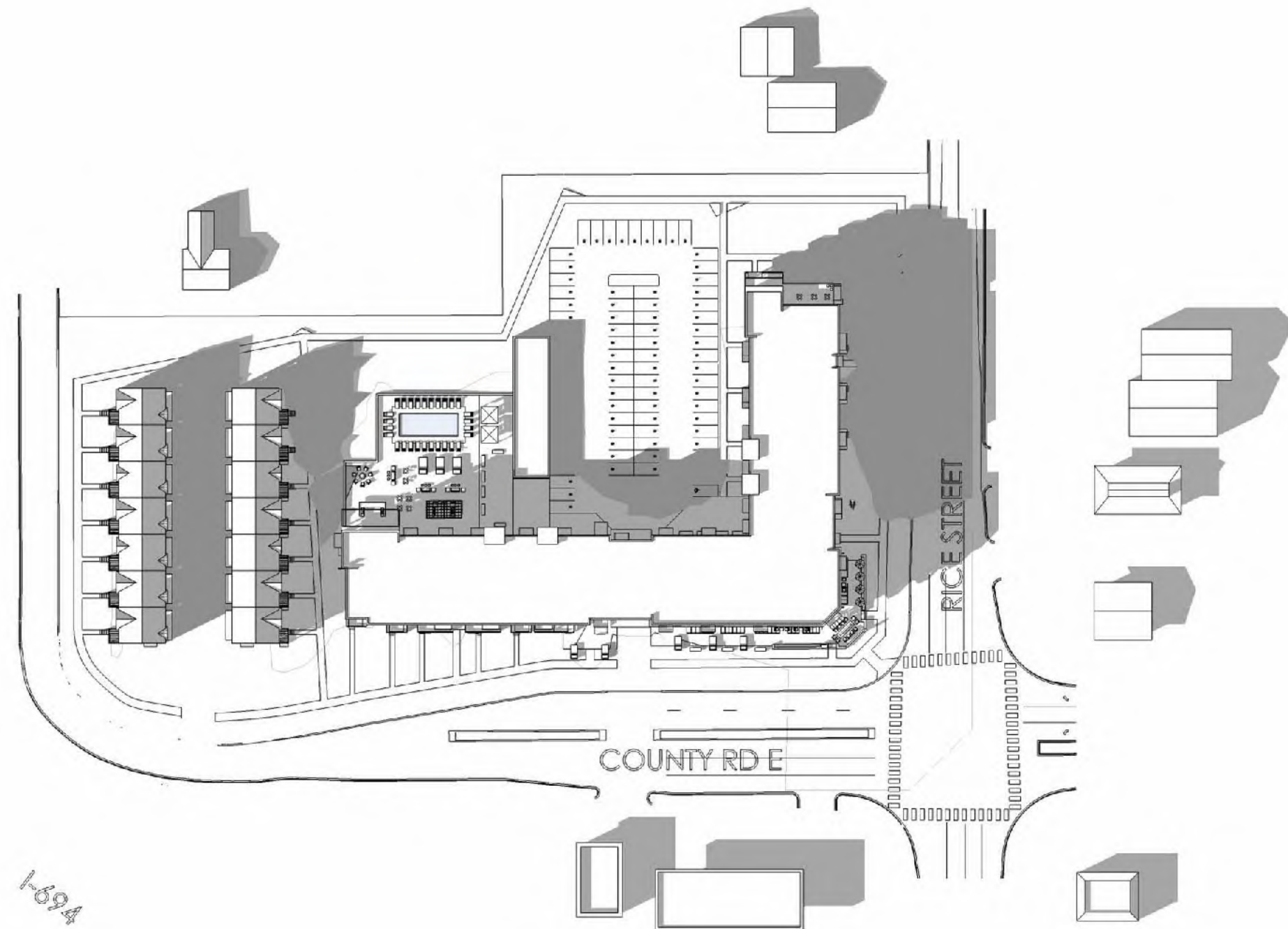
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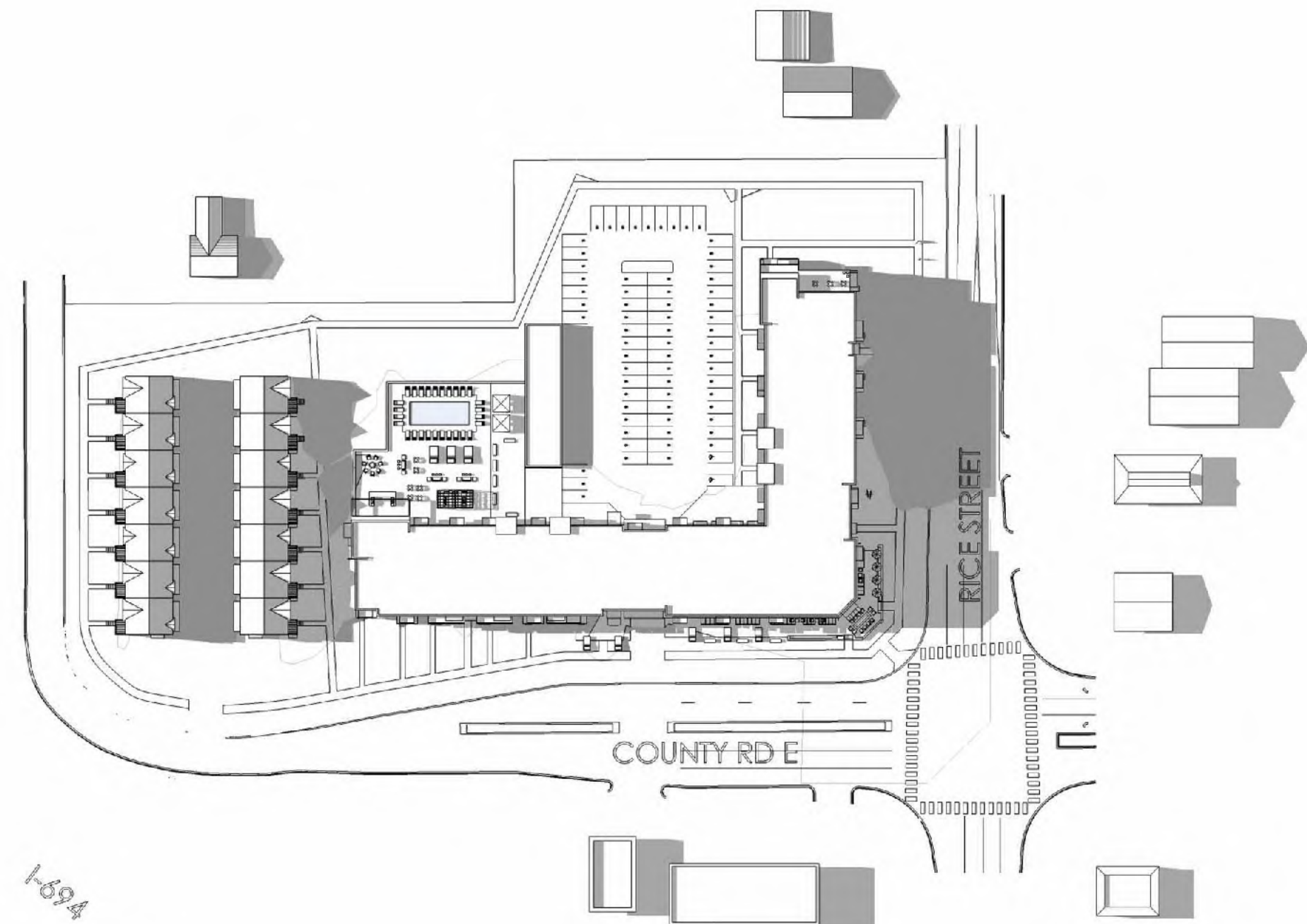
JUNE NOON 12PM



DECEMBER EVENING 3PM



MARCH/SEPTEMBER EVENING 4PM



JUNE EVENING 5PM

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KEY PLAN

SHOREVIEW MIXED-USE

SHADOW STUDY

A0.5



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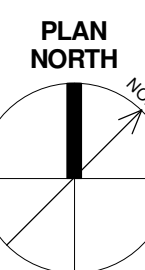
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SITE PLAN - LEVEL 1 PLAN

A1.1





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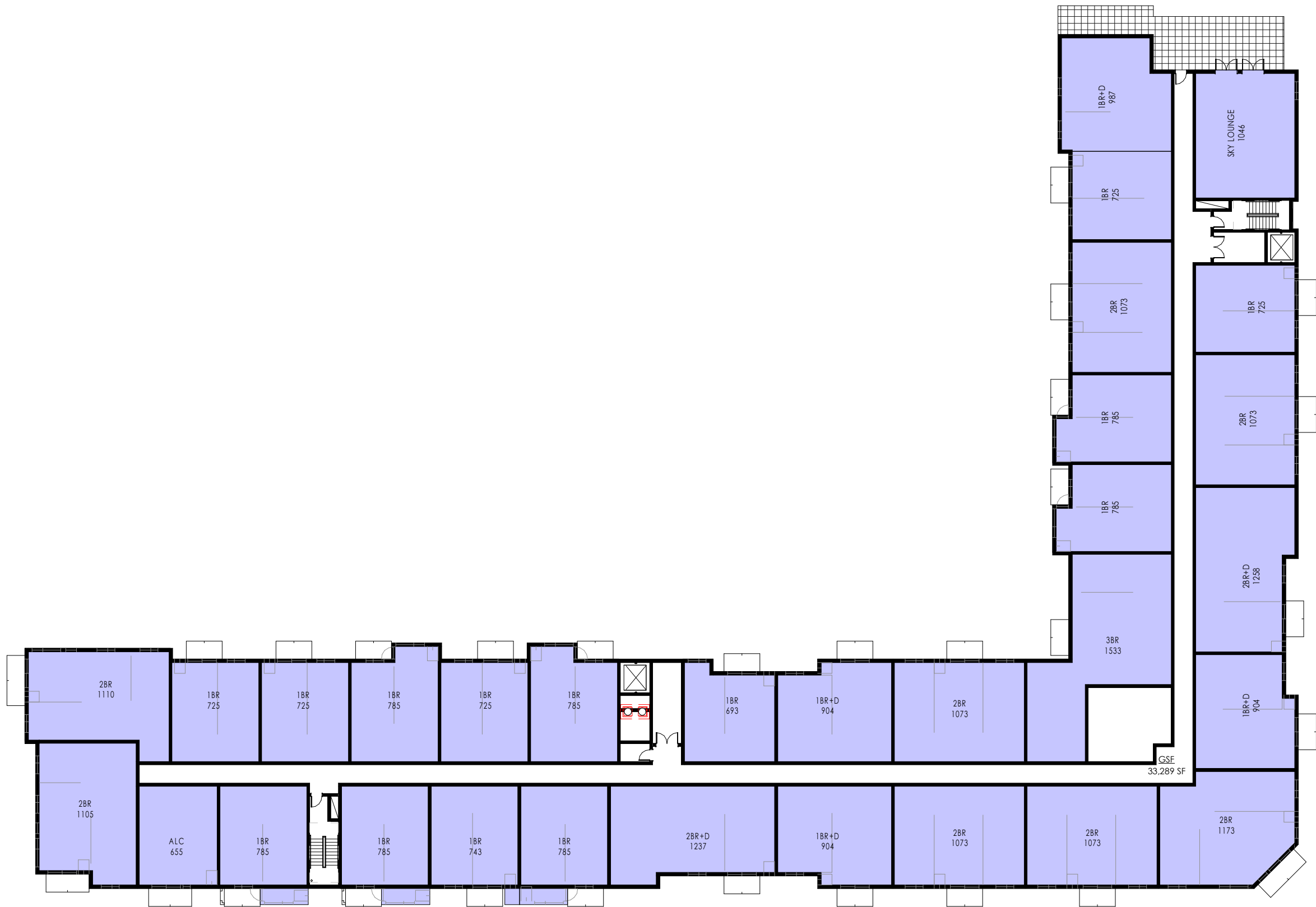
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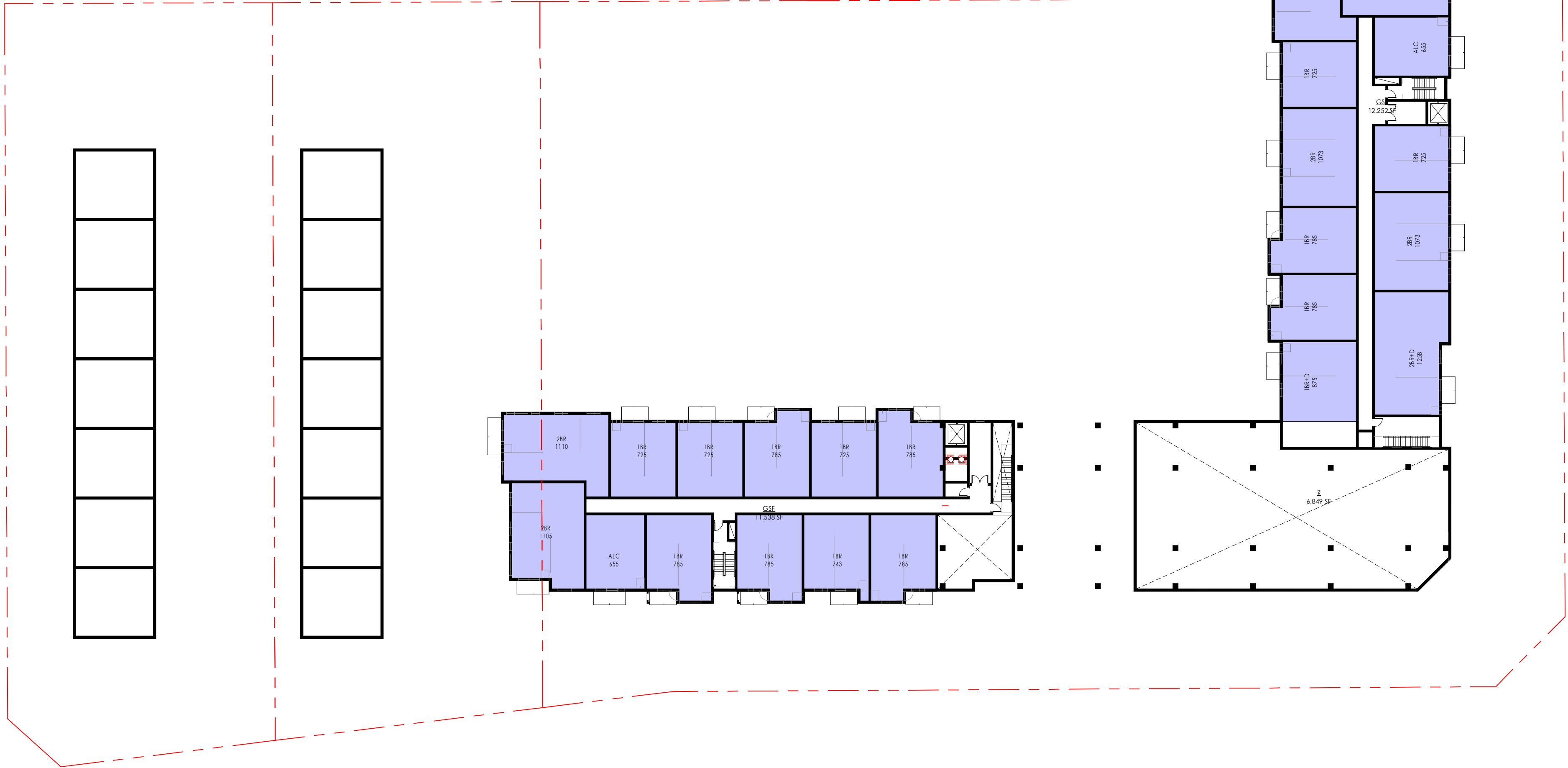
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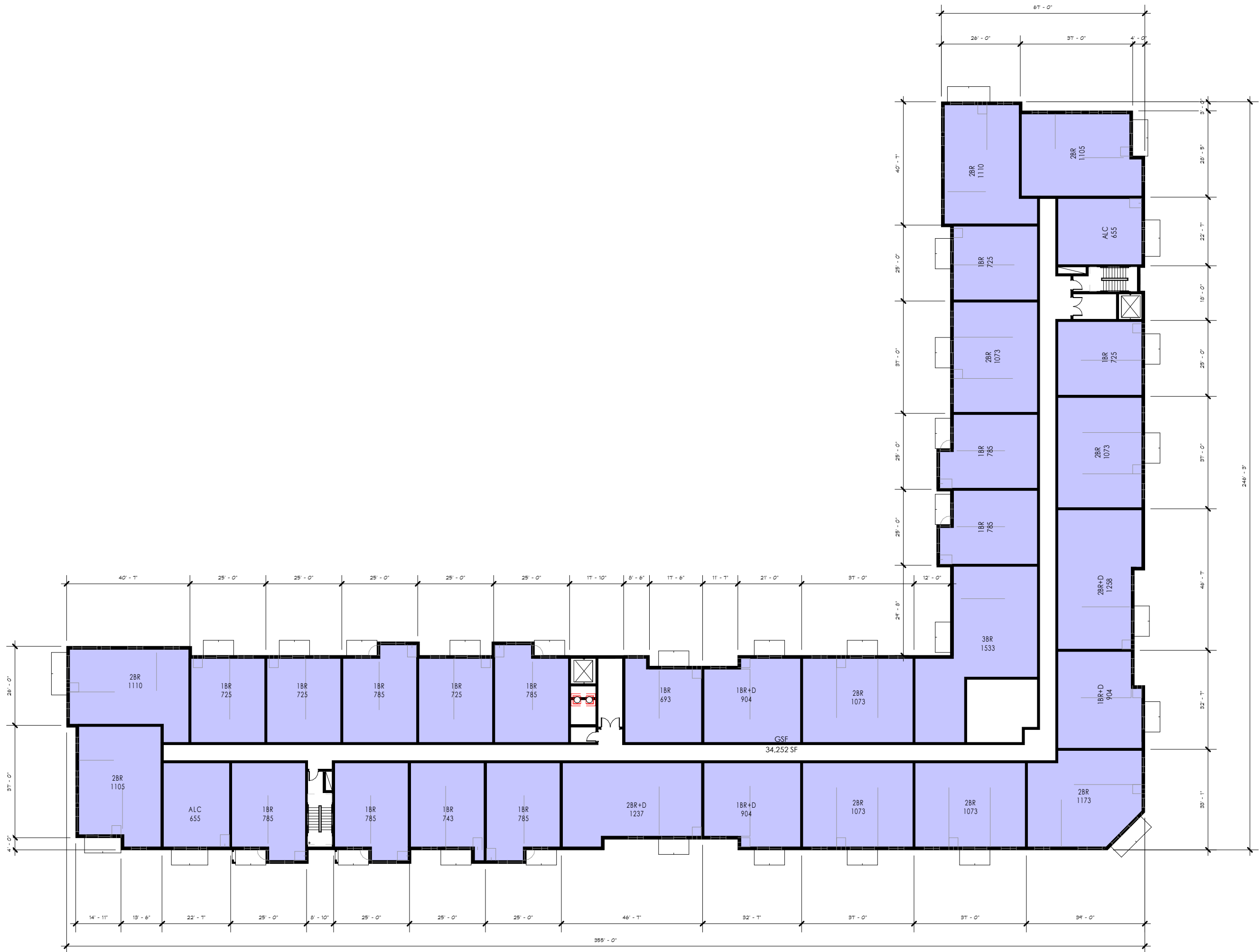
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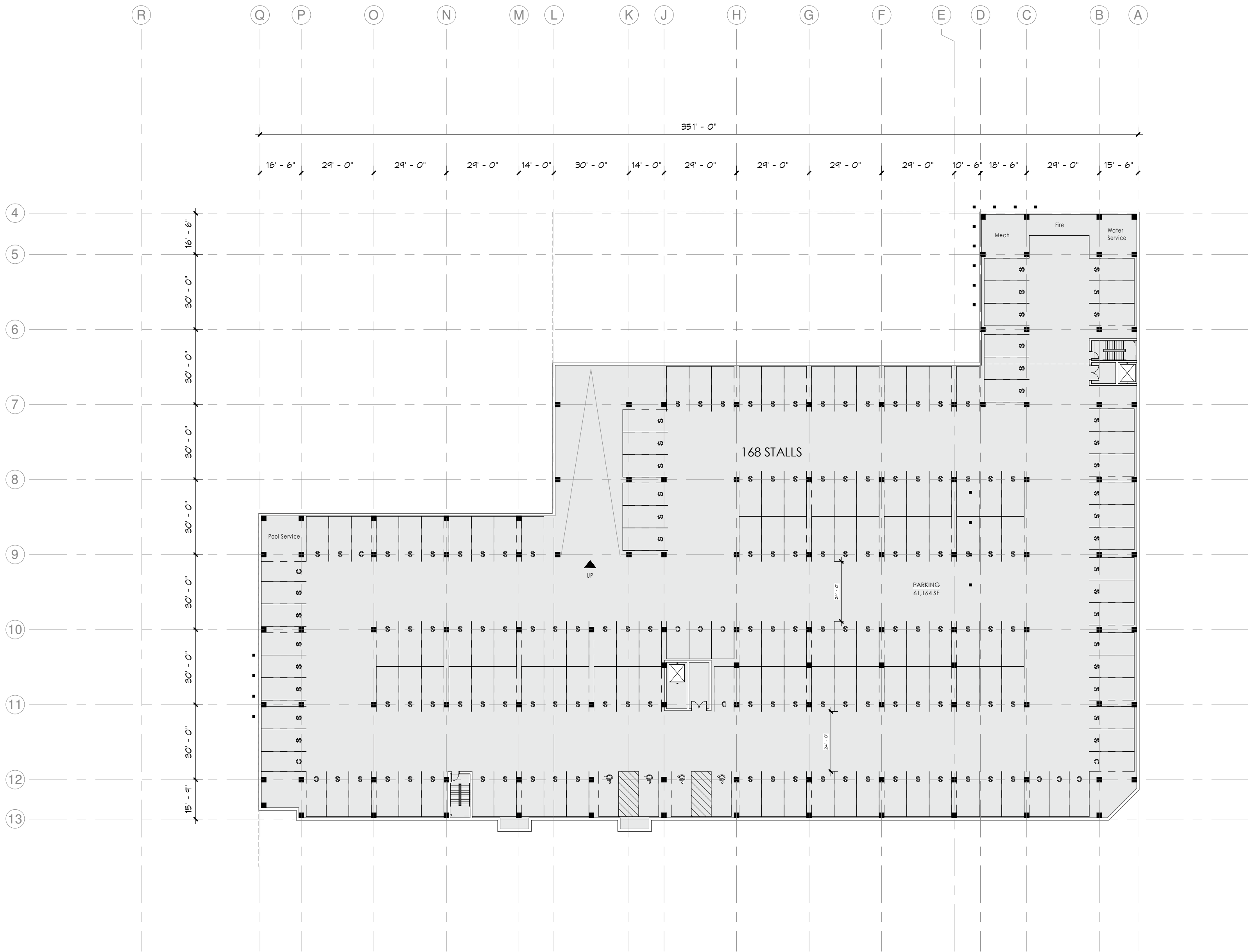
4 LEVEL 5
A1.2 1" = 30'-0"



2 LEVEL 2 PLAN
A1.2 1" = 30'-0"



3 LEVEL 3-4 PLAN
A1.2 1" = 30'-0"



1 LEVEL P1 PLAN
A1.2 1" = 30'-0"

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SHOREVIEW MIXED-USE

LEVELS P1, 2, 3-4, 5

A1.2



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f. 6 1 2 . 3 3 9 . 5 3 8 2
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EXTERIOR MATERIAL KEYNOTES

- 1A BRICK #1
- 1B BRICK #2
- 2A ARCHITECTURAL CAST STONE
- 5A METAL PANEL #1
- 5B METAL PANEL #2
- 5C METAL PANEL #3
- 6A WOOD OR FIBER CEMENT PANEL COLOR #1
- 7A STUCCO #1
- 8A ANODIZED ALUMINUM STOREFRONT (DARK BRONZE)
- 8B FIBERGLASS WINDOWS/DOORS DARK BRONZE
- 8C FIBERGLASS WINDOWS/DOORS SILVER
- 13 PREMANUFACTURED HANGING BALCONY DARK BRONZE
- 14 MECHANICAL LOUVER
- 15 SIGNAGE (TBD)

GENERAL NOTES - EXTERIOR ELEVATIONS

- TH2 TO HORIZONTAL - GRADE VARIES, REFER TO
CIVIL FOR CORRECT GRADE

NOT FOR
CONSTRUCTION



2 SOUTH ELEVATION
A3.1 3/32" = 1'-0"



1 EAST ELEVATION
A3.1 3/32" = 1'-0"

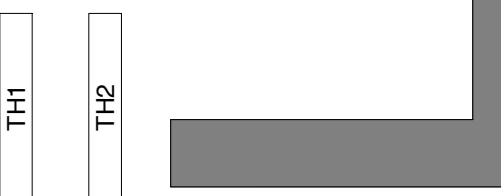
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EXTERIOR ELEVATIONS

A3.1



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License # Date

EXTERIOR MATERIAL KEYNOTES	
1A	BRICK #1
1B	BRICK #2
2A	ARCHITECTURAL CAST STONE
5A	METAL PANEL #1
5B	METAL PANEL #2
5C	METAL PANEL #3
6A	WOOD OR FIBER CEMENT PANEL COLOR #1
7A	STUCCO #1
8A	ANODIZED ALUMINUM STOREFRONT (DARK BRONZE)
8B	FIBERGLASS WINDOWS/DOORS DARK BRONZE
8C	FIBERGLASS WINDOWS/DOORS SILVER
13	PREMANUFACTURED HANGING BALCONY DARK BRONZE
14	MECHANICAL LOUVER
15	SIGNAGE (TBD)

GENERAL NOTES - EXTERIOR ELEVATIONS	
- TH2 TO MIRROR TH1 - GRADE VARIES. REFER TO CIVIL FOR CORRECT GRADE	



4 NORTH ELEVATION A
A3.2 3/32" = 1'-0"

3 NORTH ELEVATION B
A3.2 3/32" = 1'-0"



2 WEST ELEVATION A
A3.2 3/32" = 1'-0"

1 WEST ELEVATION B
A3.2 3/32" = 1'-0"

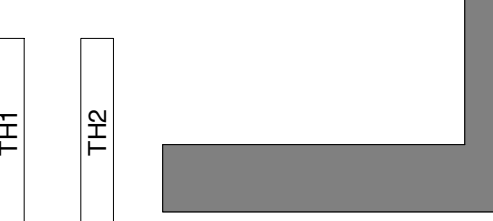
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SHOREVIEW MIXED-USE

EXTERIOR ELEVATIONS

A3.2



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f. 6 1 2 . 3 3 9 . 5 3 8 2
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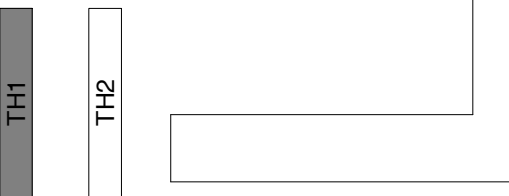
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KEY PLAN



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KEY PLAN

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SHOREVIEW MIXED-USE

EXTERIOR ELEVATIONS
(TOWNHOMES)

A3.3

EXTERIOR MATERIAL KEYNOTES

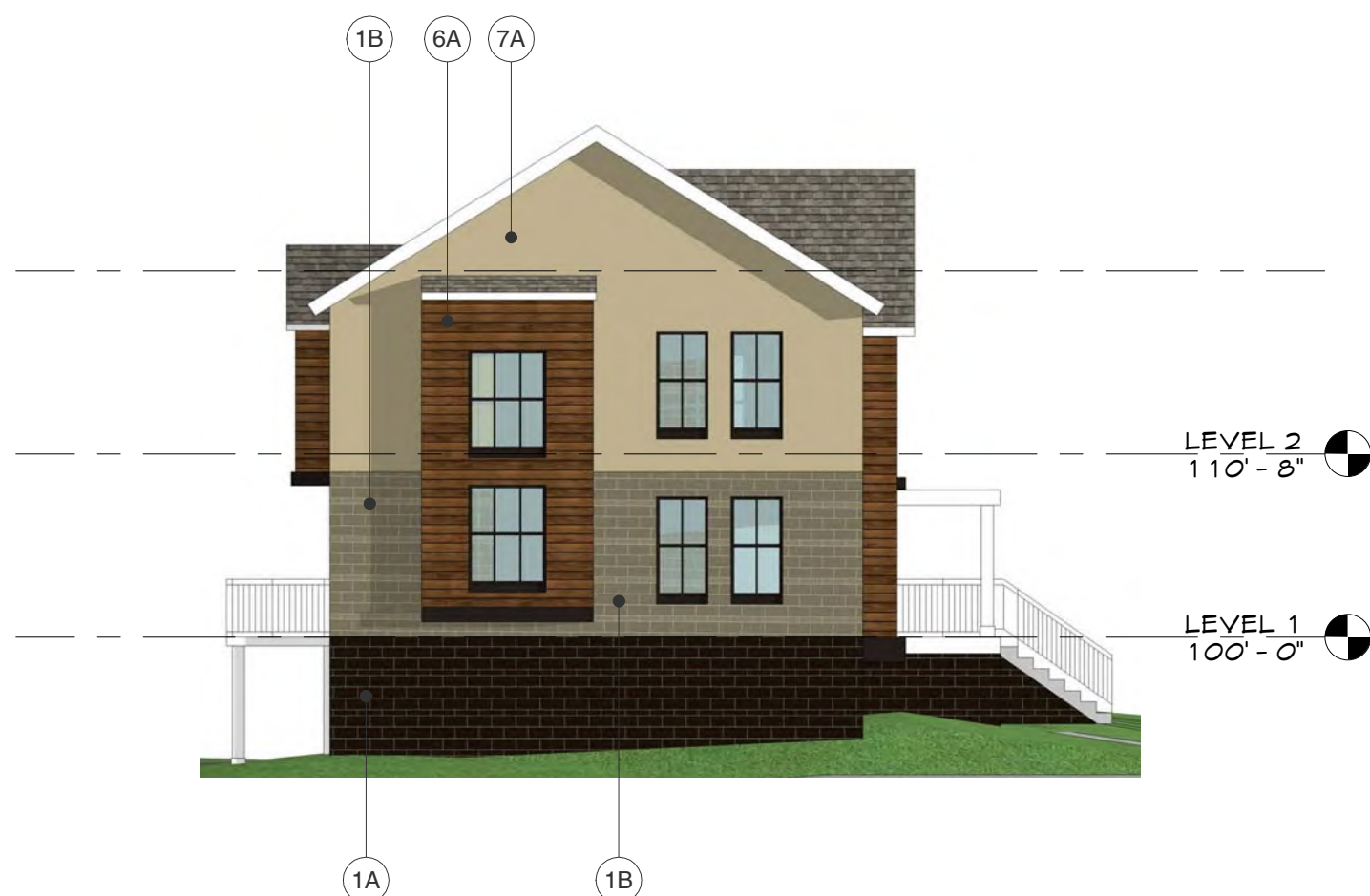
- 1A BRICK #1
- 1B BRICK #2
- 2A ARCHITECTURAL CAST STONE
- 5A METAL PANEL #1
- 5B METAL PANEL #2
- 5C METAL PANEL #3
- 6A WOOD OR FIBER CEMENT PANEL COLOR #1
- 7A STUCCO #1
- 8A ANODIZED ALUMINUM STOREFRONT (DARK BRONZE)
- 8B FIBERGLASS WINDOWS/DOORS DARK BRONZE
- 8C FIBERGLASS WINDOWS/DOORS SILVER
- 13 PREMANUFACTURED HANGING BALCONY DARK BRONZE
- 14 MECHANICAL LOUVER
- 15 SIGNAGE (TBD)

GENERAL NOTES - EXTERIOR ELEVATIONS

- TH2 TO MIRROR TH1 - GRADE VARIES, REFER TO
CIVIL FOR CORRECT GRADE



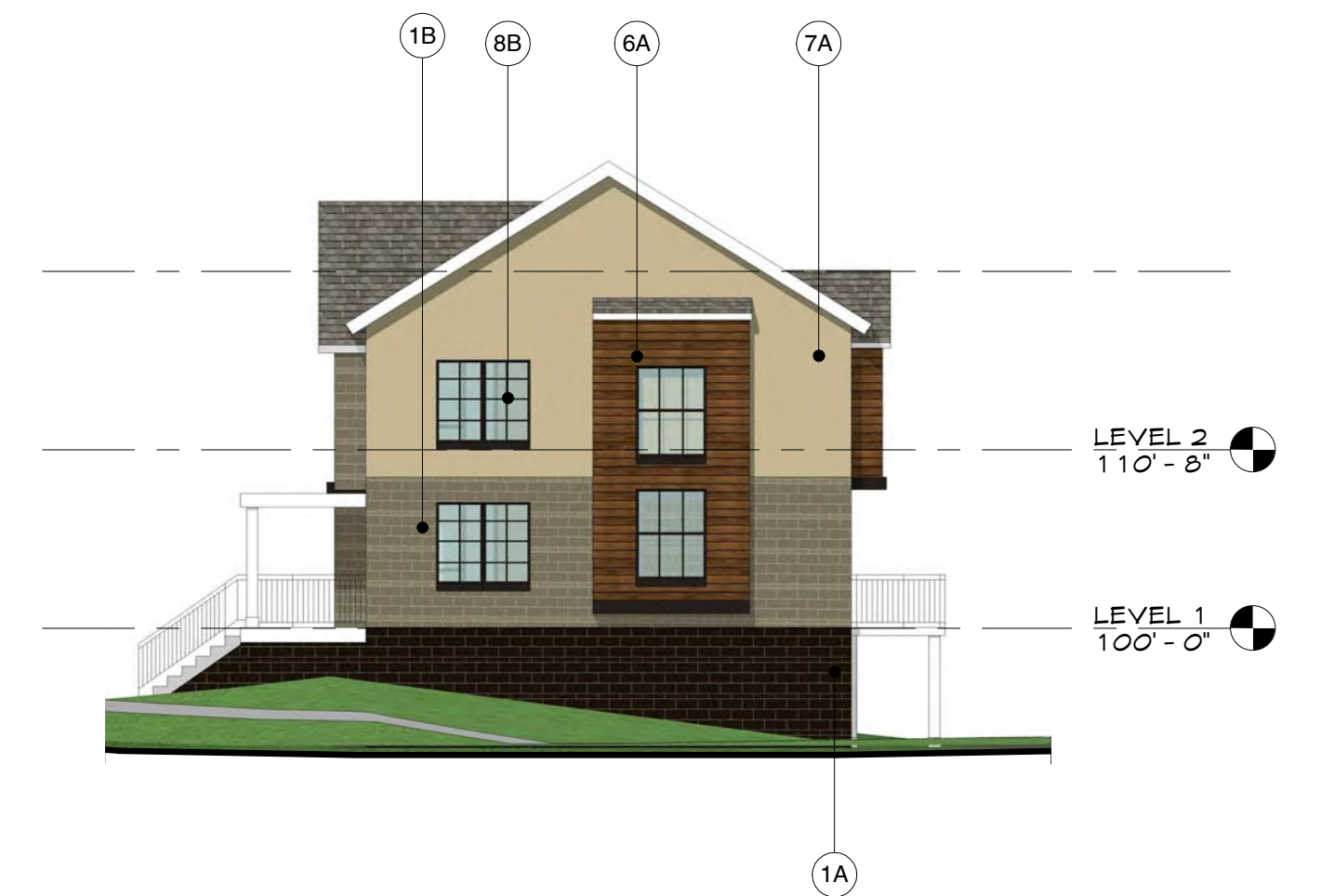
4 EAST ELEVATION AT TH1
A3.3 3/32" = 1'-0"



3 NORTH ELEVATION AT TH1
A3.3 3/32" = 1'-0"



2 WEST ELEVATION AT TH1
A3.3 3/32" = 1'-0"



1 SOUTH ELEVATION AT TH1
A3.3 3/32" = 1'-0"